



MARCH JOINT POWERS AUTHORITY

IMPORTANT COVID-19 NOTICE

IN AN EFFORT TO PROTECT PUBLIC HEALTH AND PREVENT THE SPREAD OF COVID-19 (CORONAVIRUS) AND TO ENABLE APPROPRIATE SOCIAL DISTANCING, THE AUTHORITY ENCOURAGES MEMBERS OF THE PUBLIC TO WATCH THE MEETING REMOTELY AND NOT TO ATTEND IN PERSON. IF YOU WOULD LIKE REMOTE ACCESS TO VIEW THE MEETING, PLEASE EMAIL THE CLERK AT CLERK@MARCHJPA.COM BY 8:00 A.M. ON MONDAY, MAY 9, 2022 AND WE WILL PROVIDE REMOTE ACCESS INSTRUCTIONS.

MEMBERS OF THE PUBLIC WHO WISH TO ATTEND IN PERSON ARE ENCOURAGED TO WEAR A MASK. WITH SPACE STRICTLY LIMITED, WE STRONGLY ENCOURAGE MEMBERS OF THE PUBLIC TO UTILIZE THE REMOTE ACCESS APPROACH DESCRIBED ABOVE OR TO SIMPLY PROVIDE COMMENT ON ITEMS OF INTEREST THROUGH THE METHODS BELOW.

MEMBERS OF THE PUBLIC WHO WISH TO COMMENT ON MATTERS BEFORE THE AIRPORT LAND USE STUDY COMMITTEE MAY ALSO PARTICIPATE IN THE FOLLOWING WAYS:

- (1) COMMENTS AND CONTACT INFORMATION CAN BE EMAILED TO CLERK@MARCHJPA.COM BY 8 A.M. ON THE DAY OF THE SCHEDULED MEETING TO BE INCLUDED IN THE WRITTEN RECORD; OR
- (2) A REQUEST TO SPEAK CAN BE EMAILED TO CLERK@MARCHJPA.COM AND, AT THE TIME OF THE REQUESTED AGENDA ITEM, THE CLERK WILL PLACE A PHONE CALL TO THE COMMENTER AND ALLOW THEM TO SPEAK TO THE COMMITTEE VIA SPEAKER PHONE DURING THE LIVE MEETING FOR UP TO THREE MINUTES; OR
- (3) ATTENDANCE IN PERSON, FILL OUT SLIP, AND ONCE RECOGNIZED, YOU MAY SPEAK FOR UP TO THREE MINUTES. PLEASE NOTE WE STRONGLY ENCOURAGE REMOTE OPTIONS; OR
- (4) LOGGING ON TO REMOTE ACCESS LINK AND USING “RAISE MY HAND” FUNCTION. ONCE RECOGNIZED, YOU CAN SPEAK FOR UP TO THREE MINUTES.

ONLY ONE PERSON AT A TIME MAY SPEAK BY TELEPHONE AND ONLY AFTER BEING RECOGNIZED BY THE CHAIR.

PLEASE BE MINDFUL THAT THE TELECONFERENCE WILL BE RECORDED AS ANY OTHER MEETING IS RECORDED, AND ALL OTHER RULES OF PROCEDURE AND DECORUM WILL APPLY WHEN ADDRESSING THE AIRPORT LAND USE STUDY COMMITTEE BY TELECONFERENCE. FINALLY, IT IS REQUESTED THAT ANY MEMBER OF THE PUBLIC ATTENDING WHILE ON THE TELECONFERENCE TO HAVE HIS/HER/THEIR PHONE SET ON “MUTE” TO ELIMINATE BACKGROUND NOISE OR OTHER INTERFERENCE.

To Join March Joint Powers Authority Airport Land Use Study Committee Meeting Via Zoom:

<https://us04web.zoom.us/j/6573841741>

Meeting ID: 657 384 1741

One tap mobile (from mobile or electronic device)

+16699006833,,6573841741# US (San Jose)

+13462487799,,6573841741# US (Houston)

Join by SIP, 6573841741@zoomcrc.com

Join by H.323, 162.255.37.11 (US West), 162.255.36.11 (US East)



MARCH JOINT POWERS AUTHORITY

**NOTICE OF THE
AIRPORT LAND USE STUDY COMMITTEE
MEETING**

of
THE MARCH JOINT POWERS COMMISSION
of the
MARCH JOINT POWERS AUTHORITY
and the
MARCH INLAND PORT AIRPORT AUTHORITY
and the
SUCCESSOR AGENCY - MARCH JOINT POWERS AUTHORITY
of the
FORMER MARCH JOINT POWERS REDEVELOPMENT AGENCY
City of Moreno Valley • City of Riverside • City of Perris • County of Riverside
and the
MARCH JOINT POWERS COMMISSION
of the
MARCH JOINT POWERS UTILITIES AUTHORITY
City of Moreno Valley • City of Riverside • City of Perris

Via In-Person and Zoom

Notice is hereby given that a **Airport Land Use Study Committee Meeting** of the March Joint Powers Commission of the March Joint Powers Authority will be held Via in-person and Zoom and at the **March Joint Powers Authority Offices, 14205 Meridian Parkway, Suite 140, Riverside, California 92518** on **Monday, May 9, 2022 at 9:30 a.m.**

This Notice was posted on 05/05/22 at the following locations:

March Joint Powers Authority Office
14205 Meridian Parkway, Ste. 140
Riverside, CA 92518

On May 5, 2022, Notice was sent to each member of the March Joint Powers Commission.

I hereby certify that the foregoing Notice is a full, true, and correct copy of the Notices posted for the Airport Land Use Study Committee Meeting.

Cindy Camargo

Cindy Camargo, Clerk
March Joint Powers Authority Airport Land Use Study Committee

AIRPORT LAND USE STUDY COMMITTEE
of
THE MARCH JOINT POWERS COMMISSION
of the
MARCH JOINT POWERS AUTHORITY
and the
MARCH INLAND PORT AIRPORT AUTHORITY
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SUCCESSOR AGENCY - MARCH JOINT POWERS AUTHORITY
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MARCH JOINT POWERS COMMISSION
of the
MARCH JOINT POWERS UTILITIES AUTHORITY
City of Moreno Valley • City of Riverside • City of Perris

May 9, 2022 at 9:30 a.m.

March Joint Powers Authority Airport Land Use Study Committee

Meeting Location:
In-Person and Zoom
14205 Meridian Parkway, Suite 140
Riverside, CA 92518

ALL MEETINGS ARE OPEN TO THE PUBLIC.

Interested persons are encouraged to participate in the activities of the March JPA. Anyone wishing to speak on an agenda item or on an issue of general concern should complete a "Speaker's Request Form" available in the Meeting Room and online.

ADA: If you require special accommodations during your attendance at a meeting, please contact the JPA at (951) 656-7000 at least 24 hours in advance of the meeting time.

March Joint Powers Authority
14205 Meridian Parkway, Suite 140, Riverside 92518
Phone: (951) 656-7000 FAX: (951) 653-5558

**Meeting
of the
AIRPORT LAND USE STUDY COMMITTEE
of the
MARCH JOINT POWERS AUTHORITY**

Via In-Person and Zoom

Monday, May 9, 2022 at 9:30 a.m.

**MARCH JOINT POWERS AUTHORITY-
14205 Meridian Parkway, Suite 140
Riverside, CA 92518**

AGENDA

- 1. Call to Order**
- 2. Approval of the Minutes – No previous minutes**
- 3. Public Comments**
- 4. Reports, Discussions and Actions**
 - a. Report: Introduction to General Plan Amendment GP 21-01, Specific Plan SP-21-01, Tentative Parcel Map 38063, Plot Plan PP 21-03, and Plot Plan PP 21-04: Multiple applications by Meridian Park, LLC allowing for approximately 13 buildable lots on 253.87 acres, 448.34 acres for conservation easement, 37.7 acres for public streets, 60.28 acres for public parks, and 17.72 acres for private open space for the West Campus Upper Plateau Specific Plan generally located ¼ mile south of Alessandro Boulevard on both sides of the Barton Street alignment.**
- 5. Reports and comments from Staff or Airport Land Use Study Committee members regarding activities in their jurisdictions**
- 6. Next Airport Land Use Study Committee Meeting – No future meeting planned at this time.**
- 7. Adjournment**

In accordance with Government Code section 65009, anyone wishing to challenge any action taken by the members appointed by the March Joint Powers Commission of the entity listed in this agenda above in court may be limited to raising only those issues raised at the public hearing described in the notice or raised in written correspondence delivered to the hearing body, at or prior to the public hearing. Any written correspondence submitted to one or more of the March JPA Commissioners regarding a matter on this Agenda shall be carbon copied to the Commission Clerk and the project planner, if applicable, at or prior to the meeting date first referenced above.

Copies of written documentation relating to each item of business described above are on file in the office of the March Joint Powers Authority (March JPA), 14205 Meridian Parkway, Suite 140, Riverside, California and are available for public inspection during regular office hours which are 7:30 a. m. to 5:30 p.m., Monday through Thursday, Friday-Closed. Written materials distributed to the March Joint Powers

Airport Land Use Study Committee within 72 hours of the Airport Land Use Study Committee meeting are available for public inspection immediately upon distribution in the March JPA office at 14205 Meridian Parkway, Ste. 140, Riverside, California (Government Code Section 54957.5(b)(2)). Copies of written materials may be purchased for \$0.20 per page. Pursuant to State law, this agenda was posted at least 72 hours prior to the meeting.

I hereby certify under penalty of perjury, under the laws of the State of California that the foregoing agenda was posted in accordance with the applicable legal requirements.

Dated: May 5, 2022

Signed: Cindy Camargo
Cindy Camargo, Clerk
MJPA Airport Land Use Study Committee

ADA: If you require special accommodations during your attendance at a meeting, please contact the March JPA at (951) 656-7000 at least 24 hours in advance of the meeting time.

**March Joint Powers Authority
14205 Meridian Parkway, Suite 140, Riverside, CA 92518
Phone: (951) 656-7000 FAX: (951) 653-5558**

**MARCH JOINT POWERS
AIRPORT LAND USE STUDY COMMITTEE
OF THE
MARCH JOINT POWERS AUTHORITY**

***Reports, Discussion and Actions
Agenda Item No. 4a***

Meeting Date: May 9, 2022

Reports, Discussions and Actions:

Report: Introduction to General Plan Amendment GP 21-01, Specific Plan SP-21-01, Tentative Parcel Map 38063, Plot Plan PP 21-03, and Plot Plan PP 21-04: Multiple applications by Meridian Park, LLC allowing for approximately 13 buildable lots on 253.87 acres, 448.34 acres for conservation easement, 37.7 acres for public streets, 60.28 acres for public parks, and 17.72 acres for private open space for the West Campus Upper Plateau Specific Plan, generally located ¼ mile south of Alessandro Boulevard on both sides of the Barton Street alignment.

- **GP 21-01:** A request to amend the General Plan Land Use Map eliminating 122-acres Parks/Recreation/Open Space, 63-acres of Industrial and 622.5 acres Business Park land uses to adopt the West Campus Upper Plateau Specific Plan (SP-9) on approximately 357.5 gross acres, adopting Public Facility on 2.87 acres, and adopting Parks/Recreations/Open Space on 448.05 acres for a future conservation area.
- **SP-21-01:** A request for Specific Plan approval for the West Campus Upper Plateau Specific Plan (SP-9) containing development standards, design guidelines, infrastructure master plans, maintenance responsibilities, and implementation procedures necessary to develop the West Campus Upper Plateau site consistent with the requested General Plan Amendment designations. The proposed land uses within Specific Plan SP-9 include 42.22 acres of Mixed Use, 66.35 acres of Business Park, 143.46 acres of Industrial, 28.86 acres of streets and roadways, 60.28 acres of Parks/Recreation/Open Space for parks use, 17.72 acres of open space for landscaped buffer areas, and 2.84 acres of Public Facilities.
- **TPM 38063:** A Tentative Parcel Map on 817.9 acres, providing 13 buildable lots on 253.87 acres, 448.34 acres for conservation easement, 37.7 acres for public streets, 60.28 acres for public parks, and 17.72 acres for private open space.
- **PP 21-03:** A Plot Plan proposal to allow a 1,250,000 sq/ft distribution warehouse on 59.55 acres located in the Industrial (proposed) zoning district at 20133 Cactus Avenue.
- **PP 21-04:** A plot plan proposal to allow a 550,000 sq/ft distribution warehouse on 27.58 acres located in the Industrial (proposed) zoning district at 20600 Cactus Avenue.

Applicant: Meridian Park, LLC.

Timing of Airport Land Use Study Subcommittee Discussion: This item is being brought to the Airport Land Use Study Subcommittee as an initial discussion prior to the May 12, 2022 Riverside County Airport Land Use Commission review of the aforementioned applications.

Project Location:

The project site is ¼ mile south of Alessandro Boulevard. The development is generally surrounded by county residential development to the north; the Mission Grove residential development to the northwest and west, Grove Community Church to the south, and the Orangecrest residential development to the south and southeast. The South Campus Upper Plateau project consists of approximately 253.87 acres of buildable lots and 28.86 acres of roadways generally located within the former March AFB Weapon Storage Area and adjacent land north of the Weapons Storage Area. The project also includes an approximate 60-acre park area located west of the planned Barton Street alignment and a 448-acre conservation area, which is planned to surround the developable area and parks area. Overall, the total project area inclusive of all open space and the conservation areas is 817.9 acres.

2012 Center for Biological Diversity et al. v. Bartel et al. Settlement Agreement

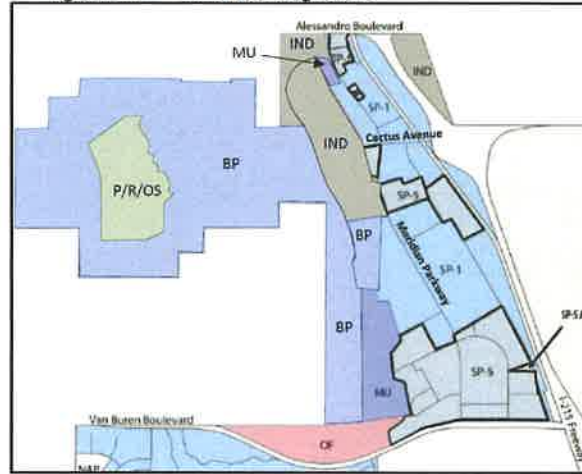
On December 29, 2003 and May 22, 2006, the US Fish and Wildlife Service and California Department of Fish and Game determined that the trade out criteria identified in the 1999 Stephen's Kangaroo Rat Biological Opinion had been satisfied allowing the trade-out of the March JPA management area for the Potrero Reserve. On August 27, 2009, the Center for Biological Diversity and San Bernardino Valley Audubon Society initiated a lawsuit against the U.S. Fish and Wildlife Service alleging a failure to conduct appropriate review under the National Environmental Quality Act. On September 21, 2012, the Center for Biological Diversity, San Bernardino Valley Audubon Society, LNR Riverside, LLC and March JPA entered into a settlement agreement to achieve a full and complete resolution of all claims asserted by the plaintiffs, to allow conservation of portions of the former management area while allowing development of other areas, as depicted below.



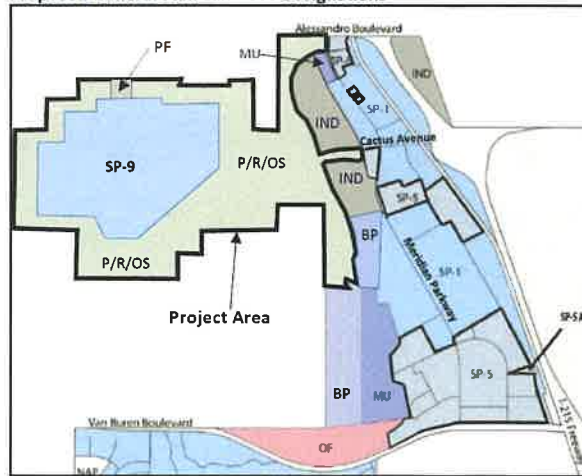
General Plan Amendment:

General Plan Amendment GP 21-01 would amend the General Plan Land Use Map from 122-acres of Parks/Recreation/Open Space, 63-acres of Industrial and 622.5 acres Business Park to Specific Plan (SP-9) on approximately 351.0 gross acres, Public Facility on 2.87 acres, and to Parks/Recreations/Open Space on 448.0 acres. The 448.05 acres would be placed in a conservation easement to manage the open space in perpetuity for wildlife and for passive recreation including hiking and mountain biking.

Existing General Plan Land Use Designations



Proposed General Plan Land Use Designations



West Campus Upper Plateau Specific Plan Land Use:

The West Campus Upper Plateau land uses includes 143.46 acres of Industrial development, 65.35 acres of Business Park development, and 42.22 of Mixed-Use development. The Specific Plan includes 60 acres of planned park space west of Barton Street. Landscape buffers totaling 14.38 acres are provided to the north and south of the project to screen/buffer the project existing residential uses to the north and south. Additionally, the landscape buffers provide for the preservation of two of the weapon storage bunkers.



We

West Campus Upper Plateau Specific Plan Design Guidelines:

The proposed West Campus Upper Plateau project design guidelines are similar to and generally consistent with the design guidelines used throughout the Meridian development. They differ in that the design objective for Meridian was to complement the existing earth tones of March Air Reserve Base. West Campus Upper Plateau, however, incorporates a more contemporary appearance through use of lighter off-white and gray color palettes and through use of stone, glass and steel accents not incorporated into the previous Meridian development. The development proposes taller concrete/masonry walls to screen truck courts (14' versus 12'), and incorporates a mix of concrete/masonry walls, metal tubular fences, and composite split rail fences for decorative and security purposes near the future conservation area. A project themed monument features is planned at the major project entrance at Cactus Avenue, with minor monumentation planned along Barton Street.





Major Monument Sign
East Project Entry on Cactus
Avenue

Minor Monument Sign
South Barton Street

Minor Monument Sign
North Barton Street

MONUMENT SIGN EXAMPLES



12' Long Minor Monument Signs at
Barton Street, North and South Entries



20' Long Major Monument Sign with Wing Walls at Cactus Avenue Site Entry

CONSERVATION / OPEN SPACE
PERIMETER FENCING



Tube Steel Fencing

SPLIT RAIL FENCING ALONG BARTON STREET
MULTI-PURPOSE TRAIL



Textured Matte Finish Vinyl



Textured Concrete



Tube Steel Fencing

West Campus Upper Plateau Specific Plan Development Standards:

The proposed development includes similar development standards to the Meridian South Campus, and allows the Business Enterprise use, a use allowing up to a 200,000 sq/ft warehouse, within the Business Park and Mixed-Use land use designations. The table below identifies the requested development standards for the Industrial, Business Park and Mixed-Use zones:

Development Standards						
Dimension	Meridian North Campus			Proposed West Campus Upper Plateau		
	Business Park	Industrial	Mixed-use	Business Park	Industrial	Mixed-use
Min. Lot Size	30,000 s/f	30,000 s/f	30,000 s/f	1 acre	5 acre	1 acre
Front Yard	20'	20'	20'	20'	20'	20'
Rear Yard	0'	0'	0'	0'	0'	0'
Street Side	20'	20'	15'	20'	20'	20'
Building Height	35' – 80' ²	35' – 80' ²	50'	50' – 80' ²	50' – 80' ²	40' – 80' ²
Screen Wall	14'	14'	14'	14'	14'	14'
F.A.R.	.45	.50	.35	.45	.50	.35
Landscape	10%	10%	20%	10%	10%	20%

Project Traffic:

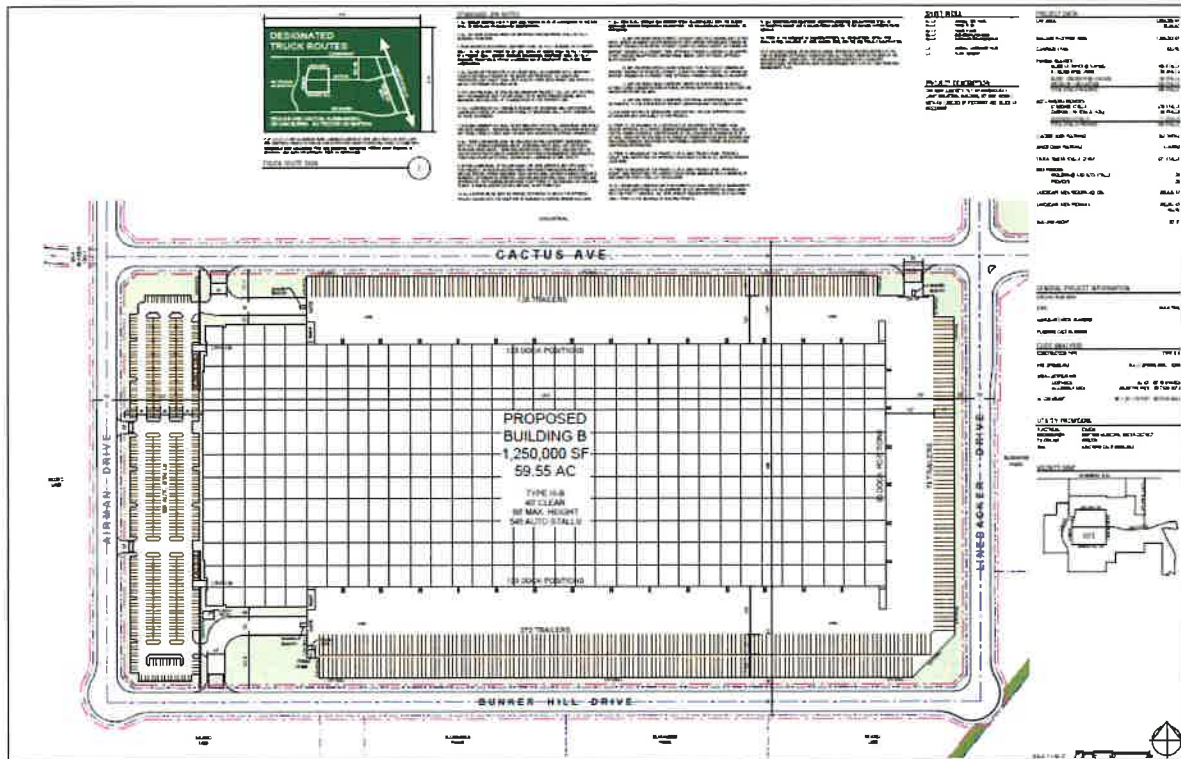
The development is projected to have 32,962 vehicle trips, consisting of 30,890 employee vehicle trips, 1,870 truck trips and 762 vehicle trips associated with the park use. The vehicular circulation is designed to direct all truck trips to Cactus Avenue or Alessandro Boulevard (via Brown Street), with no truck trips using Barton Street. Employee vehicles for the Mixed-Use lots adjacent to Barton Street would have access to Barton Street. The Specific Plan also includes development standards and design guidelines for the development.

TABLE 4-2: PROJECT TRIP GENERATION SUMMARY

Land Use	Quantity Units ¹	AM Peak Hour			PM Peak Hour			Daily
		In	Out	Total	In	Out	Total	
Building B: High-Cube Fulfillment	1,250,000 TSF							
Passenger Cars:		99	30	129	50	130	180	2,188
Trucks:		18	5	23	7	19	26	474
Total Trips²		117	35	152	57	149	206	2,662
Building C: High-Cube Fulfillment	550,000 TSF							
Passenger Cars:		44	13	57	22	57	79	964
Trucks:		8	2	10	3	8	11	208
Total Trips²		52	15	67	25	65	90	1,172
Remaining Industrial: High-Cube Fulfillment	1,212,710 TSF							
Passenger Cars:		96	29	125	49	126	175	2,122
Trucks:		18	5	23	7	18	25	460
Total Trips²		114	34	148	56	144	200	2,582
Business Park	1,370,893 TSF							
Office Passenger Cars:	351,268 TSF	436	77	513	81	390	471	3,464
Office Passenger Cars:	60,000 TSF	95	17	112	19	90	109	744
Warehouse Passenger Cars:	959,625 TSF	74	17	91	249	883	1,132	11,392
Warehouse Trucks:		31	7	38	8	30	38	548
Business Park (Mixed-Use, 75%)	448,804 TSF							
Office Passenger Cars:	134,641 TSF	193	35	228	36	179	215	1,524
Warehouse Passenger Cars:	314,163 TSF	24	6	30	82	289	371	3,730
Warehouse Trucks:		10	2	12	3	10	13	180
Total Business Park Trips		863	161	1,024	478	1,871	2,349	21,582
Retail (Mixed-Use, 25%)	106,858 TSF							
Passenger Cars:		115	70	185	272	283	555	7,216
Pass-by Reduction (AM: 0%; PM/Daily: 34%) ⁴		0	0	0	-92	-92	-185	-2,454
Total Retail Trips		115	70	185	180	191	370	4,762
Active Park	10.03 AC	33	33	66	23	23	46	502
Public Park	51.73 AC	17	17	34	12	11	23	260
Total Park Trips		50	50	100	35	34	69	762
Total Passenger Cars		1,226	344	1,570	803	2,369	3,171	31,652
Internal Trip Reduction ³		-57	-57	-114	-28	-28	-56	-560
Total Trucks		85	21	106	28	85	113	1,870
Project Total Trips		1,254	308	1,562	803	2,426	3,228	32,962

Design Plan 21-03:

Design Plan 21-03 is a proposal to construct a 1,250,000 sq/ft distribution warehouse on 59.55 acres located in the Industrial (proposed) zoning district at 20133 Cactus Avenue. Specifically, the project is the southernmost Industrial parcel bounded by the planned Cactus Avenue alignment to the north, Linebacker Drive to the east, Bunker Hill Drive to the south, and Airman Drive to the west. The site accommodates 545 passenger cars, which would include carpool, electric vehicle, and accessible stalls and 28 bike parking spaces. The building incorporates truck courts on the north and south side with 241 dock doors, 4 at-grade doors and 471 truck/trailer parking spaces. The truck courts are screened behind 14' screening walls. Access is proposed at three locations and is still under review by the March JPA traffic engineering department. The building is 1,850' in length and 660' in width, with a maximum height of 50'. The applicant is proposing 200,720 sq/ft of landscape area (7.92 % of site area).



MERIDIAN
 UPPER PLATEAU - BUILDING B - PLOT PLAN
 MERCURY PARK LLC

NO.	DESCRIPTION	DATE
1	ISSUED FOR PERMITTING	08/11/2021
2	ISSUED FOR PERMITTING	08/11/2021
3	ISSUED FOR PERMITTING	08/11/2021
4	ISSUED FOR PERMITTING	08/11/2021
5	ISSUED FOR PERMITTING	08/11/2021
6	ISSUED FOR PERMITTING	08/11/2021
7	ISSUED FOR PERMITTING	08/11/2021
8	ISSUED FOR PERMITTING	08/11/2021
9	ISSUED FOR PERMITTING	08/11/2021
10	ISSUED FOR PERMITTING	08/11/2021

A1-1-P



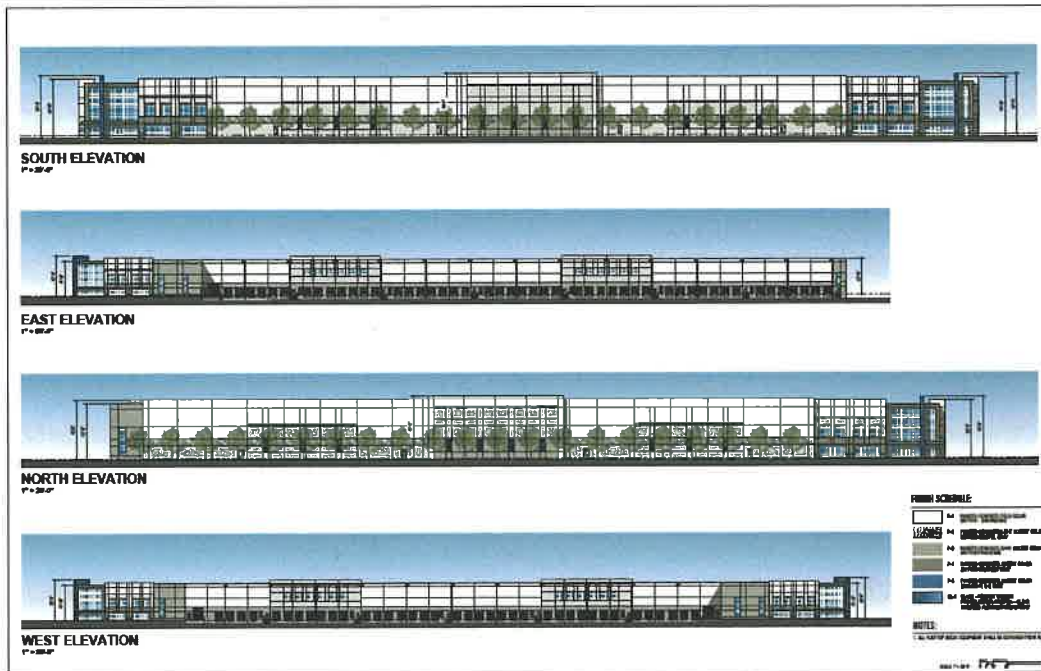
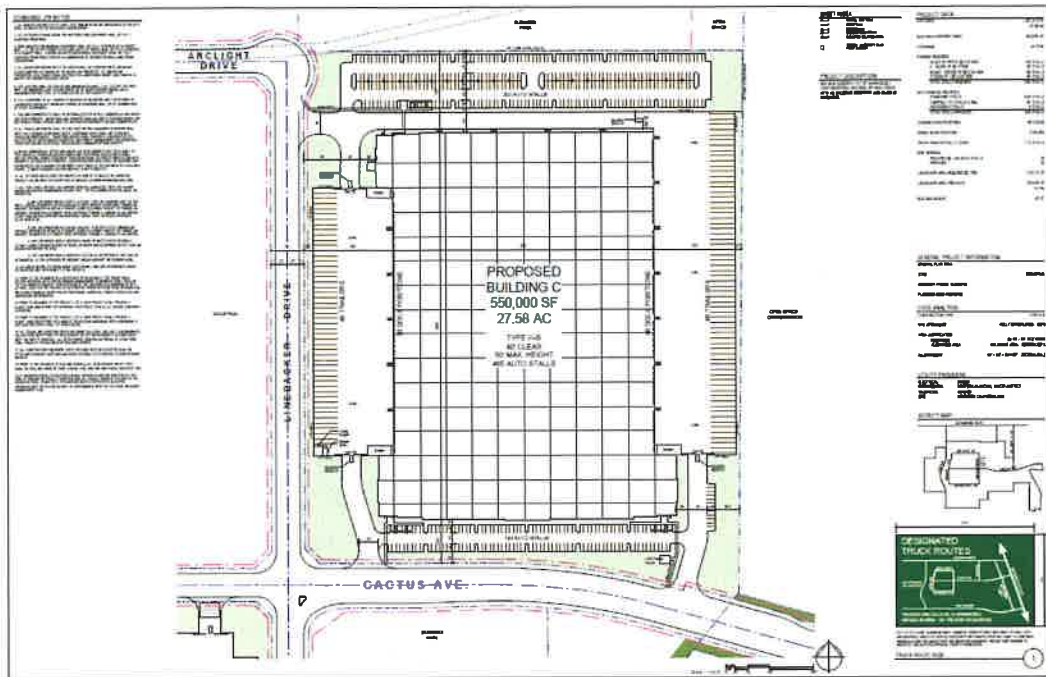
WEST CAMPUS UPPER PLATEAU - BUILDING B - ELEVATIONS
MERIDIAN PARK LLC

NO.	REVISION	DATE

A3-1

Design Plan 21-04:

Design Plan 21-04 is a proposal to construct a 550,000 sq/ft distribution warehouse on 27.58 acres located in the Industrial (proposed) zoning district at 21600 Cactus Avenue. Specifically, the project is the easternmost Industrial parcel located adjacent to Cactus Avenue and Linebacker Drive. The site accommodates 465 passenger cars, which includes carpool, electric vehicle, and accessible stalls and 24 bike parking spaces. The site incorporates truck courts on the east and west side with 86 dock doors, 3 at-grade doors and 113 truck/trailer parking spaces. The truck courts are screened behind 14' screening walls. Access is proposed at three locations and is still under review by the March JPA Traffic Engineering Department (VRPA Consultants). The building is 888' in length and 500' in width, with a maximum height of 48'. The applicant is proposing 160,000 sq/ft of landscape area (13.3 % of site area).



2018 March ARB AICUZ:

The majority of the site is within the 60-65 dB CNEL noise contour, though a portion is located outside (below) the 60 dB contour, as identified in the 2018 March Air Reserve Base Air Installation Compatibility Use Zone Study (AICUZ). The AICUZ identifies that this level of noise exposure is acceptable for industrial, commercial, recreational and residential land uses as identified within Table 6-2 of the AICUZ.

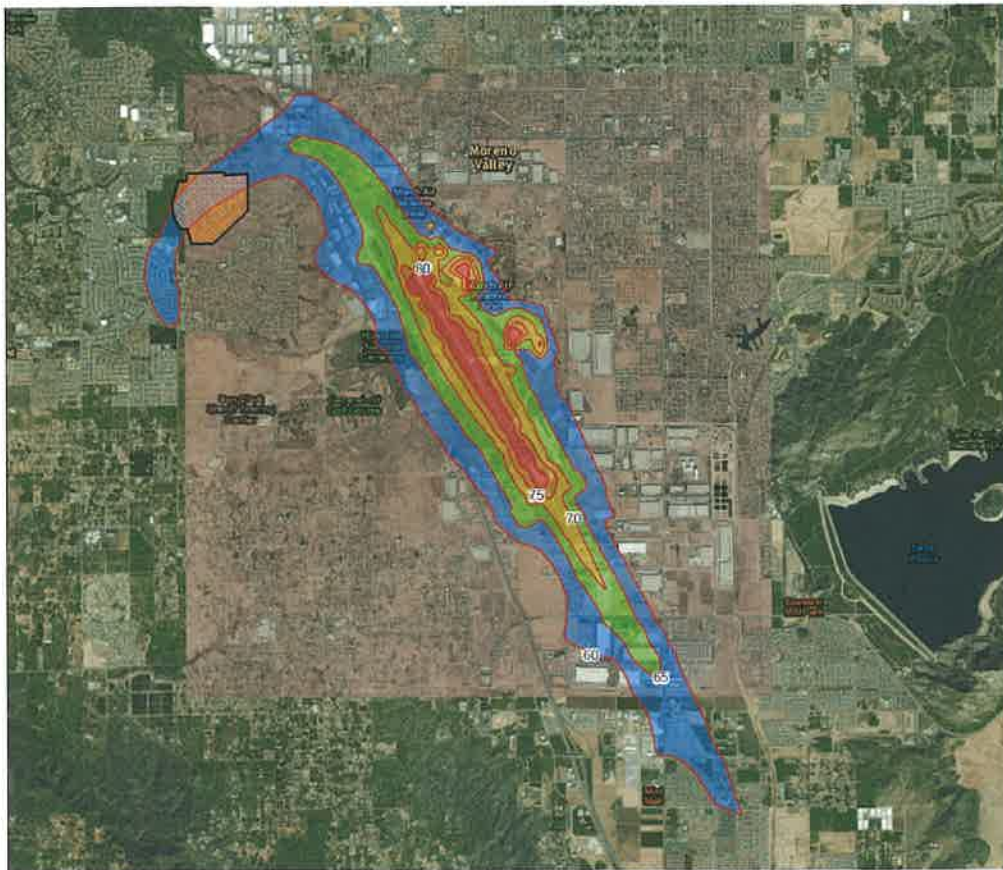
Table 6-2. Generalized Land Use Categories and Noise/Safety Compatibility

Generalized Land Use Category ³	Noise Zone (dB CNEL)						CZ	APZ I	APZ II
	<65	65-69	70-74	75-79	80-84	85+			
Residential	Yes	No ¹	No ¹	No	No	No	No	No	No
Commercial	Yes	Yes	Yes ²	Yes ²	No	No	No	Yes ²	Yes ²
Industrial	Yes	Yes	Yes	Yes	Yes ²	No	No	Yes ²	Yes ²
Public/Quasi-Public	Yes	Yes ²	Yes ²	Yes ²	No	No	No	No	Yes ²
Recreation	Yes	Yes ²	Yes ²	No	No	No	No	Yes ²	Yes ²
Open/Agriculture/Low Density	Yes	Yes ²	Yes ²	Yes ²	Yes ²	Yes ²	No	Yes ²	Yes ²
Undesignated	Yes	No	No	No	No	No	No	No	No

¹ Incompatible with exceptions

² Compatible with restrictions

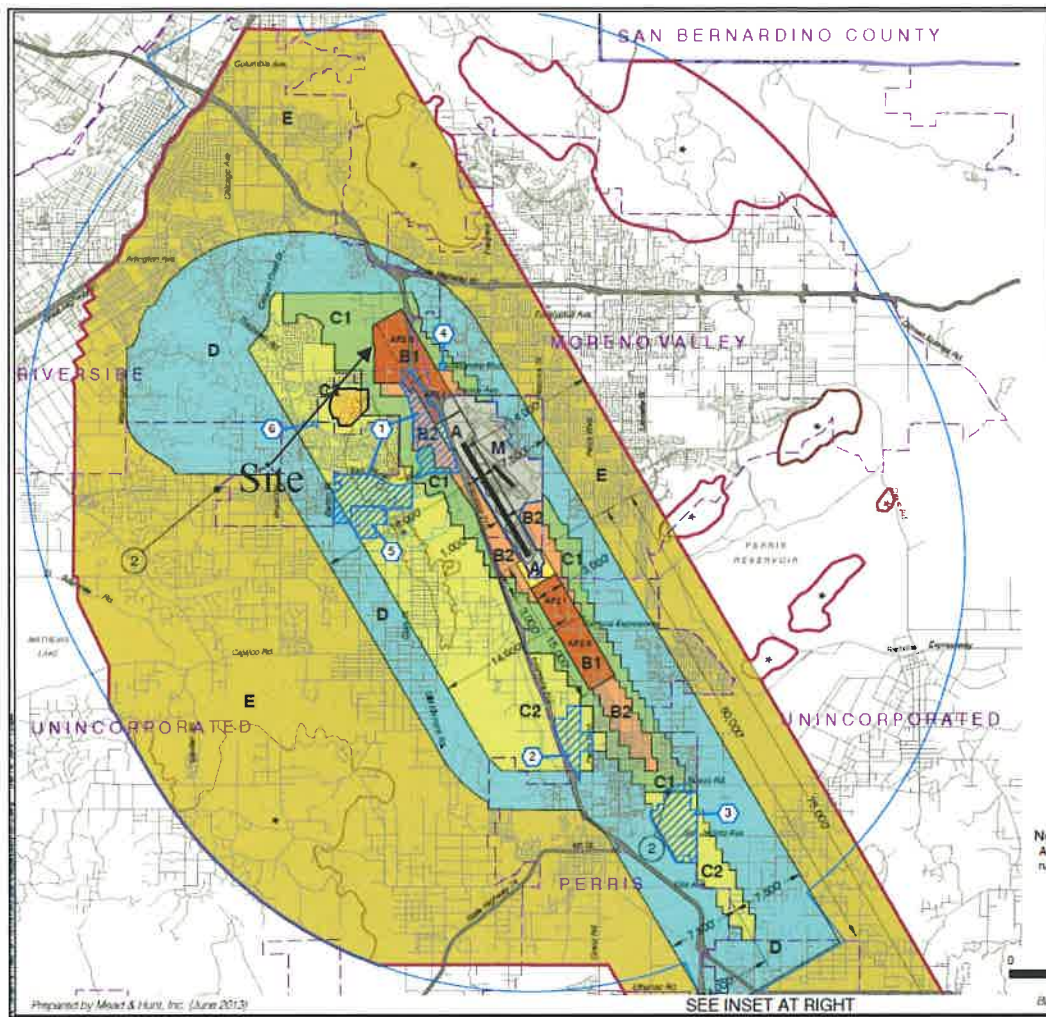
³ This generalized table demonstrates the land compatibility guidelines. Refer to Appendix A for use in determining land use compatibility.



2014 March Air Reserve Base/Inland Port Airport Compatibility Plan:

As a legislative action, the submitted applications are subject to Riverside County Airport Land Use Commission review. The site area is located within the C-1 (Primary Approach/Departure Zone) and C2 (Flight Corridor Zone). Non-residential development in the C1 compatibility zone is acceptable at an average intensity of 100 persons per acre, and a maximum density of 250 persons per acre for the most concentrated site acre. Prohibited uses in the C1 zone include children’s schools, day care centers, hospitals, congregate care centers, hotels, places of assembly and outdoor venues such as spectator-oriented sports venues, amphitheatres, and drive-

in theatres. Non-residential development in the C2 compatibility zone is acceptable at an average intensity of 200 persons per acre, and a maximum density of 500 persons per acre for the most concentrated site acre. Prohibited uses in the C2 zone include outdoor venues such as spectator-oriented sports venues, amphitheatres, and drive-in theatres.



Community Input:

A Community Zoom meeting was held on February 24, 2022, at 6:00 PM to discuss the various applications involving the West Campus Upper Plateau project. Among the primary comments were the desire to allow residential development within the project area (rather than employment uses) and opposition to warehouse/truck uses in proximity to existing neighborhoods. A follow-up meeting was held on April 21, 2022, with four principal project opponents who reside in the residential areas surrounding the project. Again, the primary issues were support for residential uses within the project area and opposition to warehouse/trucking activities. March JPA staff agreed to elicit comments from March ARB officials regarding the possibility of residential uses within the project area. Additional meetings have been held by project opponents, however JPA staff was not invited to these meetings.

Current Review Status:

As of Monday, May 5, the project submittal is in review by the reviewing departments and agencies. Numerous items have been made available to the public on the March JPA website, including: the Notice of Preparation, the Initial Study, the 2012 Settlement Agreement (CBD et. al v. Bartel et.al), the recorded February 24, 2022 Community Zoom Meeting, Plot Plan 21-03, Plot Plan 21-04, and the draft Land Use Plan.

Schedule:

No definitive project schedule has been established. The next major step would be release of the draft EIR, which would likely occur during the summer of 2022.

Attachments:

1. Riverside County Airport Land Use Commission Staff Report, dated May 12, 2022
2. Draft West Campus Upper Plateau Specific Plan (2nd Draft), dated April 2022

Attachment #1

Riverside County Airport Land Use Commission Staff Report, dated May 12, 2022

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.5

HEARING DATE: May 12, 2022

CASE NUMBER: ZAP1515MA22 - Meridian Park West, LLC (Representative: Waypoint Property Group)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NO: GP 21-01 (General Plan Amendment), SP21-01 (Specific Plan), PP21-03 and PP21-04 (Plot Plans), TTM38063 (Tentative Tract Map)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zones B1, B2, C1, C2 (High Terrain Zone)

Noise Levels: Between 60 - 70 CNEL contour (with a portion below 60 CNEL contour)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed General Plan Amendment and Specific Plan CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and also find the proposed Plot Plans and Tentative Tract Map CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: The applicant proposes the West Campus Upper Plateau Specific Plan, encompassing 817.9 acres within multiple Airport Compatibility Zones located southerly of Alessandro Boulevard, westerly of Meridian Parkway, northerly of Grover Community Drive, and easterly of Trautwein Road. As part of this project, the applicant proposes amending the General Plan land uses to increase Parks/Recreation and Open Space from 122 acres to 453 acres, eliminate approximately 622.5 acres of Business Park, eliminate approximately 63 acres of Industrial property, approve a 2.6 acre Public Facility area for an existing water tank, and adopt the West Campus Upper Plateau Specific Plan (SP-) on approximately 351 acres and creating policies for the future recordation of a 445 acre Conservation Area. The applicant also proposes to adopt Specific Plan SP-9 containing development standards, design guidelines, infrastructure master plans, maintenance responsibilities, phasing schedule, and implementation procedures necessary to develop a 359 acre business park and adjacent park space. The Specific Plan proposes 43.1 acres of Mixed Use, 66.4 acres of Business Park, 143.3 acres of Industrial, 28.9 acres of streets

and roadways, 10 acres of developed Parks/Recreation/Open Space, 64.5 acres of undeveloped Parks/Recreation/Open Space, and 3.5 acres of Public Facilities. The Specific Plan will adopt zoning on the properties consistent with the Specific Plan land use designations. The applicant also proposes to construct 2 industrial buildings with mezzanines on separate parcels totaling 1,820,000 square feet on (combined) 115.88 acres, located northerly of (future roads) Bunker Hill Drive, easterly of Airman Drive, southerly of Arclight Drive, and westerly of Linebacker Drive. (Only development entitlements for PP21-03 and PP21-04 have been submitted with this application. No development projects for the other parcels have been proposed at this time.) The applicant also proposes a tentative tract map to divide 359.6 acres into 17 buildable lots and 7 lettered lots for streets/open space.

PROJECT LOCATION: The overall 817.9 acres project area is located southerly of Alessandro Boulevard, westerly of Meridian Parkway, northerly of Grover Community Drive, and easterly of Trautwein Road. The proposed 2 industrial buildings are located approximately 9,821 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the overall project boundary is located within Zones B1 (22.22 acres), B2 (15 acres), C1 (306 acres) and C2 (483 acres). However, the only physical development proposed at this time (2 industrial buildings) are located entirely within Compatibility Zones C1 and C2, which limits average intensity to 100 people per acre and 200 people per acre respectively.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan (ALUCP) and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Manufacturing – 1 person per 200 square feet,
- Warehouse – 1 person per 500 square feet, and
- Office – 1 person per 200 square feet.

The project proposes to construct 2 industrial buildings with mezzanines totaling 1,820,000 square feet on separate parcels in separate compatibility zones (C1 and C2). An individual lot-by-lot analysis is included below:

- Building B, Parcel 17 (59.61 acres) in Zone C2, proposes a 1,260,000 square foot industrial manufacturing building, which includes 1,240,000 square feet of manufacturing area, 10,000 square feet of first floor office area, and 10,000 square feet of second floor office mezzanine area, accommodating an occupancy of 6,300 people, and resulting in an average intensity of 106 people per acre, which is consistent with the Compatibility Zone C2 average intensity criterion of 200 people per acre.
- Building C, Parcel 5 (27.58 acres) in Zone C1, proposes a 560,000 square foot industrial warehouse building, which includes 540,000 square feet of warehouse area, 10,000 square feet of first floor office area, and 10,000 square feet of second floor office mezzanine area, accommodating an occupancy of 1,180 people, and resulting in an average intensity of 43 people per acre, which is consistent with the Compatibility Zone C1 average intensity

criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). An individual lot-by-lot analysis is included below:

- Building B. 545 standard vehicles and 471 trailer spaces, accommodating a total occupancy of 1,289 people, resulting in an average intensity of 22 people per acre, which is consistent with the Compatibility Zone C2 average intensity criterion of 200 people per acre.
- Building C. 465 standard vehicles and 113 trailer spaces, accommodating a total occupancy of 811 people, resulting in an average intensity of 29 people per acre, which is consistent with the Compatibility Zone C1 average intensity criterion of 100 people per acre.

Non-Residential Single-Acre Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the overall project boundary is located within Zones B1 (22.22 acres), B2 (15 acres), C1 (306 acres) and C2 (483 acres). However, the only physical development proposed at this time (2 industrial buildings) are located entirely within Compatibility Zones C1 and C2, which limits maximum single-acre intensity to 250 people per acre and 500 people per acre respectively. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area for each of the buildings are as follows:

- Building B includes 33,560 square feet of manufacturing area, 10,000 square feet of first floor office area, 10,000 square feet of second floor office mezzanine area, resulting in a single acre intensity of 268 people, which is consistent with the Compatibility Zone C2 single acre intensity criterion maximum of 500 people.
- Building C includes 33,560 square feet of warehouse area, 10,000 square feet of first floor office area, 10,000 square feet of second floor office mezzanine area, resulting in a single acre intensity of 167 people, which is consistent with the Compatibility Zone C1 single acre intensity criterion maximum of 250 people.

March Air Reserve Base/United States Air Force Input: Given that the project's vicinity is located near the primary runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. As of the time this staff report was prepared, comments were still pending from the Air Force.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones B1, B2, C1, and C2. The portions of the overall 817-acre site that are located within Zones B1 and B2 are designated Open Space – Conservation, which will remain passive with no development. The proposed 2 industrial buildings (and future development parcels) are located within Zones C1 and C2 where industrial development is permitted.

Noise: Although the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

depicts the entire 817-acre project boundary as being between the 60 - 70 CNEL range from aircraft noise (with a portion below the 60 CNEL contour), the location of the proposed 2 industrial buildings (and other future industrial developable parcels) are entirely located between the 60 – 65 CNEL contour range from aircraft noise (with a portion below the 60 CNEL contour). While the proposed 2 industrial buildings are not a noise-sensitive use and would not require special measures to mitigate aircraft-generated noise, such measures may be required to achieve reduced interior noise levels of 45 dBA CNEL in office areas as required pursuant to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (AMSL). At a distance of approximately 9,821 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,633 feet AMSL. The site's finished floor elevation is 1,726 feet AMSL and proposed building height is 50 feet, resulting in a top point elevation of 1,776 feet AMSL. Therefore, review of the buildings for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) was required (the project is located within the High Terrain Zone [HTZ], which automatically requires FAA OES review). The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study Nos. 2022-AWP-2725-OE thru 2022-AWP-2730-OE, 2022-AWP-2732-OE and 2022-AWP-2733-OE to this project. Its status is currently a "work in progress".

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C).

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 9,821 feet), the project utilizes underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

General Plan Amendment/Specific Plan: The applicant proposes amending the General Plan land uses to increase Parks/Recreation and Open Space from 122 acres to 453 acres, eliminate approximately 622.5 acres of Business Park, eliminate approximately 63 acres of Industrial property, approve a 2.6. acre Public Facility area for an existing water tank, and adopt the West Campus Upper Plateau Specific Plan (SP-9) on approximately 351 acres and creating policies for the future recordation of a 445 acre Conservation Area. The applicant also proposes to adopt Specific Plan SP-9 containing development standards, design guidelines, infrastructure master plans, maintenance responsibilities, phasing schedule, and implementation procedures necessary to develop a 359 acre business park and adjacent park space. The Specific Plan proposes 43.1 acres of Mixed Use, 66.4 acres of Business Park, 143.3 acres of Industrial, 28.9 acres of streets and roadways, 10 acres of developed Parks/Recreation/Open Space, 64.5 acres of undeveloped Parks/Recreation/Open Space, and 3.5 acres of Public Facilities. The Specific Plan will adopt zoning on the properties consistent with the Specific Plan land use designations. The Specific Plan

also contains language requiring project's conformity to the 2014 March ALUCP.

It is important to note that although the project's boundary is located within Zones B1 (22.22 acres), B2 (15 acres), C1 (306 acres) and C2 (483 acres), only developable land use designations are exclusively located within Zones C1 and C2. The proposed Open-Space Conservation Area land use designation is the nearest portion of the project to the Base, and is located within Zones B1 and B2. The area will be used as biological and wildlife conservation, with wildlife crossings, and recreational trails.

The proposed amendments would be consistent with the Compatibility Plan as long as the underlying development's intensity is consistent with the compatibility criteria.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, places of assembly (including but not limited to places of worship and theaters)
 - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.

- (g) Other Hazards to flight.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property, and be recorded as a deed notice.
 4. The project has been conditioned to utilize underground detention systems, which shall not contain surface water or attract wildlife. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated to construct 2 industrial buildings with mezzanines on separate parcels totaling 1,820,000 square feet. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

X:\AIRPORT CASE FILES\March\ZAP1515MA22\ZAP1515MA22sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.

**THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND**

NOT TO ATTRACT BIRDS

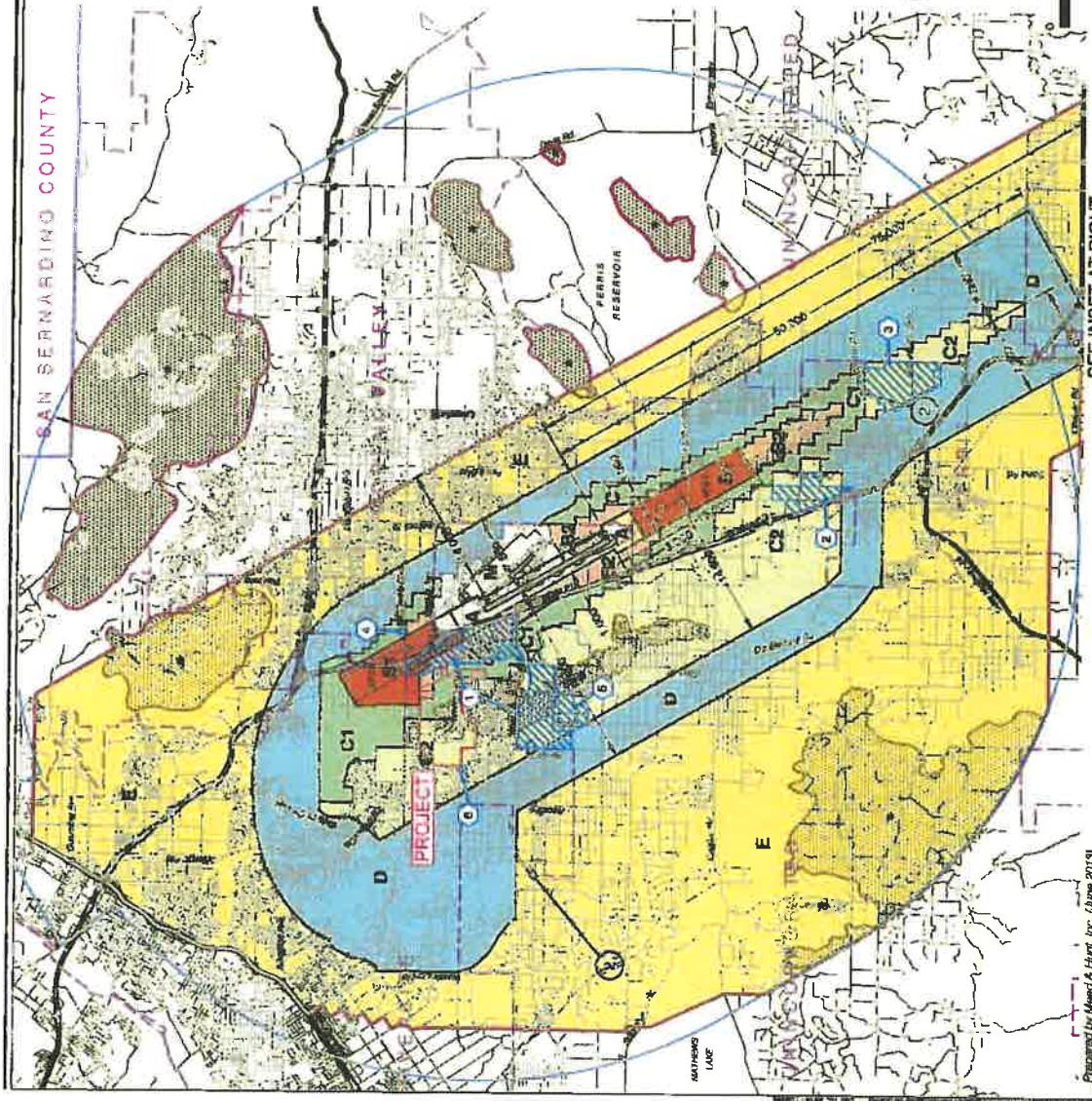
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

- Compatibility Zones**
- Zone A
 - Zone B1
 - Zone B2
 - Zone C1
 - Zone C2
 - Zone D
 - Zone E
 - Zone M
- High Terrain Zone**
 FAR Part 77 Military Outer Horizontal Surface Limits
 FAR Part 77 Notification Area
- Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.**
- Point at which departing aircraft typically reach 3,000 feet above runway end.**

Boundary Lines

- March Air Reserve Base / Air Force Property
 - March Joint Powers Authority Property Line
 - County Boundary
 - City Limits
 - Site-Specific Exemption (existing local agency commitments to development projects)
- ① March JPA, March Address Conversion/Mapsheet
 ② Ferris, Harvest Landing
 ③ Ferris, Park West
 ④ Moreno Valley, Affordable Housing
 ⑤ March JPA, Ben Clark Trailing Center
 ⑥ Riverside, Major East Subdivision

INSET



Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)

NOTE:
 All dimensions are measured from runway ends and centerlines.

Scale: 0 1 2 MILES

Base map source: County of Riverside 2013

Map MA-1

Compatibility Map
 March Air Reserve Base / Inland Port Airport

Prepared by: Mead & Hunt, Inc. (June 2013)

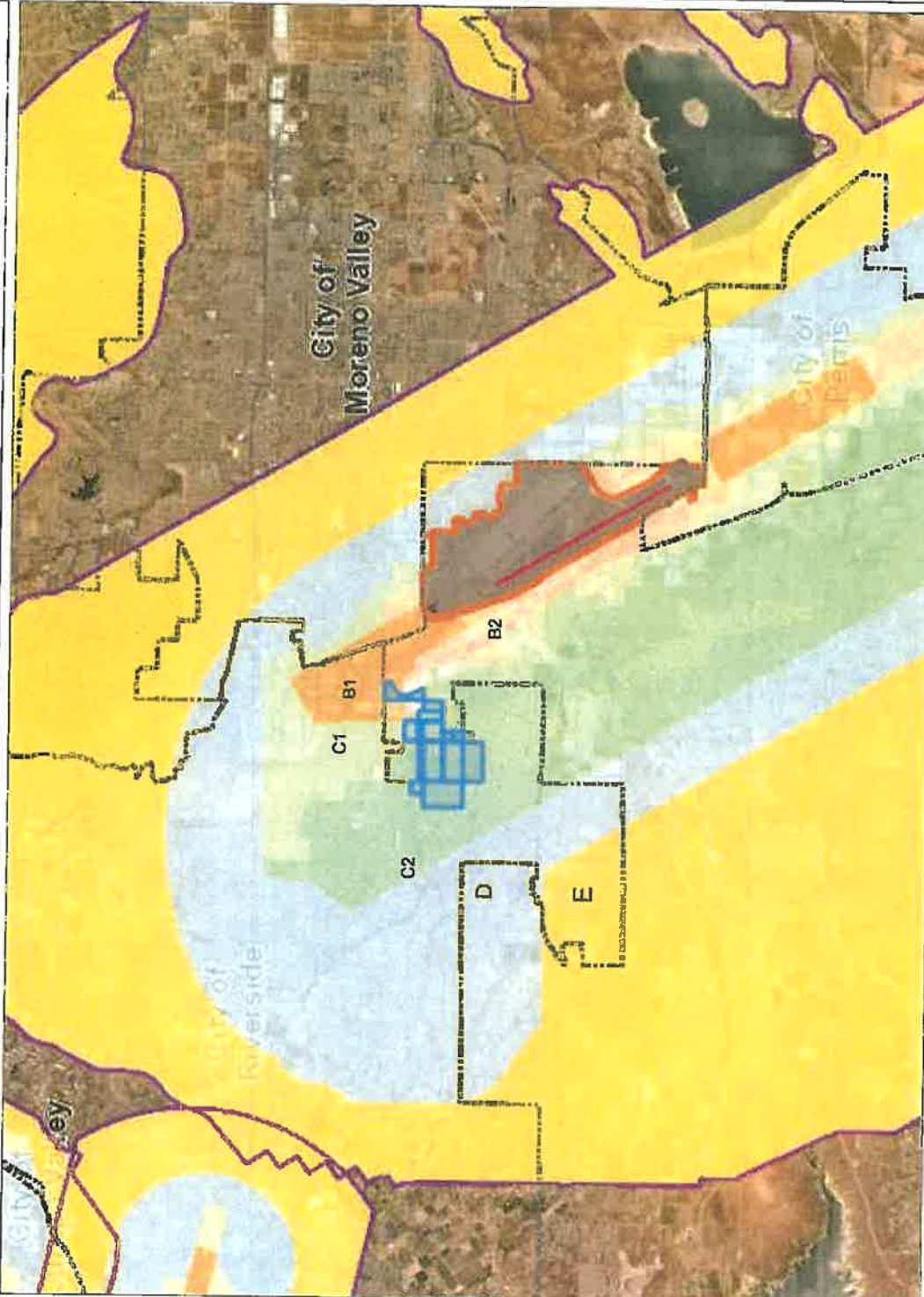
Map My County Map



Legend



- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- OTHER COMPATIBILITY ZONE

A	A-EXC1
B1	B1-APZ I
	B1-APZ I-EXC1
	B1-APZ II
	B1-APZ II-EXC1
	B1-EXC1
B2	B2-EXC1
C	C
C1	C1-EXC1
	C1-EXC3
	C1-EXC4
C1-HIGHT	C1-HIGHT
C2	C2
	C2-EXC1
	C2-EXC2
	C2-EXC3
	C2-EXC5
	C2-EXC6



Notes

"IMPORTANT" Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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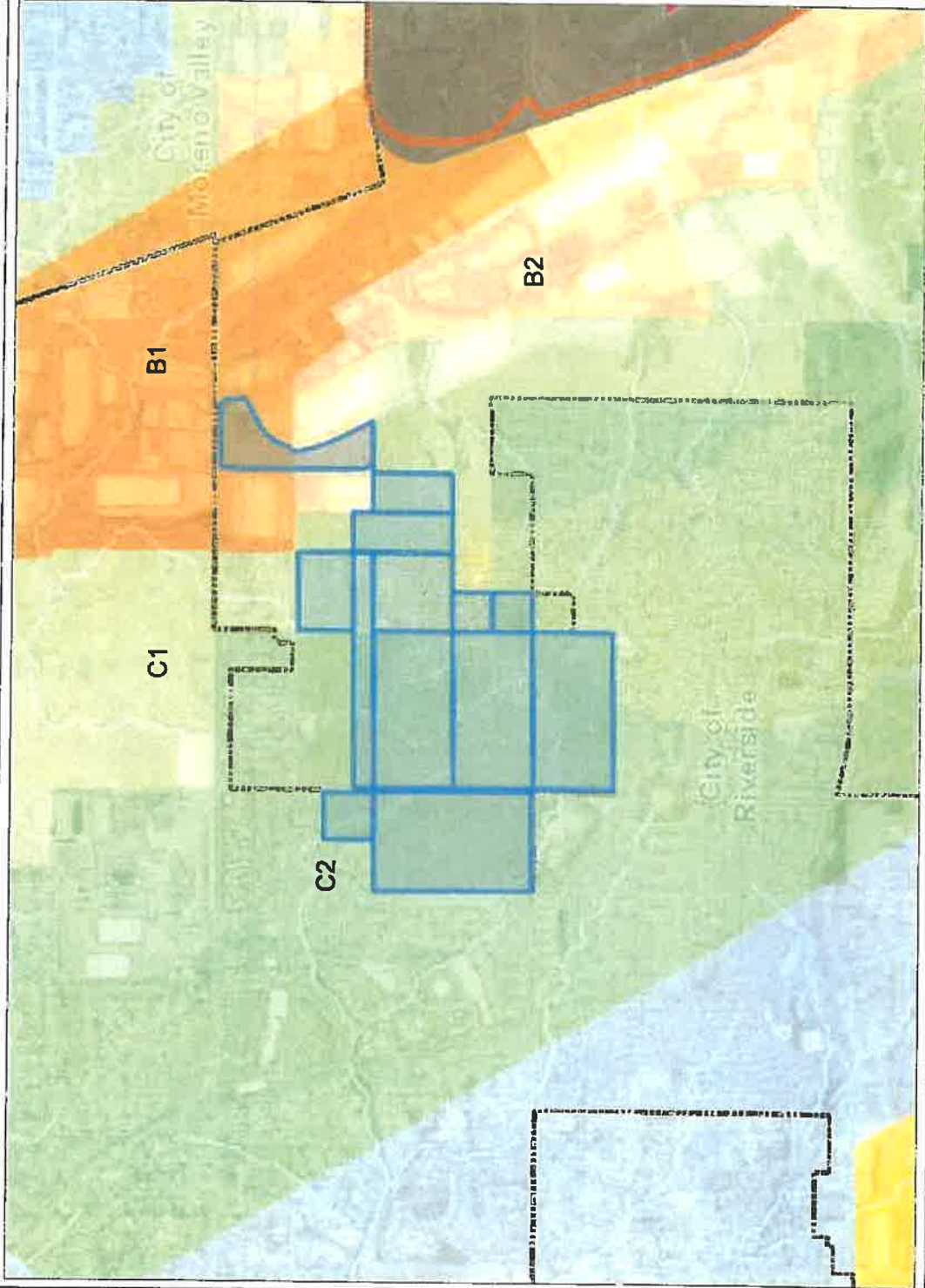
Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- OTHER COMPATIBILITY ZONE

A	A-EXC1
B1	B1
B1-APZ I	B1-APZ I
B1-APZ I-EXC1	B1-APZ I-EXC1
B1-APZ II	B1-APZ II
B1-APZ II-EXC1	B1-APZ II-EXC1
B1-EXC1	B1-EXC1
R2	R2
R2-EXC1	R2-EXC1
C	C
C1	C1
C1-EXC1	C1-EXC1
C1-EXC3	C1-EXC3
C1-EXC4	C1-EXC4
C1-HIGHT	C1-HIGHT
C2	C2
C2-EXC1	C2-EXC1
C2-EXC2	C2-EXC2
C2-EXC3	C2-EXC3
C2-EXC5	C2-EXC5
C2-EXC6	C2-EXC6



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6,157 Feet

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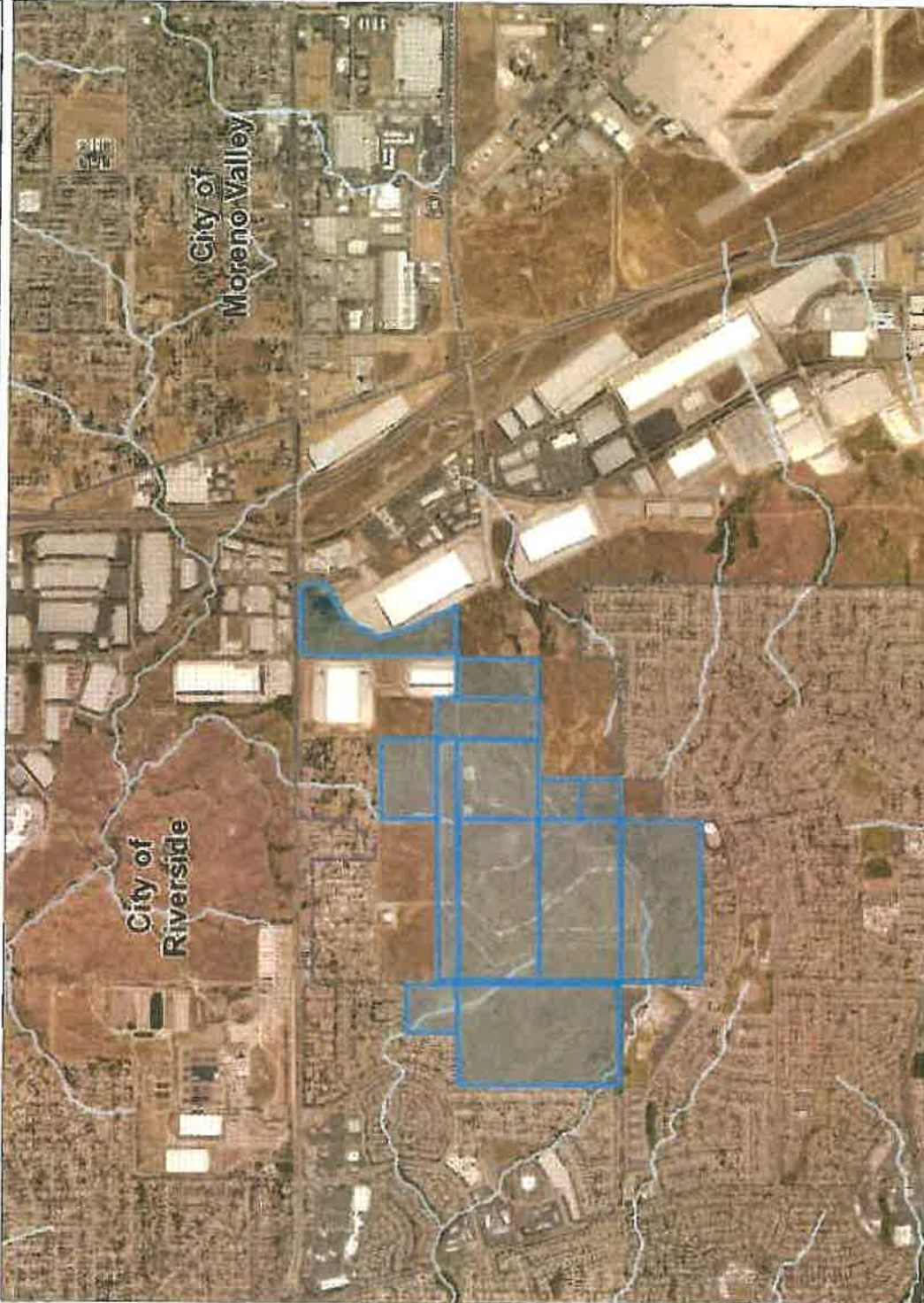
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Map My County Map



Legend

- Blue Line Streams
- City Areas
- World Street Map



Notes

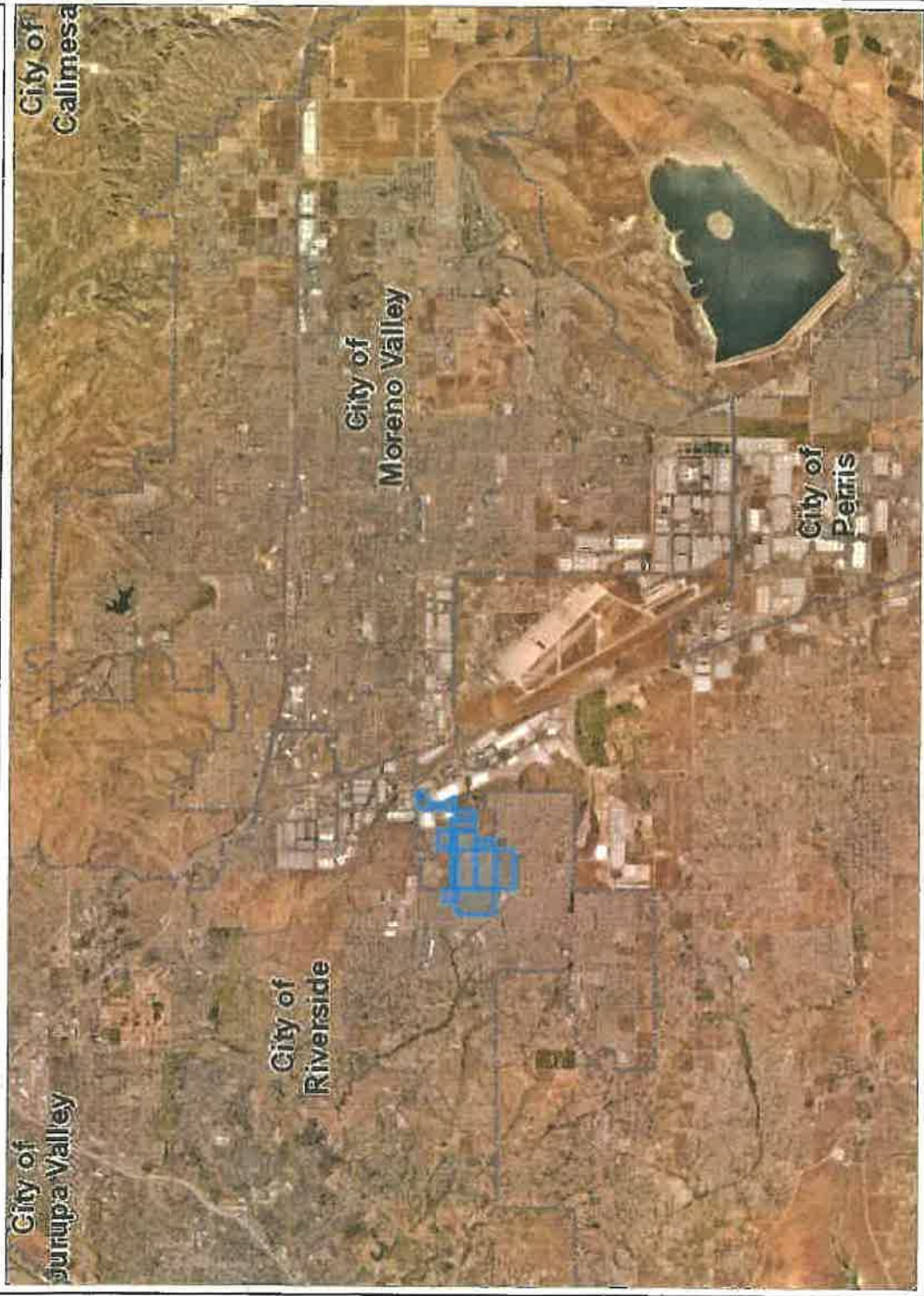
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Map My County Map



- Legend**
- City Areas
 - World Street Map

Notes

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Map My County Map



Legend

-  Blue Line Streams
-  City Areas
-  World Street Map



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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blue-line Streams
- City Areas
- World Street Map

Notes

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LAND USE LEGEND

MIXED USE	42.27 AC
BUSINESS PARK	65.35 AC
INDUSTRIAL	143.48 AC
STREETS	37.7 AC
PUBLIC FACILITIES:	2.04 AC
PARK	17.04 AC
OPEN SPACE:	17.04 AC
NET DEVELOPABLE	365.55 AC
PUBLIC FACILITY - EXISTING	2.87 AC
OPEN SPACE- CONSERVATION	446.05 AC
GROSS ACREAGE	817.50 AC

1-218

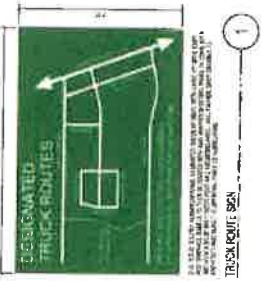


MERIDIAN
WEST CAMPUS
UPPER PLATEAU
 MERIDIAN PARK LLC

LAND USE PLAN

RGA

NO. 10017167
 DATE: 05/11/11
 PROJECT NAME: WEST CAMPUS
 SHEET NO. 1-218



STANDARD NOTES

1. ALL DIMENSIONS ARE SHOWN UNLESS OTHERWISE NOTED.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AUTHORITIES.

3. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AND UTILITIES AT ALL TIMES.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES AND STRUCTURES.

5. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AND EROSION CONTROL MEASURES THROUGHOUT CONSTRUCTION.

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES AND UTILITIES.

7. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AND EROSION CONTROL MEASURES THROUGHOUT CONSTRUCTION.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES AND UTILITIES.

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18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES AND UTILITIES.

19. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AND EROSION CONTROL MEASURES THROUGHOUT CONSTRUCTION.

20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES AND UTILITIES.

SHEET INDEX

NO.	DESCRIPTION
1	GENERAL NOTES
2	PROPOSED BUILDING B
3	TRUCK ROUTES
4	TRUCK ROUTE SIGN
5	UTILITY PROVISIONS
6	GENERAL PROJECT INFORMATION
7	UTILITY PROVISIONS
8	GENERAL PROJECT INFORMATION
9	UTILITY PROVISIONS
10	GENERAL PROJECT INFORMATION
11	UTILITY PROVISIONS
12	GENERAL PROJECT INFORMATION
13	UTILITY PROVISIONS
14	GENERAL PROJECT INFORMATION
15	UTILITY PROVISIONS
16	GENERAL PROJECT INFORMATION
17	UTILITY PROVISIONS
18	GENERAL PROJECT INFORMATION
19	UTILITY PROVISIONS
20	GENERAL PROJECT INFORMATION

PROJECT DATA

PROJECT NAME: UPPER PLATEAU - BUILDING B - PLOT PLAN

CLIENT: MERIDIAN PARK LLC

DATE: 11/15/2023

DRAWN BY: [Name]

CHECKED BY: [Name]

SCALE: AS SHOWN

PROJECT NO: A1-1-P

DATE: 11/15/2023

PROJECT LOCATION: [Address]

PROJECT OWNER: MERIDIAN PARK LLC

PROJECT MANAGER: [Name]

PROJECT ARCHITECT: [Name]

PROJECT ENGINEER: [Name]

PROJECT SURVEYOR: [Name]

PROJECT LANDSCAPE ARCHITECT: [Name]

PROJECT CIVIL ENGINEER: [Name]

PROJECT ELECTRICAL ENGINEER: [Name]

PROJECT MECHANICAL ENGINEER: [Name]

PROJECT PLUMBING ENGINEER: [Name]

PROJECT STRUCTURAL ENGINEER: [Name]

PROJECT ENVIRONMENTAL ENGINEER: [Name]

PROJECT GEOTECHNICAL ENGINEER: [Name]

PROJECT TRANSPORTATION ENGINEER: [Name]

PROJECT MARINE ENGINEER: [Name]

PROJECT AERONAUTICAL ENGINEER: [Name]

PROJECT CHEMICAL ENGINEER: [Name]

PROJECT METALLURGICAL ENGINEER: [Name]

PROJECT INDUSTRIAL ENGINEER: [Name]

PROJECT AGRICULTURAL ENGINEER: [Name]

PROJECT BIOMEDICAL ENGINEER: [Name]

PROJECT BIOMEDICAL EQUIPMENT ENGINEER: [Name]

PROJECT BIOMEDICAL ELECTRONICS ENGINEER: [Name]

PROJECT BIOMEDICAL MATERIALS ENGINEER: [Name]

PROJECT BIOMEDICAL MANUFACTURING ENGINEER: [Name]

PROJECT BIOMEDICAL DESIGN ENGINEER: [Name]

PROJECT BIOMEDICAL TESTING ENGINEER: [Name]

PROJECT BIOMEDICAL QUALITY ENGINEER: [Name]

PROJECT BIOMEDICAL REGULATORY ENGINEER: [Name]

PROJECT BIOMEDICAL SALES ENGINEER: [Name]

PROJECT BIOMEDICAL SUPPORT ENGINEER: [Name]

PROJECT BIOMEDICAL TRAINING ENGINEER: [Name]

PROJECT BIOMEDICAL RESEARCH ENGINEER: [Name]

PROJECT BIOMEDICAL DEVELOPMENT ENGINEER: [Name]

PROJECT BIOMEDICAL INNOVATION ENGINEER: [Name]

PROJECT BIOMEDICAL ENTREPRENEUR ENGINEER: [Name]

PROJECT BIOMEDICAL LEADER ENGINEER: [Name]

PROJECT BIOMEDICAL TEAM ENGINEER: [Name]

PROJECT BIOMEDICAL COMMUNITY ENGINEER: [Name]

PROJECT BIOMEDICAL SOCIETY ENGINEER: [Name]

PROJECT BIOMEDICAL ORGANIZATION ENGINEER: [Name]

PROJECT BIOMEDICAL INDUSTRY ENGINEER: [Name]

PROJECT BIOMEDICAL ACADEMY ENGINEER: [Name]

PROJECT BIOMEDICAL ASSOCIATION ENGINEER: [Name]

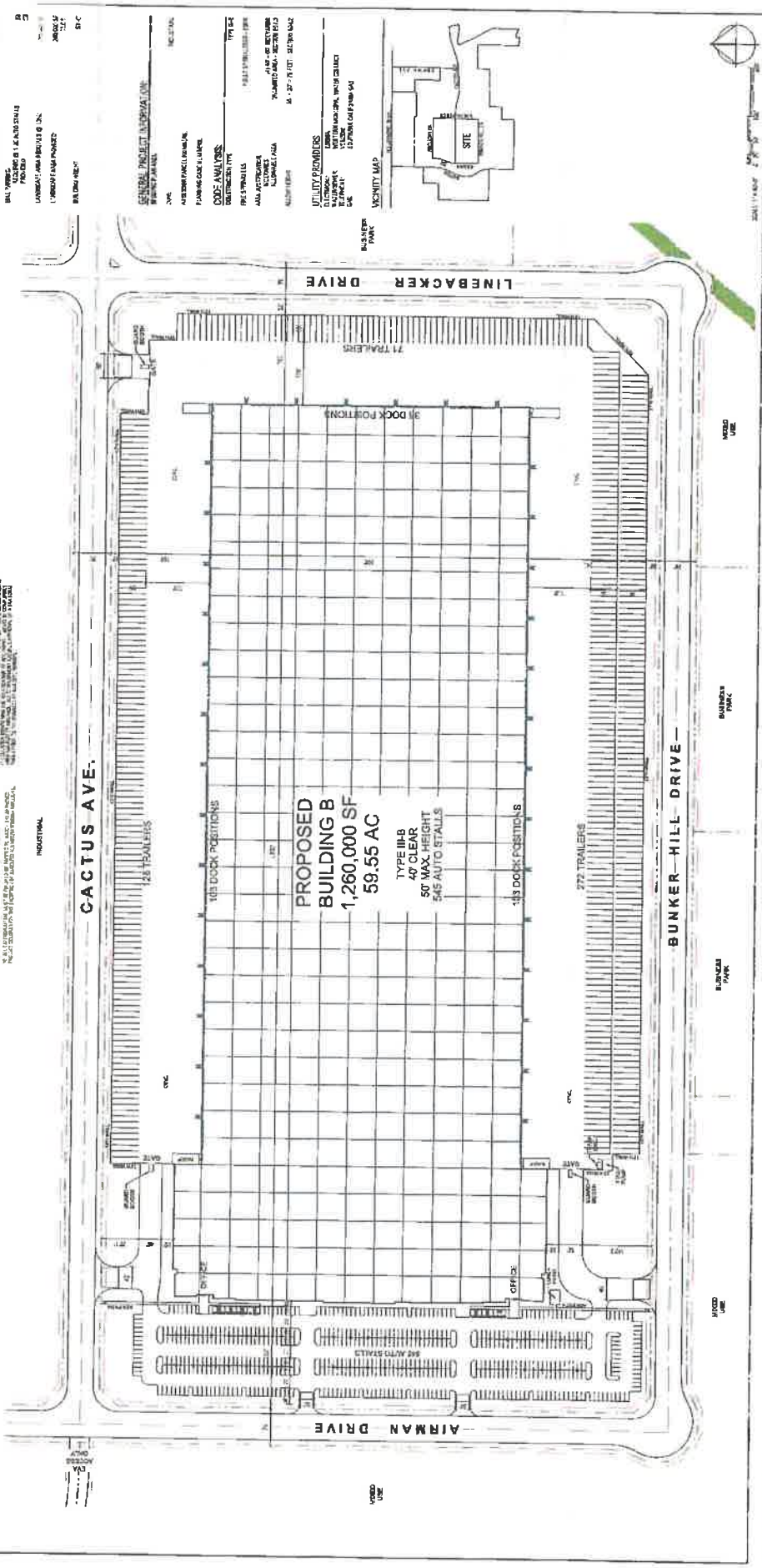
PROJECT BIOMEDICAL SOCIETY ENGINEER: [Name]

PROJECT BIOMEDICAL ORGANIZATION ENGINEER: [Name]

PROJECT BIOMEDICAL INDUSTRY ENGINEER: [Name]

PROJECT BIOMEDICAL ACADEMY ENGINEER: [Name]

PROJECT BIOMEDICAL ASSOCIATION ENGINEER: [Name]



MERIDIAN

UPPER PLATEAU - BUILDING B - PLOT PLAN

MERIDIAN PARK LLC

A1-1-P

NO.	DATE	DESCRIPTION
1	11/15/2023	ISSUED FOR PERMIT
2		
3		
4		
5		
6		
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FINISH SCHEDULE:

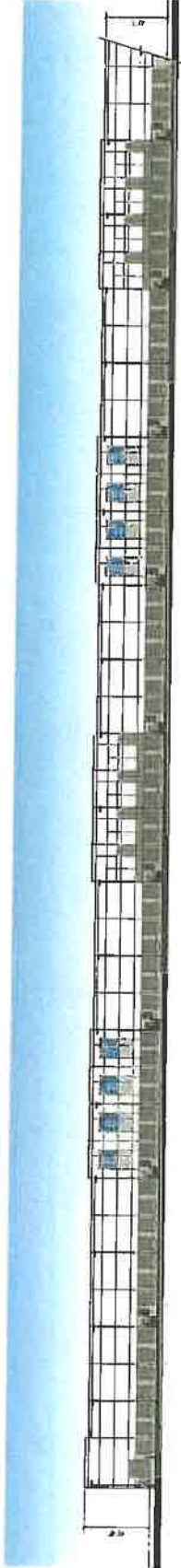
P-1	PAINTED CONCRETE (REDUCED COLOR)
P-2	PAINTED CONCRETE (MEDIUM COLOR)
P-3	PAINTED CONCRETE (DARKER COLOR)
P-4	PAINTED CONCRETE (DARKEST COLOR)
P-5	PAINTED CONCRETE (MATCH EXISTING)
C-1	GLASS PANELS WITH ALUMINUM FRAME
C-2	GLASS PANELS WITH ALUMINUM FRAME (MATCH EXISTING)

NOTES:

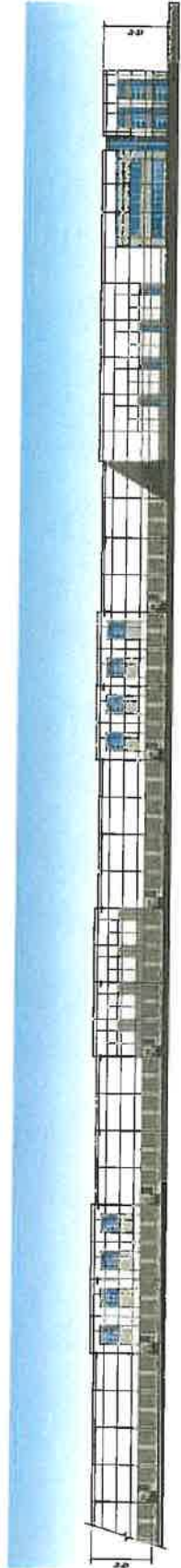
1. ALL NOTED AREA COMPONENT SHALL BE REFERRED FROM INT.



WEST ELEVATION
1" = 30'-0"



PARTIAL NORTH ELEVATION (SOUTH ELEVATION SIM.)
1" = 30'-0"



PARTIAL NORTH ELEVATION (SOUTH ELEVATION SIM.)
1" = 30'-0"



EAST ELEVATION
1" = 30'-0"

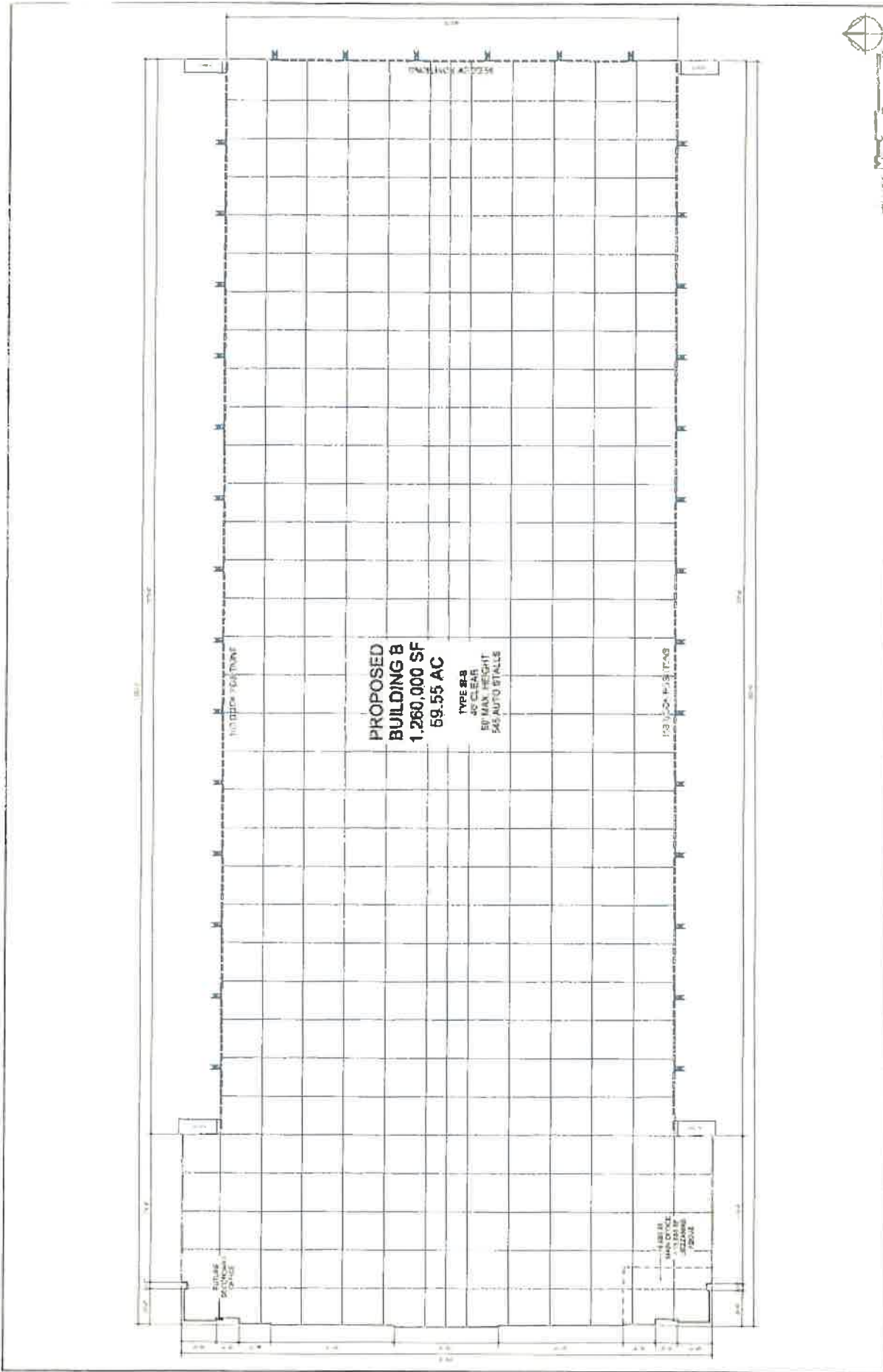


MERIDIAN

WEST CAMPUS UPPER PLATEAU - BUILDING B - ELEVATIONS
MERIDIAN PARK LLC

DATE:	11/11/2020
DESIGNER:	ALAN WALKER
CHECKER:	ALAN WALKER
SCALE:	AS SHOWN
PROJECT:	WEST CAMPUS UPPER PLATEAU - BUILDING B
SHEET:	A3-1

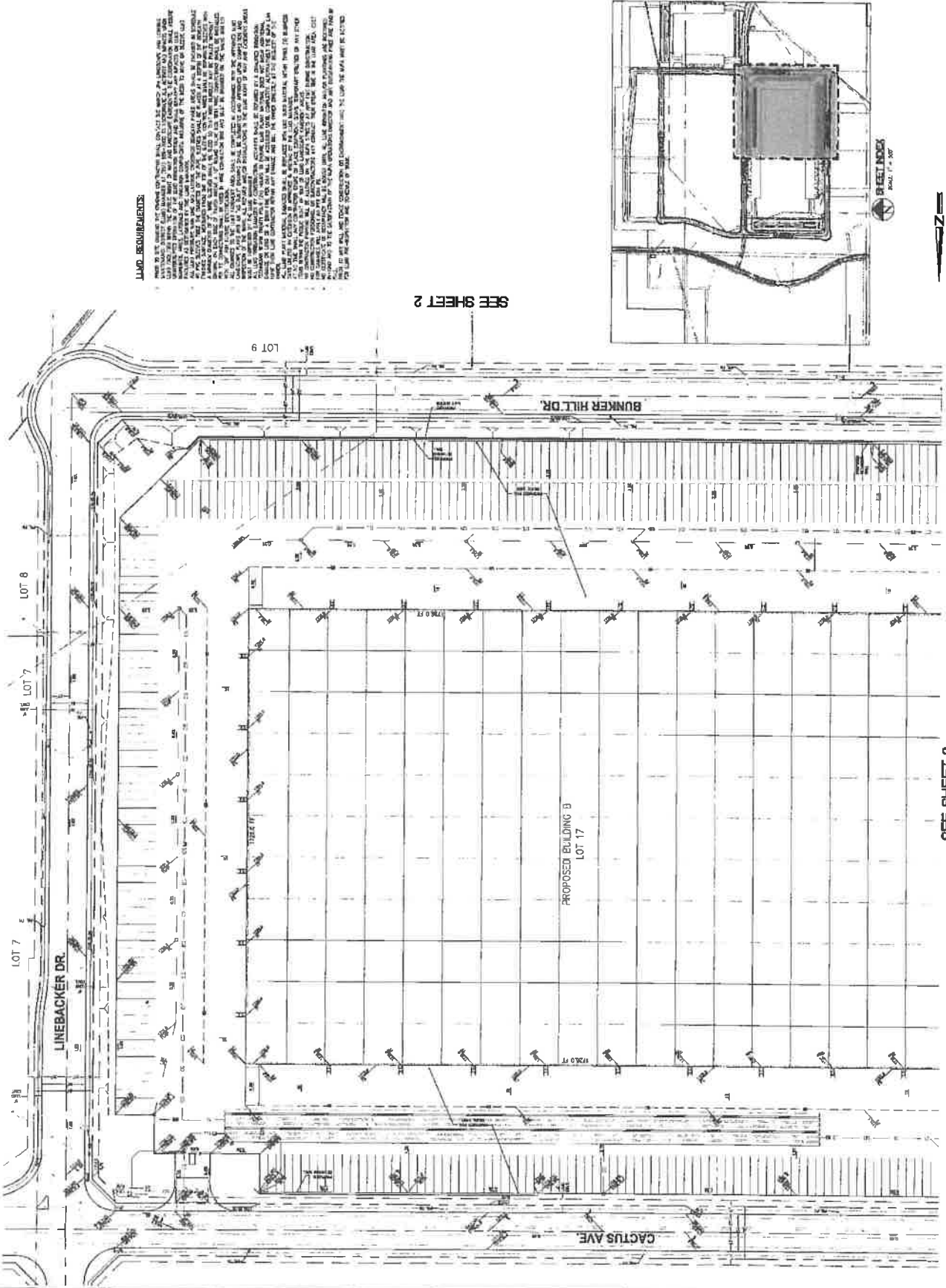
A3-1



MERIDIAN
UPPER PLATEAU - BUILDING B - FLOOR PLAN
MERIDIAN PARK LLC

NO.	DATE	REVISIONS	BY	CHKD

A2-1-P



LAND REQUIREMENTS

PLEASE REFER TO THE GENERAL NOTES ON SHEET 17 FOR THE REQUIREMENTS FOR THE PROJECT. THE GENERAL NOTES ON SHEET 17 SHALL BE READ IN CONJUNCTION WITH THE GENERAL NOTES ON SHEET 18, 19, AND 20. THE REQUIREMENTS FOR THE PROJECT ARE AS FOLLOWS:

1. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED ON LOT 17, WHICH IS A 1.00 ACRE PARCEL. THE BUILDING SHALL BE A THREE-STORY STRUCTURE WITH A TOTAL FLOOR AREA OF 100,000 SQUARE FEET. THE BUILDING SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE 2022 CALIFORNIA BUILDING CODE AND THE 2022 CALIFORNIA PLUMBING CODE.

2. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 10 FEET FROM ALL SIDES OF LOT 17. THE BUILDING SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 20 FEET FROM THE FRONT AND REAR BOUNDARIES OF LOT 17.

3. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 5 FEET FROM THE SIDE BOUNDARIES OF LOT 17.

4. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 10 FEET FROM THE FRONT AND REAR BOUNDARIES OF LOT 17.

5. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 5 FEET FROM THE SIDE BOUNDARIES OF LOT 17.

6. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 10 FEET FROM THE FRONT AND REAR BOUNDARIES OF LOT 17.

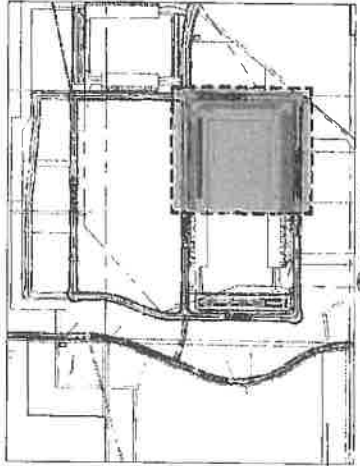
7. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 5 FEET FROM THE SIDE BOUNDARIES OF LOT 17.

8. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 10 FEET FROM THE FRONT AND REAR BOUNDARIES OF LOT 17.

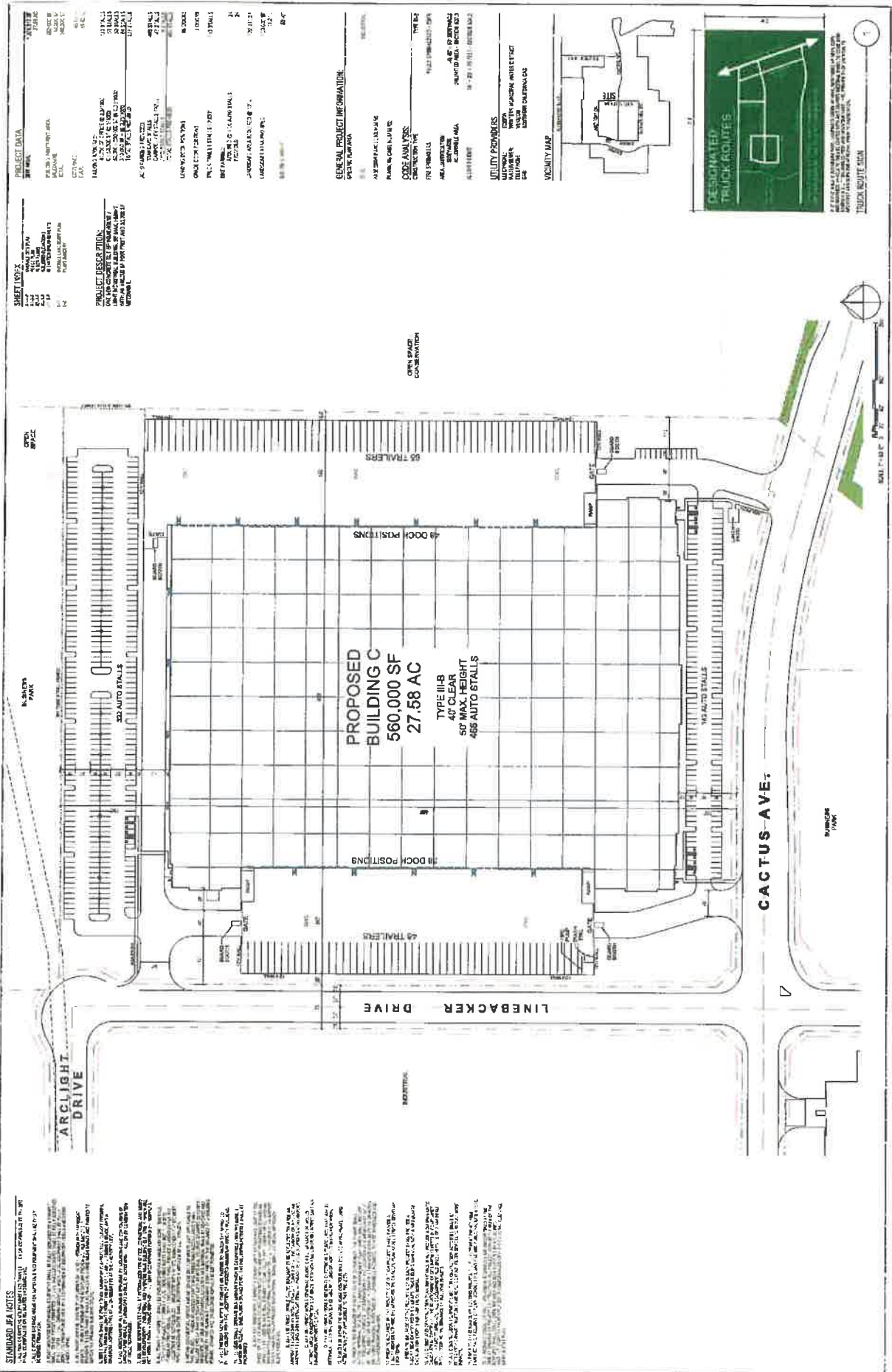
9. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 5 FEET FROM THE SIDE BOUNDARIES OF LOT 17.

10. THE PROPOSED BUILDING B SHALL BE CONSTRUCTED WITH A MINIMUM SETBACK OF 10 FEET FROM THE FRONT AND REAR BOUNDARIES OF LOT 17.

SEE SHEET 2



SEE SHEET 2



PROJECT DATA

PROJECT NAME: ...
 CLIENT: ...
 PROJECT LOCATION: ...
 PROJECT NUMBER: ...
 SHEET NUMBER: ...

PROJECT DESCRIPTION:
 THIS PROJECT IS A PROPOSED ...
 THE BUILDING WILL BE USED FOR ...
 THE TOTAL AREA OF THE BUILDING IS ...

GENERAL PROJECT INFORMATION:

DATE: ...
 DRAWN BY: ...
 CHECKED BY: ...
 APPROVED BY: ...

GENERAL PROJECT INFORMATION:

PROJECT NUMBER: ...
 SHEET NUMBER: ...
 DATE: ...

VELOCITY PROMOTERS

...
 ...
 ...



TRUCK ROUTE SIGN

...
 ...
 ...

DATE	DESCRIPTION

NO.	DATE	BY	REVISION

STANDARD JPA NOTES

1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
 2. ALL DIMENSIONS ARE TO BE VERIFIED BY THE CONTRACTOR.
 3. ALL DIMENSIONS ARE TO BE VERIFIED BY THE CONTRACTOR.
 4. ALL DIMENSIONS ARE TO BE VERIFIED BY THE CONTRACTOR.

VELOCITY PROMOTERS

...
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VELOCITY PROMOTERS

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VELOCITY PROMOTERS

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VELOCITY PROMOTERS

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VELOCITY PROMOTERS

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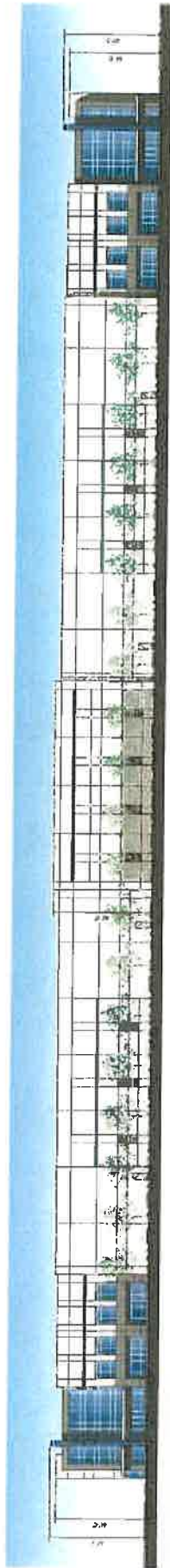
RG

MERIDIAN

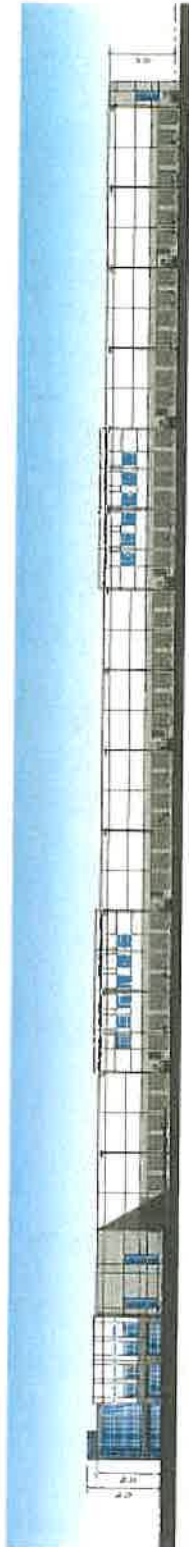
UPPER PLATEAU - BUILDING C - PLOT PLAN

MERIDIAN PARK, LLC

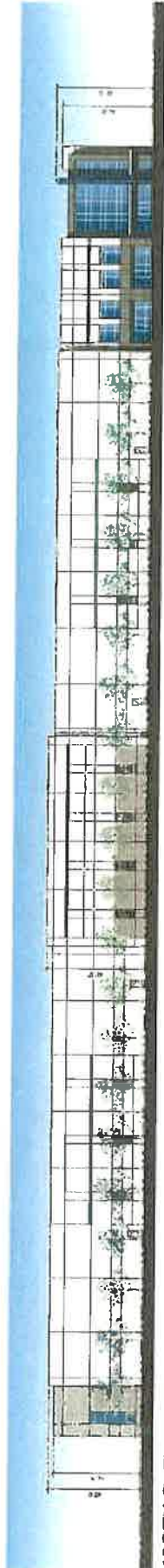
A11-P



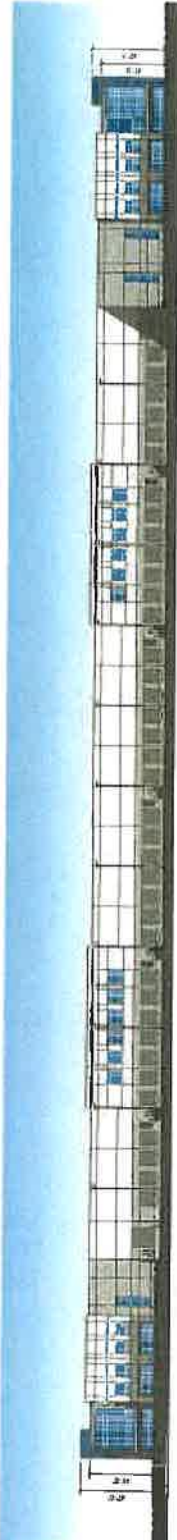
SOUTH ELEVATION
1" = 30'-0"



EAST ELEVATION
1" = 30'-0"



NORTH ELEVATION
1" = 30'-0"



WEST ELEVATION
1" = 30'-0"

FINISH SCHEDULE:

- 1 PAINTED CONCRETE FLOOR DECK
SP 105 - CONCRETE
- 2 PAINTED CONCRETE LIGHT ACCENT COLOR
SP 105 - CONCRETE
- 3 PAINTED CONCRETE DARK ACCENT COLOR
SP 105 - CONCRETE
- 4 PAINTED CONCRETE ACCENT COLOR
SP 105 - CONCRETE
- 5 PAINTED CONCRETE ACCENT COLOR
SP 105 - CONCRETE
- 6 PAINTED CONCRETE ACCENT COLOR
SP 105 - CONCRETE
- 7 PAINTED CONCRETE ACCENT COLOR
SP 105 - CONCRETE

NOTES:

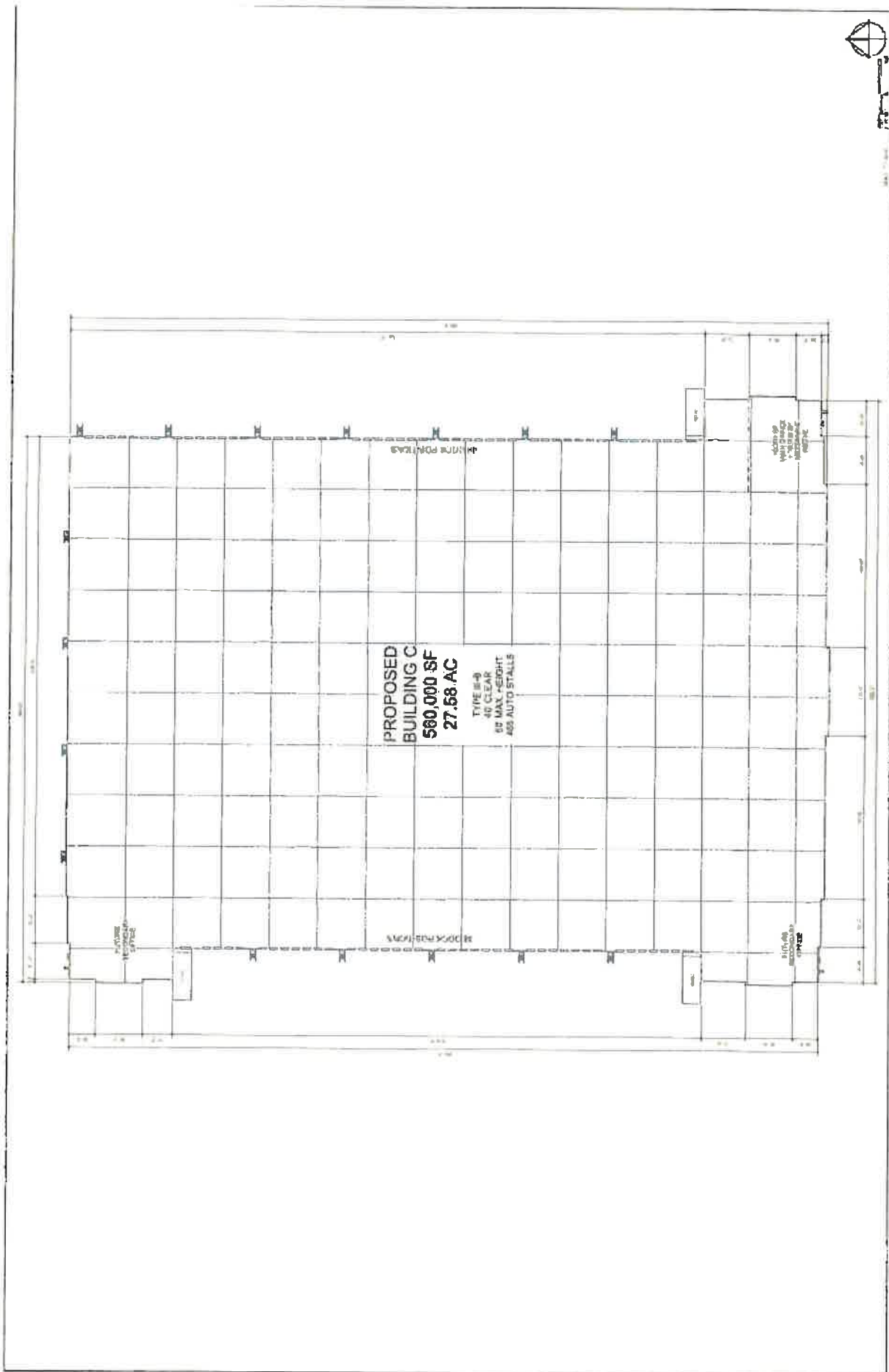
* ALL ROOFING AND CLADDING SHALL BE INDICATED FROM VERTICAL




MERIDIAN
SOUTH CAMPUS - BUILDING C - ELEVATIONS
 MERIDIAN PARK LLC

REVISIONS		
NO.	DATE	DESCRIPTION

A3-1

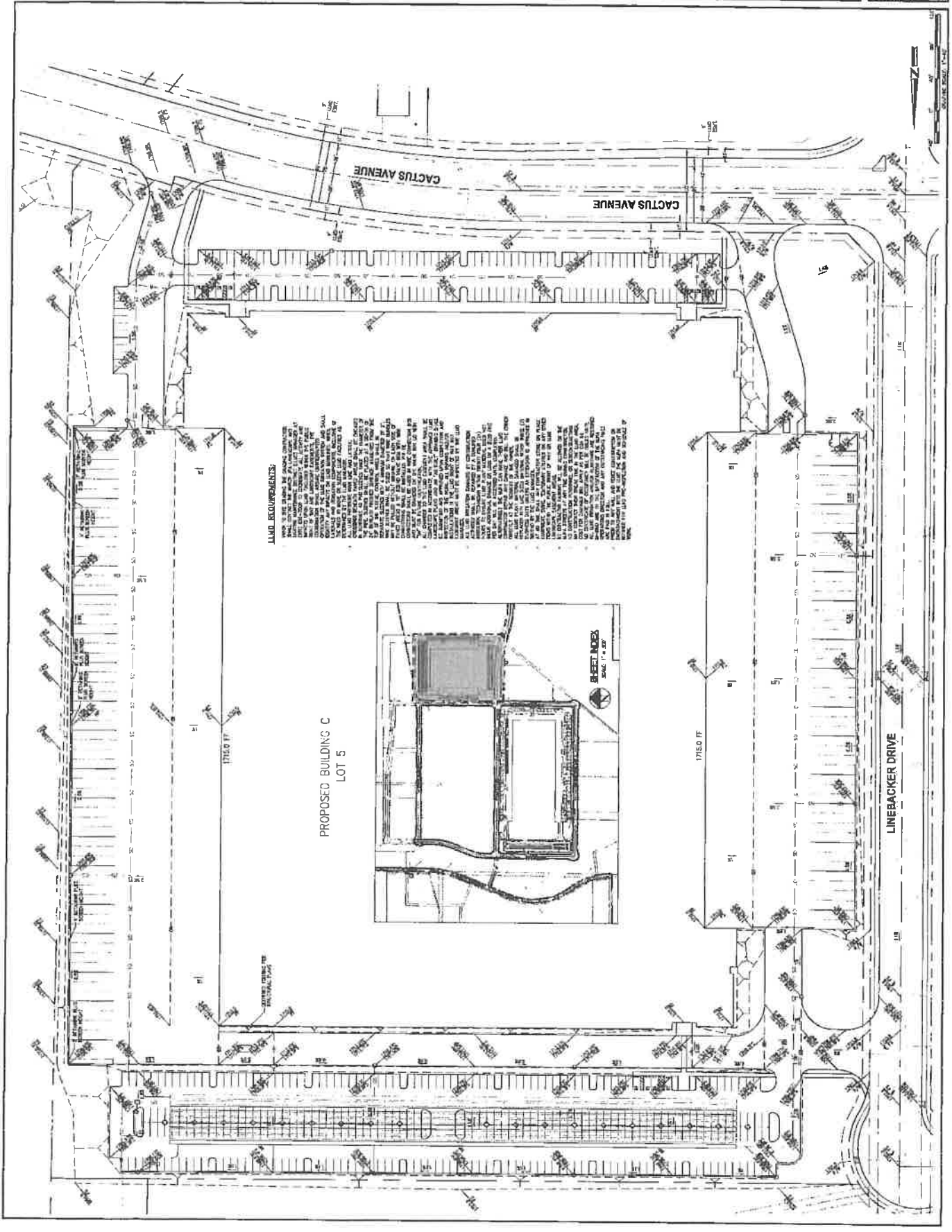


REVISIONS		A2-1-P
NO.	DATE	

MERIDIAN
 UPPER PLATEAU - BUILDING C - FLOOR PLAN
 MERIDIAN PARK LLC



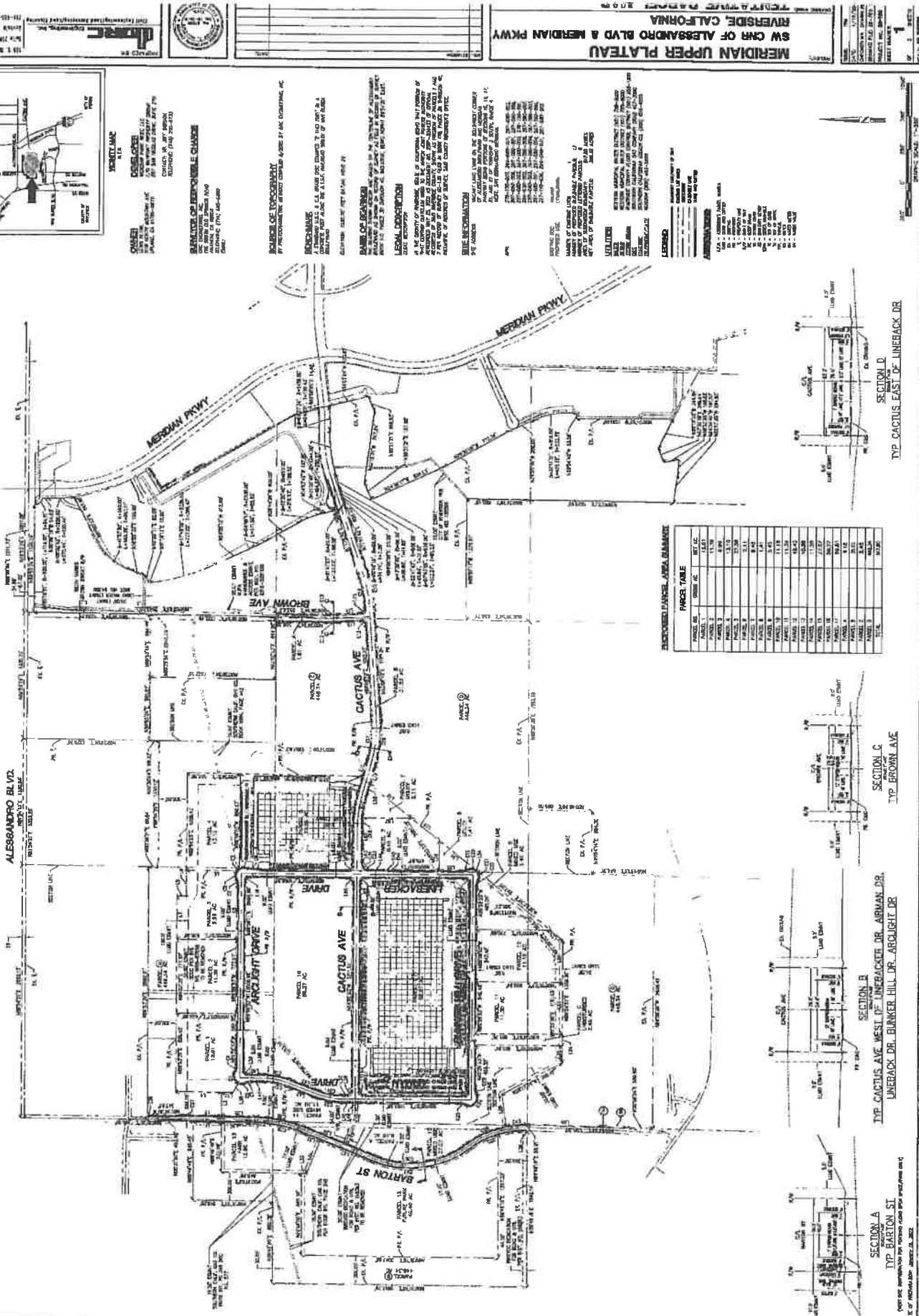
NO.	DATE



UTIL. REQUIREMENTS:
 THE UTILITIES SHOWN ON THIS PLAN ARE BASED ON THE RECORD DRAWINGS FOR THE EXISTING UTILITIES AND SHALL BE VERIFIED BY THE FIELD SURVEY AND SHALL BE SHOWN ON THE RECORD DRAWINGS. THE UTILITIES SHOWN ON THIS PLAN ARE BASED ON THE RECORD DRAWINGS FOR THE EXISTING UTILITIES AND SHALL BE VERIFIED BY THE FIELD SURVEY AND SHALL BE SHOWN ON THE RECORD DRAWINGS.

PROPOSED BUILDING C
 LOT 5

TENTATIVE PARCEL MAP No. 38063



OWNER
 DORIS J. WILSON
 103 E. 24 Street
 Santa Ana, CA 92701
 (714) 241-1882

DESIGNER
 JAMES R. WILSON
 103 E. 24 Street
 Santa Ana, CA 92701
 (714) 241-1882

REPLACEMENT OF DEPENDABLE CHANGE
 THE ABOVE IS A REVISION TO THE ORIGINAL PARCEL MAP NO. 38063 DATED 07/14/2009.

RANGE OF IMPROVEMENT
 THE IMPROVEMENTS ARE TO BE CONSIDERED AS PER THE CITY OF SANTA ANA, CALIFORNIA.

BENCHMARK
 THE BENCHMARK IS A 1/4" X 1/4" X 1/4" IRON PIN SET IN CONCRETE AT THE INTERSECTION OF CACTUS AVE AND BROWN AVE.

LEGAL DESCRIPTION
 A TRACT OF LAND IN THE CITY OF SANTA ANA, CALIFORNIA, BEING THE TRACT DESCRIBED AS FOLLOWS: A CERTAIN PORTION OF THE TRACT OF LAND DESCRIBED IN PARCEL MAP NO. 38063, DATED 07/14/2009, AS SHOWN ON SAID PARCEL MAP.

PREPARED FOR
 DORIS J. WILSON
 103 E. 24 Street
 Santa Ana, CA 92701
 (714) 241-1882

PREPARED BY
 JAMES R. WILSON
 103 E. 24 Street
 Santa Ana, CA 92701
 (714) 241-1882

REVISIONS

NO.	DATE	DESCRIPTION
1	07/14/2009	ORIGINAL PARCEL MAP
2	07/14/2009	REVISION TO CORRECT ACREAGE
3	07/14/2009	REVISION TO CORRECT LOT DIMENSIONS

SECTION A
 TYP BARTON ST
 LINERACK DR. BUNKER HILL DR. AIRMAN DR.

SECTION B
 TYP CACTUS AVE WEST OF LINERACK DR. AIRMAN DR.
 LINERACK DR. BUNKER HILL DR. AIRMAN DR.

SECTION C
 TYP BROWN AVE

SECTION D
 TYP CACTUS EAST OF LINERACK DR.

PROPOSED PARCEL AREA SUMMARY

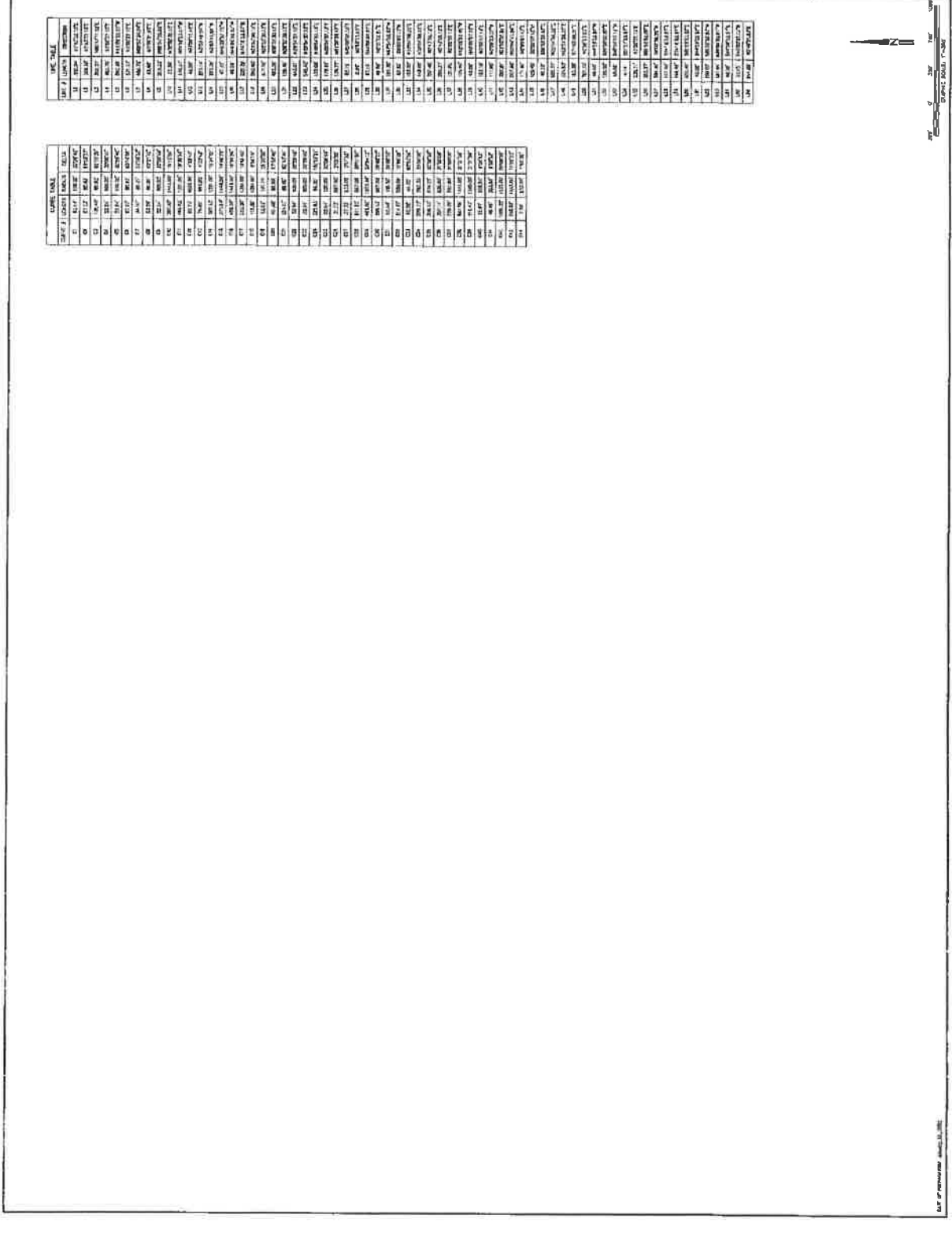
PARCEL NO.	ACRES	NET AC.
PARCEL 1	11.76	11.76
PARCEL 2	8.89	8.89
PARCEL 3	13.26	13.26
PARCEL 4	2.11	2.11
PARCEL 5	8.42	8.42
PARCEL 6	3.41	3.41
PARCEL 7	11.18	11.18
PARCEL 8	11.29	11.29
PARCEL 9	79.81	79.81
PARCEL 10	8.12	8.12
PARCEL 11	3.42	3.42
PARCEL 12	14.28	14.28
PARCEL 13	11.29	11.29
PARCEL 14	79.81	79.81
PARCEL 15	8.12	8.12
PARCEL 16	3.42	3.42
PARCEL 17	14.28	14.28
PARCEL 18	11.29	11.29
PARCEL 19	79.81	79.81
PARCEL 20	8.12	8.12
PARCEL 21	3.42	3.42
PARCEL 22	14.28	14.28
PARCEL 23	11.29	11.29
PARCEL 24	79.81	79.81
PARCEL 25	8.12	8.12
PARCEL 26	3.42	3.42
PARCEL 27	14.28	14.28
PARCEL 28	11.29	11.29
PARCEL 29	79.81	79.81
PARCEL 30	8.12	8.12
PARCEL 31	3.42	3.42
PARCEL 32	14.28	14.28
PARCEL 33	11.29	11.29
PARCEL 34	79.81	79.81
PARCEL 35	8.12	8.12
PARCEL 36	3.42	3.42
PARCEL 37	14.28	14.28
PARCEL 38	11.29	11.29
PARCEL 39	79.81	79.81
PARCEL 40	8.12	8.12
PARCEL 41	3.42	3.42
PARCEL 42	14.28	14.28
PARCEL 43	11.29	11.29
PARCEL 44	79.81	79.81
PARCEL 45	8.12	8.12
PARCEL 46	3.42	3.42
PARCEL 47	14.28	14.28
PARCEL 48	11.29	11.29
PARCEL 49	79.81	79.81
PARCEL 50	8.12	8.12
PARCEL 51	3.42	3.42
PARCEL 52	14.28	14.28
PARCEL 53	11.29	11.29
PARCEL 54	79.81	79.81
PARCEL 55	8.12	8.12
PARCEL 56	3.42	3.42
PARCEL 57	14.28	14.28
PARCEL 58	11.29	11.29
PARCEL 59	79.81	79.81
PARCEL 60	8.12	8.12
PARCEL 61	3.42	3.42
PARCEL 62	14.28	14.28
PARCEL 63	11.29	11.29
PARCEL 64	79.81	79.81
PARCEL 65	8.12	8.12
PARCEL 66	3.42	3.42
PARCEL 67	14.28	14.28
PARCEL 68	11.29	11.29
PARCEL 69	79.81	79.81
PARCEL 70	8.12	8.12
PARCEL 71	3.42	3.42
PARCEL 72	14.28	14.28
PARCEL 73	11.29	11.29
PARCEL 74	79.81	79.81
PARCEL 75	8.12	8.12
PARCEL 76	3.42	3.42
PARCEL 77	14.28	14.28
PARCEL 78	11.29	11.29
PARCEL 79	79.81	79.81
PARCEL 80	8.12	8.12
PARCEL 81	3.42	3.42
PARCEL 82	14.28	14.28
PARCEL 83	11.29	11.29
PARCEL 84	79.81	79.81
PARCEL 85	8.12	8.12
PARCEL 86	3.42	3.42
PARCEL 87	14.28	14.28
PARCEL 88	11.29	11.29
PARCEL 89	79.81	79.81
PARCEL 90	8.12	8.12
PARCEL 91	3.42	3.42
PARCEL 92	14.28	14.28
PARCEL 93	11.29	11.29
PARCEL 94	79.81	79.81
PARCEL 95	8.12	8.12
PARCEL 96	3.42	3.42
PARCEL 97	14.28	14.28
PARCEL 98	11.29	11.29
PARCEL 99	79.81	79.81
PARCEL 100	8.12	8.12

103 E. 24 Street Santa Ana, CA 92701 (714) 241-1882

SW COR OF ALESSANDRO BLVD & MERIDIAN PKWY
RIVERSIDE, CALIFORNIA
TENTATIVE PARCEL MAP



197 5 81 1000 600
101-10-1001
Map of Riverside, California, showing the location of the proposed parcel map.



PARCEL	GRID	NO.
1W1A1B1C1D1E1	1011	101
1W1A1B1C1D1E2	1012	102
1W1A1B1C1D1E3	1013	103
1W1A1B1C1D1E4	1014	104
1W1A1B1C1D1E5	1015	105
1W1A1B1C1D1E6	1016	106
1W1A1B1C1D1E7	1017	107
1W1A1B1C1D1E8	1018	108
1W1A1B1C1D1E9	1019	109
1W1A1B1C1D1E0	1020	110

GRID	NO.
1011	101
1012	102
1013	103
1014	104
1015	105
1016	106
1017	107
1018	108
1019	109
1020	110

The West Campus Upper Plateau Specific Plan attached to the RCALUC report was omitted due to the quality of the images. However, a separate copy of the Specific Plan is provided as attachment B.

Attachment #2

Draft West Campus Upper Plateau Specific Plan (2nd Draft), dated April 2022

WEST CAMPUS UPPER PLATEAU

SPECIFIC PLAN NO. XXX

Prepared for:
MARCH JOINT POWERS AUTHORITY

Developed by:
MERIDIAN PARK WEST, LLC

Prepared by:
T&B PLANNING, INC.



WEST CAMPUS UPPER PLATEAU

SPECIFIC PLAN NO. XXXX

Prepared for:

MARCH JOINT POWERS AUTHORITY
14205 Meridian Parkway, Suite 140
Riverside, CA 92518
(951) 656-7000

Developed by:

MERIDIAN PARK WEST, LLC
1156 N. Mountain Avenue
Upland, CA 91786
(909) 949-7593
Contact: Adam Collier

Prepared by:

T&B PLANNING, INC.
3200 El Camino Real, Suite 100
Irvine, CA 92602
(714) 505-6360
Contact: Les Johnson
JOB NUMBER: 1076-008

Second Draft: April, 2022

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ES EXECUTIVE SUMMARY

ES.1 PROJECT OVERVIEW

The West Campus Upper Plateau Specific Plan covers approximately 817.9 acres in the northwestern portion of the March Joint Powers Authority (MJPA). The Specific Plan property is located south of Alessandro Boulevard, west of Meridian Parkway, north of Grove Community Drive, and east of Trautwein Road. The City of Riverside surrounds the northern, western, and southern ends of the Specific Plan Area, along with some small County of Riverside "islands" north of the Specific Plan Area.

ON September 12, 2012, a Settlement Agreement was entered between and among the Center for Biological Diversity (CBD), the San Bernardino Valley Audubon Society, MJPA, and LNR Riverside LLC as the complete settlement of the claims and actions raised in *Center for Biological Diversity v. Jim Bartel, et al.* (CBD Settlement Agreement, MJPA 2012). The CBD Settlement Agreement contemplated the division of western acreage under the jurisdiction of the MJPA, including the Project site, into a Conservation Area, Developable Area, Proposed Park Area and Water Quality/Open Space Area.

The location of the West Campus Upper Plateau in regional and local contexts is depicted in *Figure ES-1, Regional Location Map*, which shows the relationship of the Specific Plan property with nearby cities, counties, and unincorporated communities. *Figure ES-2, Local Vicinity Map*, depicts the surrounding land use of the Specific Plan Area.

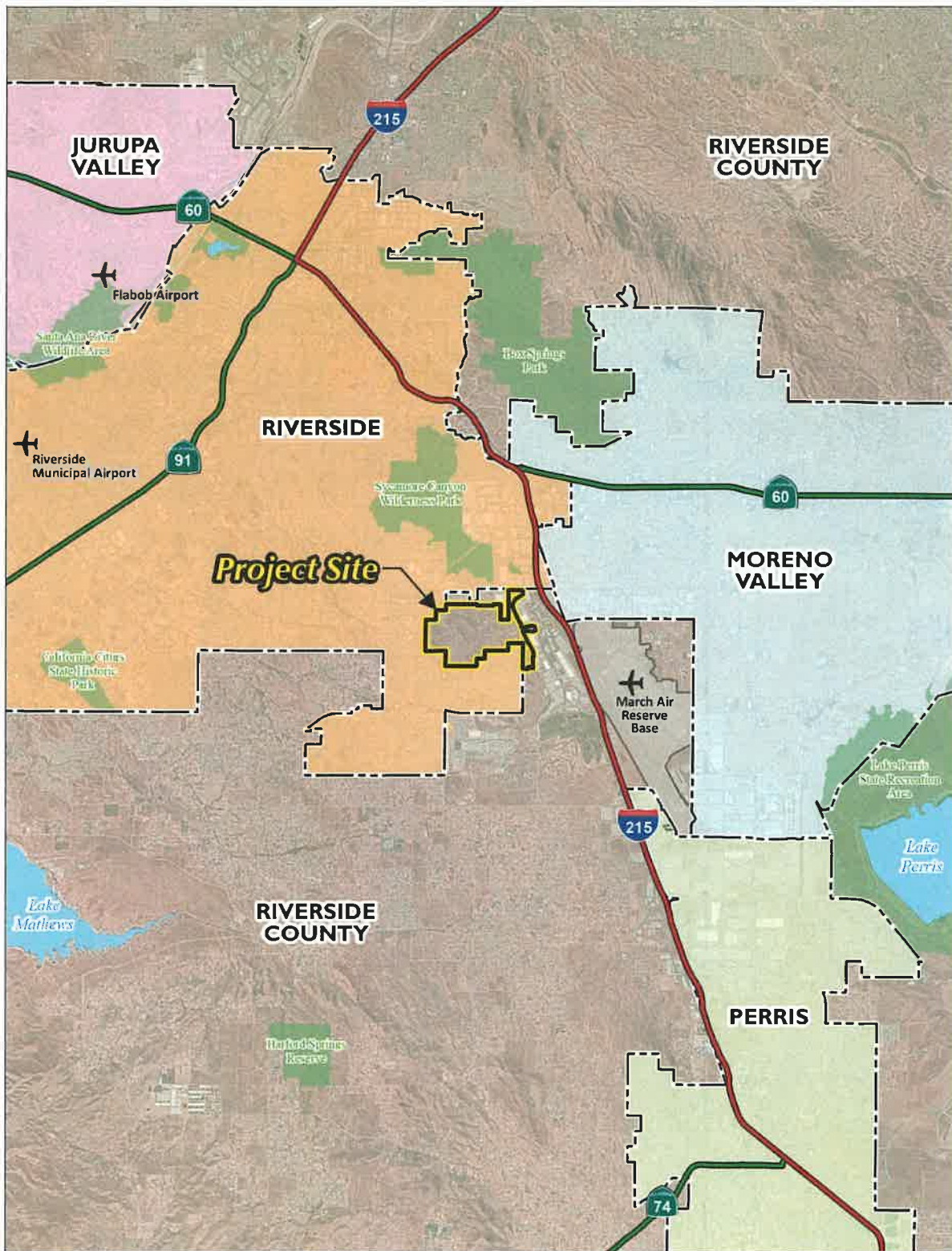
The information contained in this Specific Plan provides guidance for a development accommodating Business Park, Industrial, Mixed Used, Public Facilities, and Open Space land uses. The West Campus Upper Plateau Specific Plan is envisioned to contain industrial, business park, and non-residential mixed-use buildings supported by public roads and utility infrastructure systems, private driveways, parking lots, truck courts, lighting, landscaping, signage, and other functional and decorative features. Hiking and biking trails are provided within the Open Space surrounding the proposed development area to encourage recreational activities by surrounding residents, employees, and visitors.

The Business Park, Industrial, and Mixed Used land uses are surrounded by Open Space areas that provide a minimum 300' buffer for the nearby residents in the City of Riverside and County of Riverside. As designed, building users are expected to be a mixture of businesses that bring job opportunities and economic growth to the MJPA and the surrounding cities.

The following land use types specified in the MJPA General Plan would be developed as part of the West Campus Upper Plateau project:

- **Business Park (BP):** including administrative, financial, light manufacturing, and commercial services.
- **Industrial (IND):** including manufacturing, warehousing, e-commerce and associated uses.
- **Mixed Use (MU):** complementary uses, including commercial retail, office, research and development, industrial and others.

- **Park/Recreation/Open Space (P/R/OS):** primarily passive open spaces and recreational areas.
- **Public Facilities (PF):** range of public, quasi-public, and private uses such as public cultural and historical facilities, government facilities, public utilities, and major roads.

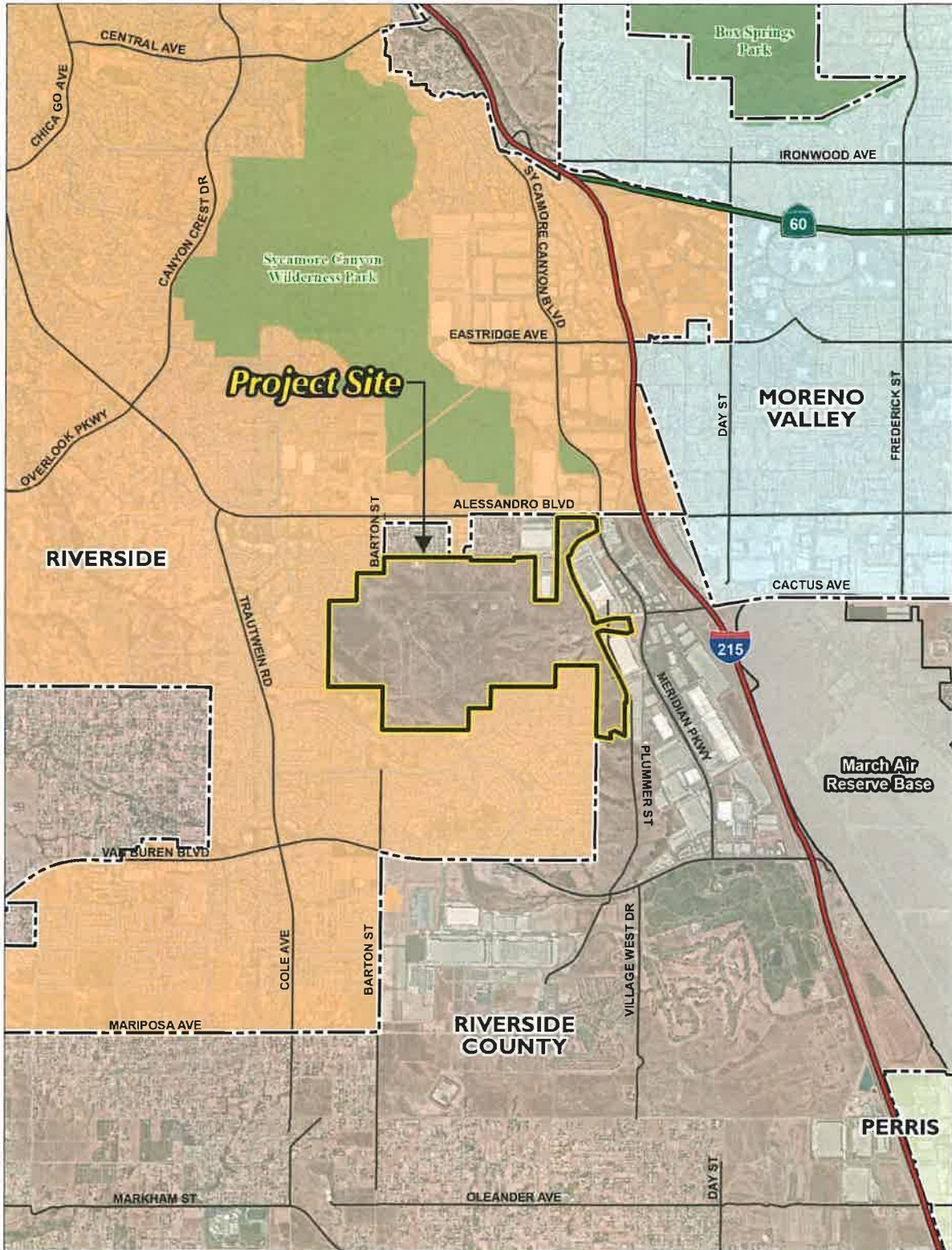


Source(s): ESRI, Nearmap (2021), RCTMLA (2021)

Figure ES-1



Regional Location Map



Source(s): ESRI, Nearmap (2021), RCTMLA (2021)

Figure ES-2



Local Vicinity Map

ES.2 OTHER GOVERNING DOCUMENTS

In addition to this Specific Plan, which includes a Land Use Plan, Infrastructure Plan, Development Regulations, Design Guidelines, and Implementation Plan, the following documents also contain applicable information relevant to the project site:

- March Joint Powers Authority General Plan. Established in 1999, the General Plan includes goals and polices pertaining to land use, transportation, noise/air quality, housing, resource management, and safety/risk management that pertain to approximately 4,400 acres of land administered by the March Joint Powers Authority.
- March Joint Powers Authority Development Code. These regulatory provisions govern over topics on which this Specific Plan's development regulations do not address. Note: where the requirements of this Specific Plan differ from the requirements of the MJPA Development Code, this Specific Plan takes precedence.
- Air Installations Compatible Use Zone Study and Airport Land Use Compatibility Plan for the March Air Reserve Base. The Air Installations Compatible Use Zone Study was finalized in 2018. This document provides a land use compatibility analysis resulting in a number of policies and guidelines intended to ensure the continued operation of the March Air Reserve Base while minimizing hazards and impacts to the built environment and future development surround the Base.
- Mitigation Monitoring and Reporting Program (MMRP). An Environmental Impact Report (EIR) was prepared in compliance with the California Environmental Quality Act (CEQA) for the West Campus Upper Plateau Specific Plan. The MMRP stipulates measures required to be implemented to mitigate the environmental effects associated with the future development represented in the Specific Plan Area.

ES.3 SPECIFIC PLAN COMPONENTS

The West Campus Upper Plateau Specific Plan is organized into the following chapters.

Chapter 1 – Introduction:

Describes the purpose and objectives of this Specific Plan, related entitlement approvals for implementing development, and the general relationship between this Specific Plan and the March Joint Powers Authority General Plan.

Chapter 2 – Land Use:

Describes the West Campus Upper Plateau's development plan, which includes Industrial, Business Park, Mixed Use, Public Facility, Park, Open Space, and Open Space – Conservation land uses. This chapter also provides information on the open space areas identified in the Specific Plan, which constitute the majority of acreage within the project area.

Chapter 3 – Development Regulations:

Provides information on various applicable development regulations in the MJPA, including permitted, conditional and ancillary land use of the West Campus Upper Plateau. This chapter also includes development standards for the Specific Plan area.

Chapter 4 – Design Guidelines:

Provides the site planning, landscaping, and architectural theme within the West Campus Upper Plateau Specific Plan. This chapter provides guidelines on architectural design, landscape design, streetscapes, walls and fencing, and signage.

Chapter 5 – Transportation:

Describes the overall circulation and street network proposed to serve the Specific Plan, including street cross sections and integration/connection with the abutting existing road system.

Chapter 6 – Infrastructure and Grading:

Provides information on the planned backbone water, sewer, and storm drain systems; the planned dry utility network; and the preliminary grading concept for the development of the Specific Plan.

Chapter 7 – Implementation:

Provides the policies and procedures for the MJPA's review and approval of implementing projects within the West Campus Upper Plateau Specific Plan. This chapter describes the methods and procedures for interpreting and amending the Specific Plan, as necessary. A summary of maintenance responsibilities is also identified in this Chapter.

Chapter 8 – Consistency with the General Plan:

Include a matrix evaluating the consistency of the West Campus Upper Plateau Specific Plan to each of the applicable policies of the MJPA General Plan.

1 INTRODUCTION

1.1. SPECIFIC PLAN PURPOSE

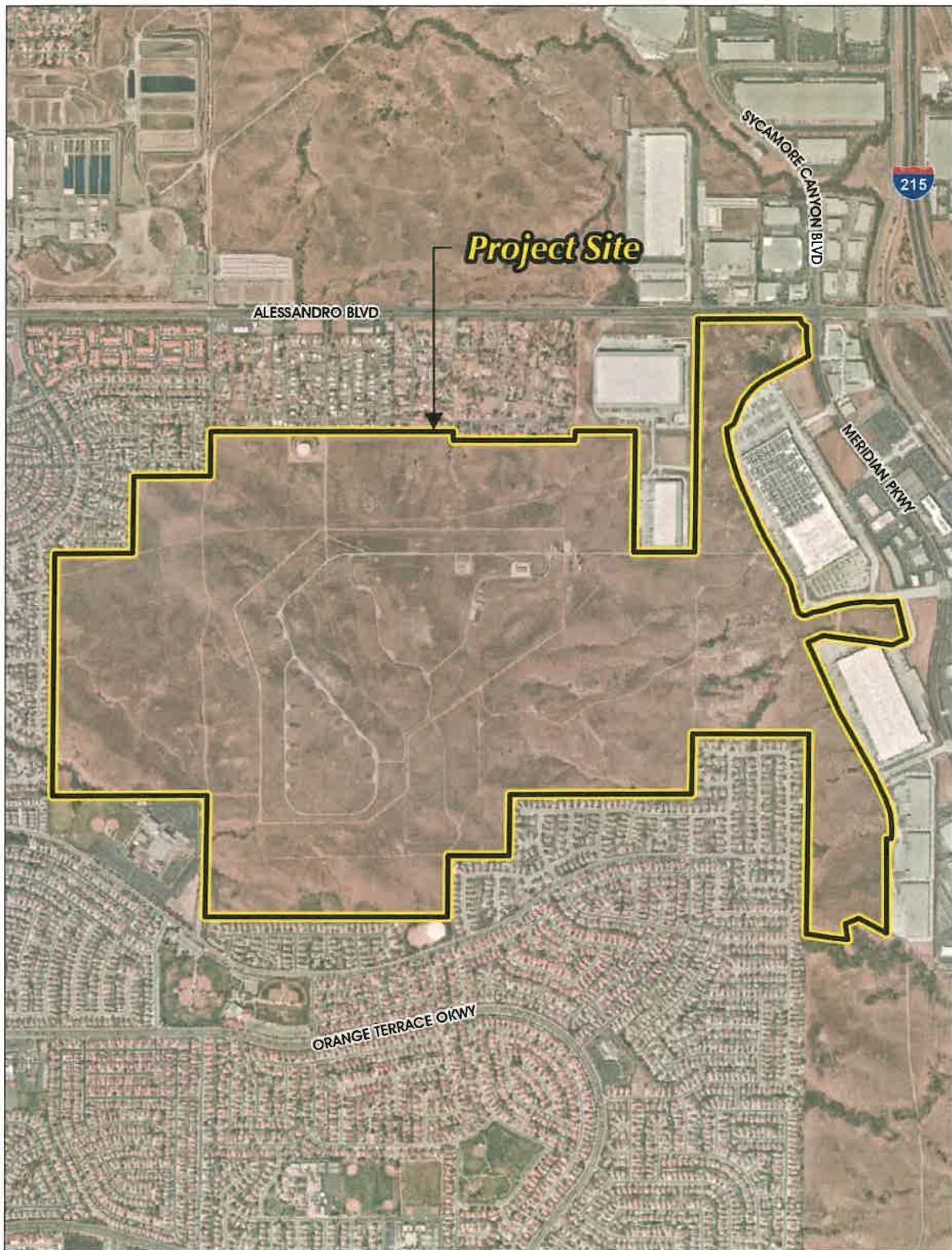
The purpose of this Specific Plan is to guide and direct the development of the subject project site into a master-planned industrial park, known as the West Campus Upper Plateau. The site is located within the western portion of the March Joint Powers Authority (MJPA) jurisdiction, more specifically within the West March Planning Subarea, west of the current terminus of Cactus Avenue. Projects proposed for development within the boundaries of the West Campus Upper Plateau Specific Plan are required to demonstrate substantial conformity with the standards and information contained in this Specific Plan.

Situated near Interstate 215 with access to two additional major freeways, development within the West Campus Upper Plateau is poised to successfully accommodate users who rely upon access and close proximity to the local and regional transportation network. The Specific Plan area is located less than one mile west of Interstate 215, less than 3 miles southwest of State Route 60, and approximately 6 miles southeast of State Route 91. Proximity to these routes provide not only the ability to quickly receive material and move goods but also provide ease of workforce access. *Figure 1-1, Aerial Photograph*, depicts the surrounding land use of the Specific Plan area.

1.2. SPECIFIC PLAN OBJECTIVES

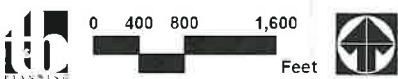
This Specific Plan achieves the following objectives:

- Provide a land use plan for the development of a state-of-the-art commerce area that accommodates modern business and industrial activities.
- Attract and sustain industrial, business park and mixed uses within the Specific Plan area that are buffered from sensitive uses by ample open space and landscape.
- Locate businesses that rely on transportation efficiency in an area of the MJPA that offers convenient access to the state highway system.
- Provide opportunities for positive economic benefit to the MJPA and region, including new net revenues which can be used for vital services.
- Diversify the MJPA's range of employment-generating land uses.
- Provide opportunities for the development and operation of active and passive use parks and trails that take advantage of and embrace the location.
- Identify capital improvements for water, recycled water, sewer, storm drain, and circulation facilities that serve planned land uses within and adjacent to the Specific Plan area.
- Define guidelines and standards for architecture, landscaping, entry monuments/signage, and walls and fencing within the Specific Plan area.
- Set forth a development phasing sequence that is aligned with a logical sequence for the installation of supporting on-site and off-site infrastructure.
- Implement the terms and conditions agree upon in the September 12, 2012, Settlement Agreement entered into between and among the CBD, the San Bernardino Audubon Society, MJPA, and LNR Riverside LLC, as the complete settlement of the claims and actions raised in *Center for Biological Diversity v. Jim Bartel, et al.*
- Implement the Conservation Area as a means of environmental protection.



Source(s): ESRI, Nearthmap (2021), RCTMLA (2021)

Figure 1-1



Aerial Photograph

1.3. AUTHORITY

This Specific Plan is a regulatory document prepared pursuant to the provisions of California Government Code §§ 65450 through 65457, which grants local government agencies the authority to prepare Specific Plans for the systematic implementation of their General Plan for all or part of the area covered by the General Plan. While the March Joint Powers Authority General Plan covers over 4,400 acres, this Specific Plan concentrates on the future development of the approximately 807.54-acre West Campus Upper Plateau property.

California Government Code §§ 65450 through 65457 establish the authority to adopt a Specific Plan, identify the required contents of a Specific Plan, and mandate consistency with the General Plan. According to California Government Code § 65451:

- (a) A Specific Plan shall include text and a diagram which specify all the following in detail:
- (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
 - (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
 - (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
 - (4) A program of implementation measures including regulations, programs, public works projects, and financing measures, necessary to carry out items (1), (2), and (3).
- (b) The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.

This Specific Plan includes each of the required elements listed above and establishes the essential link between the policies of the March Joint Power Authority General Plan and the West Campus Upper Plateau property. All future development plans and implementing construction activities within this Specific Plan are required to be consistent with the requirements set forth in this Specific Plan and with all other applicable City regulations.

1.4. BACKGROUND AND HISTORY

Since 1988, the federal government closed and realigned military bases throughout the United States. In order to limit the economic disruption caused by base closures, the California State Legislature authorized the formation of joint powers authorities to regulate the redevelopment of closed/realigned military installations. Joint powers authorities are empowered to activate a redevelopment agency for each base to be closed. In 1993, the federal government, through the Defense Base Closure and Realignment Commission, called for the realignment of March Air Force Base (MAFB) and for a substantial reduction in its military use. In April 1996, MAFB was re-designed as an Air Reserve Base (ARB). The cities of Moreno Valley, Perris, and Riverside, and the County of Riverside formed the



March Joint Powers Authority (MJPA), which continues to serve as the reuse authority for the over 4,400 acres of declared surplus property. The MJPA prepared several planning, policy, and regulatory documents to guide the redevelopment of the former MAFB. These documents include:

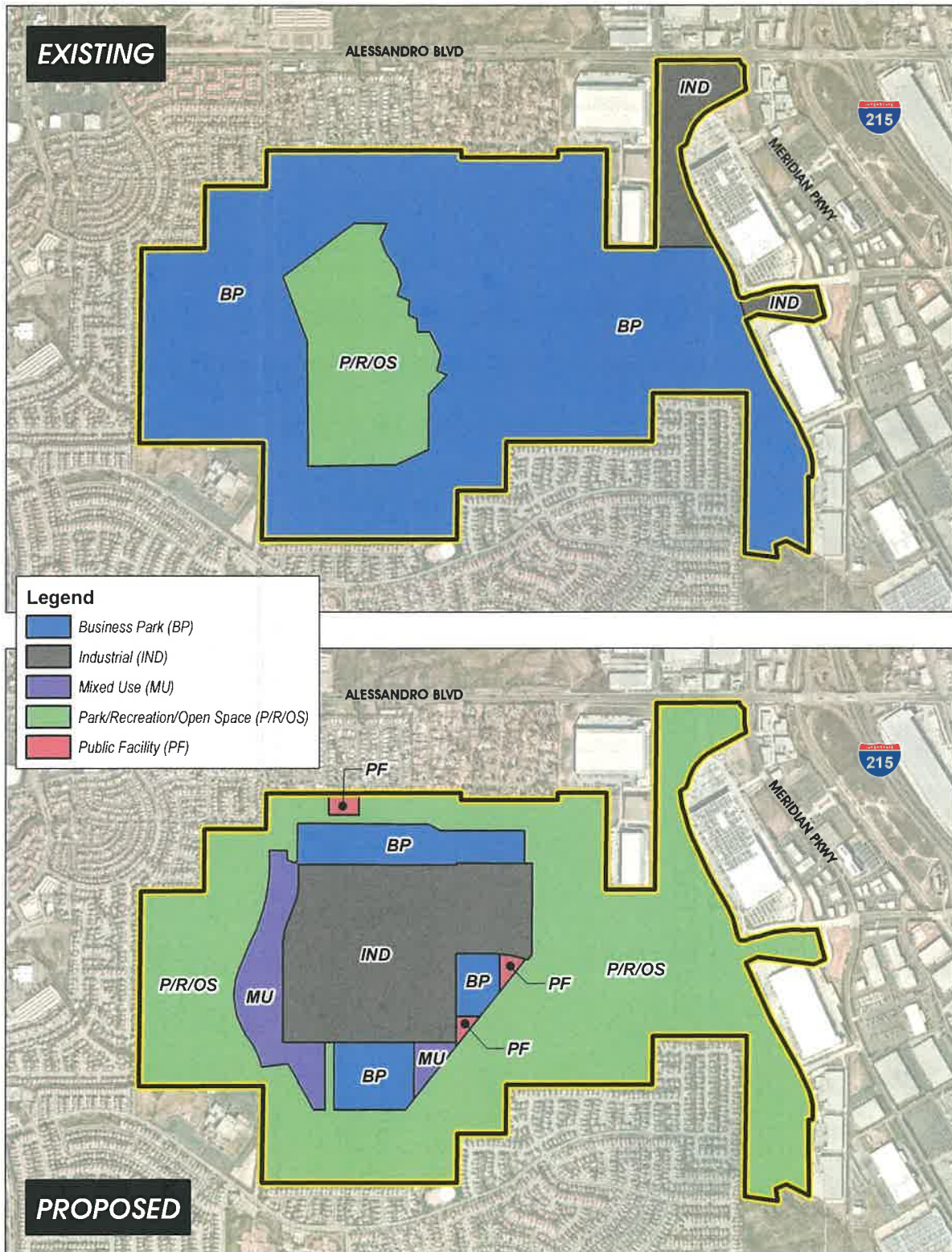
- March Air Force Base Master Reuse Plan, MJPA (November 1995)
- Final Environmental Impact Statement: Disposal of Portions of March Air Force Base (February 1996)
- Final Environmental Impact Report for the March Air Force Base Redevelopment Project (June 1996)
- Redevelopment Plan for the March Air Force Base Redevelopment Project (June 1996)
- March Joint Powers Authority Development Code (July 1997)
- General Plan for the March Joint Powers Authority (September 1999)
- Master Environmental Impact Report for the General Plan of the March Joint Powers Authority (September 1999)
- March Business Center Statutory Development Agreement (2003)
- MJPA General Plan Amendment (February 2003)
- March Business Center Design Guidelines (2003)
- Final Air Installations Compatible Use Zone Study, March Air Reserve Base (2018)

On September 12, 2012, a Settlement Agreement was entered between and among the Center for Biological Diversity (CBD), the San Bernardino Valley Audubon Society, MJPA, and LNR Riverside LLC as the complete settlement of the claims and actions raised in *Center for Biological Diversity v. Jim Bartel, et al.* (CBD Settlement Agreement, MJPA 2012). The CBD Settlement Agreement contemplated the division of the West Campus Upper Plateau Specific Plan Area into a Conservation Area, Developable Area, Proposed Park Area and Water Quality/Open Space Area. The Specific Plan land use plan contain herein is designed representative of and to be consistent with this Settlement Agreement.

1.5. PLANNING CONTEXT

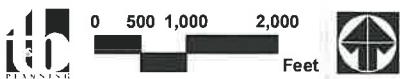
Figure 1-2, *Existing & Proposed General Plan Land Uses*, depicts the current and proposed General Plan land use designations in the West Campus Upper Plateau Specific Plan Area. Figure 1-3, *Existing and Proposed Zoning*, depict the existing and proposed zoning for the West Campus Upper Plateau.



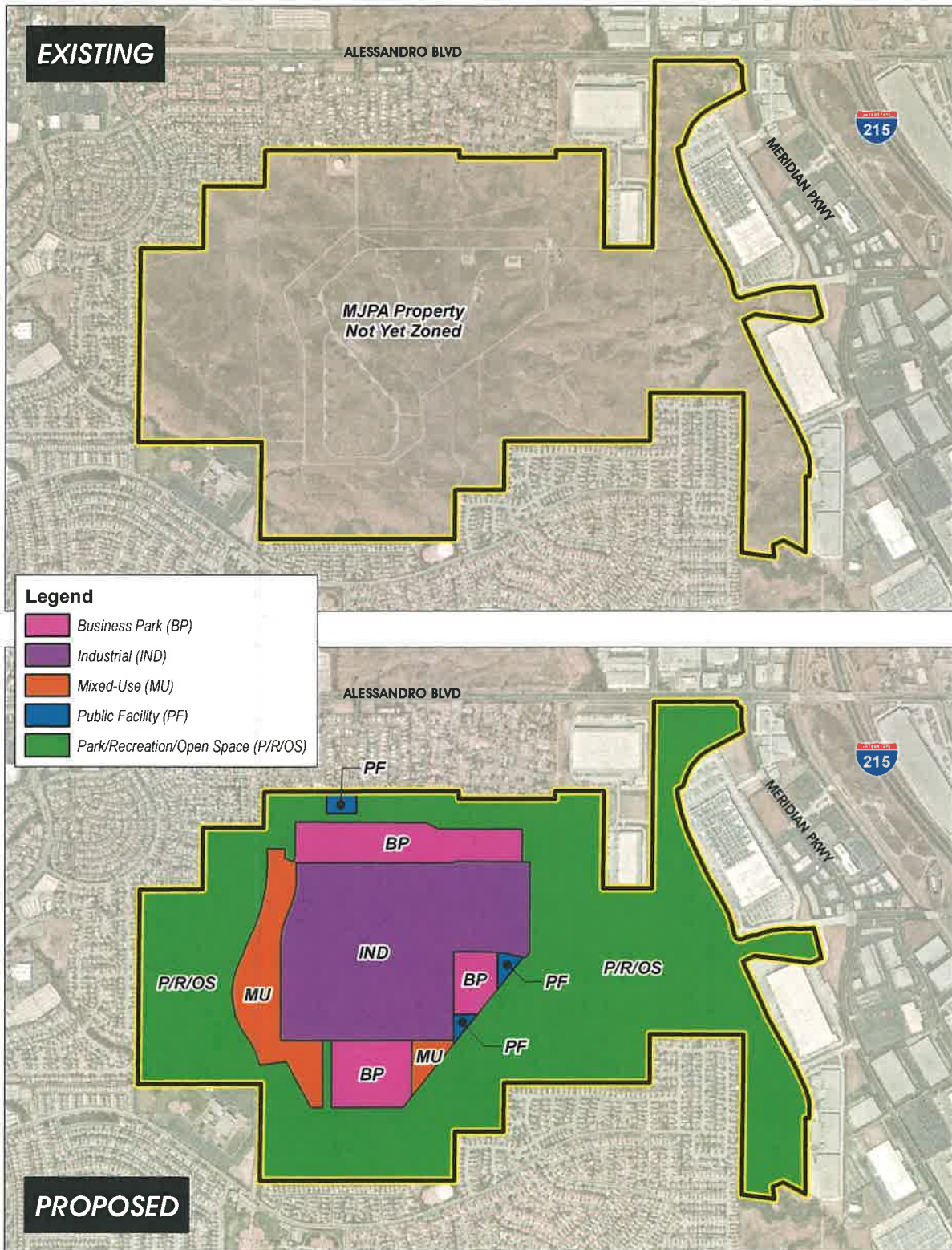


Source(s): ESRI, March JPA General Plan (2017), Nearmap (2021)

Figure 1-2

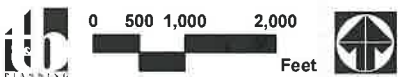


Existing & Proposed General Plan Land Uses



Source(s): ESRI, March JPA (2021), Nearmap (2021)

Figure 1-3



Existing & Proposed Zoning

1.7. ENVIRONMENTAL IMPACT REPORT

An EIR was prepared (SCH No. 2021110304) in accordance with the provisions under CEQA to evaluate and disclose the potential environmental consequences of the West Campus Upper Plateau Specific. The EIR serves as a project-wide environmental document for the West Campus Upper Plateau Specific Plan Area. The March Joint Powers Authority (MJPA) serves as the lead agency in the preparation and certification of the EIR. The West Campus Upper Plateau Specific Plan and EIR jointly serve a path to develop the Specific Plan Area as intended, taking into account applicable policies, goals, objectives, and environmental considerations of the MJPA General Plan.



1.8. DISCRETIONARY ACTIONS

The following discretionary actions will be required as part of the West Campus Upper Plateau Specific Plan:

- **GENERAL PLAN AMENDMENT:** A General Plan Amendment to the land use plan described herein is necessary to reflect the changes to land uses and ultimate roadway configuration as represented in *Figure 1-2, Existing & Proposed General Plan Land Uses*.
- **SPECIFIC PLAN:** The West Campus Upper Plateau Specific Plan requires review by and approval of the MJPA. Once adopted, this Specific plan will create a comprehensive land use document that identifies and defines land uses within the Specific Plan.
- **CHANGE OF ZONE:** Under the existing MJPA Zoning Map, the West Campus Upper Plateau Specific Plan Project Area did not have zoning designations identified. As a result of the West Campus Upper Plateau Specific Plan, the MJPA will establish zoning within the Specific Plan Area that is consistent with the land uses and locations identified within the Specific Plan.
- **TENTATIVE TRACT MAP:** A Tentative Tract Map will be approved by the MJPA for the Specific Plan Area indicating the approximate boundaries and dimensions of parcels and streets. Following the Tentative Tract Map, a Final Map will become the legal document that identifies the developable parcels within the Specific Plan.
- **PLOT PLANS:** All development within the Specific Plan property shall be subjected to a Plot Plan review. Adoption of this Specific Plan by the MJPA includes the design guidelines contained in *Chapter 4, Design Guidelines*, which shall be the design criteria by which development projects with the Specific Plan shall be reviewed during the Plot Plan review.
- **DEVELOPMENT AGREEMENT:** Due to the scale and complexity of the Project, a Development Agreement is proposed to vest the Project entitlements and fees, ensure financing of public improvements required by the conditions of approval, and provide certain Community Benefits including compliance with the terms of the 2012 Settlement Agreement, and provision of new public benefits, including, but not limited to, expansion of employment opportunities for area residents.

2 LAND USE

2.1. LAND USE OVERVIEW

This chapter identifies the types of land uses to be allowed in the Specific Plan Area and provides regulations and standards to govern future development. In accordance with the General Plan, this Specific Plan accommodates land uses that support future growth and development in the area. The West Campus Upper Plateau Specific Plan Land Use provisions reference the following policies, regulations, and guidelines:

- MJPA General Plan (1999)
- MJPA Development Code (1997)

This chapter specifies broad land use categories that will guide the development of the Specific Plan Area. Within each broad category, specific land uses are identified, together with an indication of whether such uses are permitted, subject to a conditional use permit, or not allowed. In addition, development regulations that will govern the development of the individual projects comprising the West Campus Upper Plateau are described.

2.2. PURPOSE AND APPLICABILITY

The following items describe the relationship of the Specific Plan land use regulations in the context of other land use documents developed by the JPA.

1. Terms used in these regulations and guidelines shall have the same definitions as given in the MJPA Development Code ("Development Code") and the General Plan of the MJPA ("General Plan") unless otherwise defined in the Specific Plan.
2. Any details or issues not specifically covered in the Specific Plan regulations shall be subject to the regulations of the Development Code
3. The Specific Plan Land Use regulations are adopted pursuant to Section 65450 of the State of California Government Code et seq. It is specifically intended by such adaptation that the development standards herein shall regulate all development within the Specific Plan Area.

2.3. LAND USE COMPATIBILITY

The Specific Plan establishes development patterns to limit the potential for land use conflicts, both within the West Campus Upper Plateau and in relation to other uses in the vicinity. A key consideration guiding the development is the proximity of Air Reserve Base Runway 14/32. The Riverside County Airport Land Use Commission published an Airport Land Use Plan in 1984. This plan established land use restrictions within Airport Influenced Area, which consist of imaginary surfaces extending outward from an airport's runway. In 1998, an Air Installation Compatible Use Zone (AICUZ) Study was completed to identify land use restrictions and height limitations within the Airport Influenced Area. Additional information regarding aviation regulations is provided in Section 2.5 below.

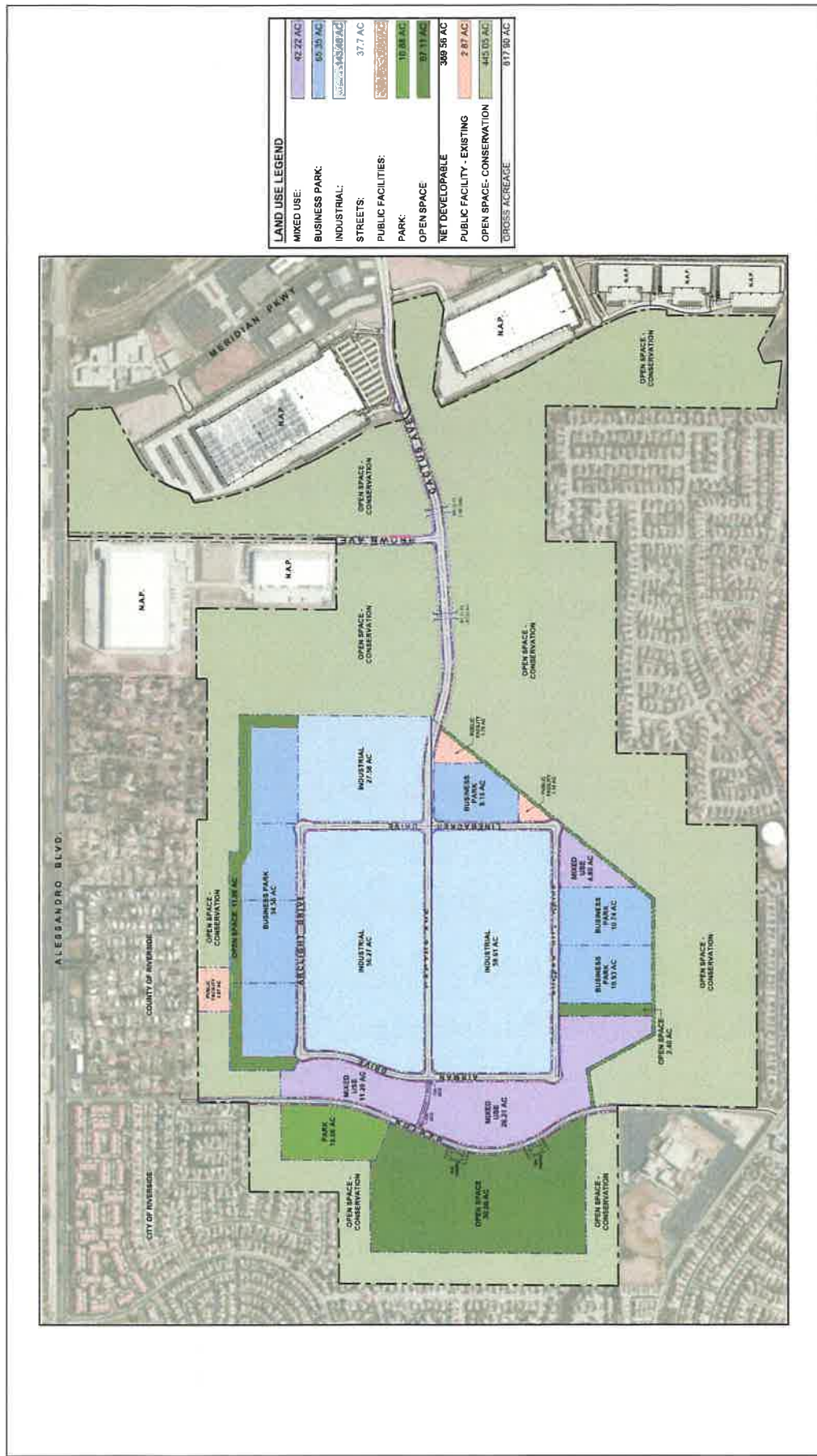


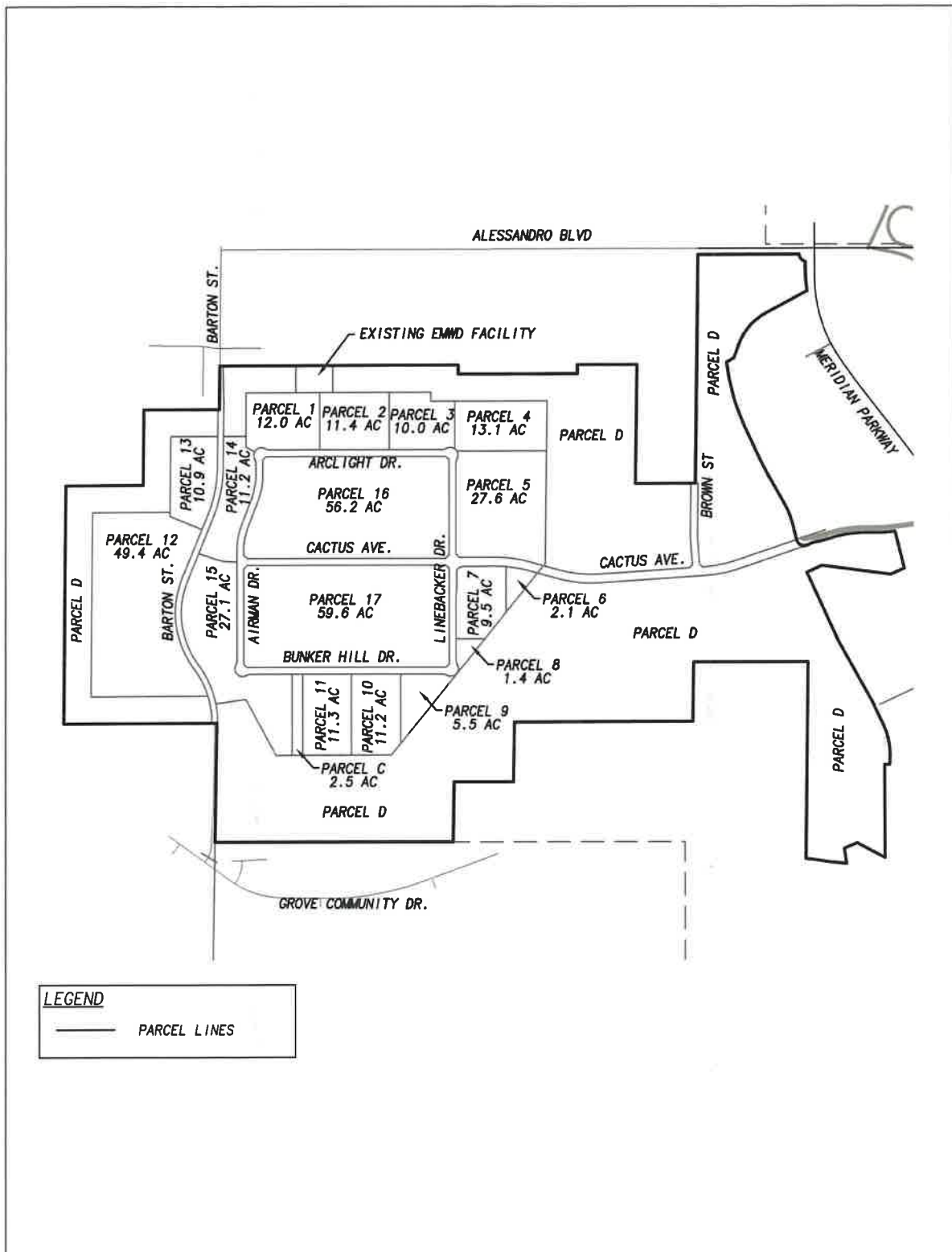
Figure 2-1

Conceptual Land Use Plan

Source(s): Mettlen/RGA Office of Architects Design (02-04-2022)



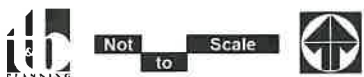
Specific Plan No. XXX



LEGEND
 ——— PARCEL LINES

Source(s): DRC Engineering (01-18-2022)

Figure 2-2



Proposed TPM Exhibit

2.4. LAND USE PLAN

The West Campus Upper Plateau Specific Plan is an 817.9-acre master-planned industrial park that provides industrial, commercial, and office use and a substantial amount of open space and recreational use. The open space and parks are also provided for the employees of the West Campus Upper Plateau, visitors, and the surrounding residents. *Figure 2-1, Conceptual Land Use Plan*, and *Figure 2-2, Proposed TPM Exhibit*, depict the physical arrangement and the major roads within the Specific Plan Area. *Table 2-1, Land Use Plan Statistical Summary*, provides the acreages and development intensity for each land use designation within the West Campus Upper Plateau Specific Plan.

This section of the Specific Plan identifies the following five land use districts: Business Park, Industrial, Mixed Used, Park/Recreation/Open Space, and Public Facility. These districts are summarized below:

Business Park:

Business Park uses include administrative, financial, governmental, and community support services; research and development centers; light manufacturing; parcel delivery terminal; vocational education and training facilities; business and trades schools; and emergency services. Business Park areas are generally served by arterial roadways, providing automobile and transit access. These areas are characterized as major employment concentrations. Development in this category, except for warehousing, is generally within a campus-like setting or cluster development pattern. Outdoor storage as a primary use is prohibited.

Industrial:

Industrial may support a wide range of manufacturing and non-manufacturing uses from warehouse and distribution facilities to industrial activities. Uses supported include warehousing/distribution and assemblage of non-hazardous products and materials or retailing related to manufacturing activity; and parcel delivery terminal on no less than 60 acres. Uses may include open storage, office/industrial park; light industry; manufacturing; research and development centers; maintenance shops; and emergency services center. The area devoted to outdoor storage may not exceed the building area.

Mixed-Use:

Mixed uses include a variety of complementary land uses, including commercial, business park, office, medical, educational and vocational, research and development, and services. Industrial, warehousing, and outdoor storage is prohibited.

Park/Recreation/Open Space:

Park/Recreation/Open Space uses include all passive and active park or recreation areas whether private or public in the Planning Area. Active recreation activities include outdoor athletic fields and public parklands. Passive activities include natural preserves with trails, along with designated arid natural open space areas. The Park/Recreation/Open Space uses will also include civic uses such as police and fire substations.

Public Facility:

Though only a limited amount of acreage is provided for this district, Public Facility uses include a wide range of public, quasi-public, and private uses such as public cultural and historical facilities, government administrative offices and facilities, public utilities, and major transportation corridors. However, land uses determined to be sensitive to, or incompatible with aviation operations shall be excluded.

Table 2-1 Land Use Plan Statistical Summary

Land Use Designation	Acreages	Maximum Floor Area Ratio	Maximum Building Square Footage
Mixed Use	42.22	0.60	1,103,462 SF
Business Park	65.35	0.75	2,134,985 SF
Industrial	143.56	0.60	3,752,084 SF
Public Facilities	5.71	---	---
Open Space			
Park	10.88	---	---
Open Space	67.11	---	---
Open Space - Conservation	445.47	---	---
Roadways	37.70	---	---
Total	817.90		6,990,531 SF

2.5. OVERLAY DISTRICTS

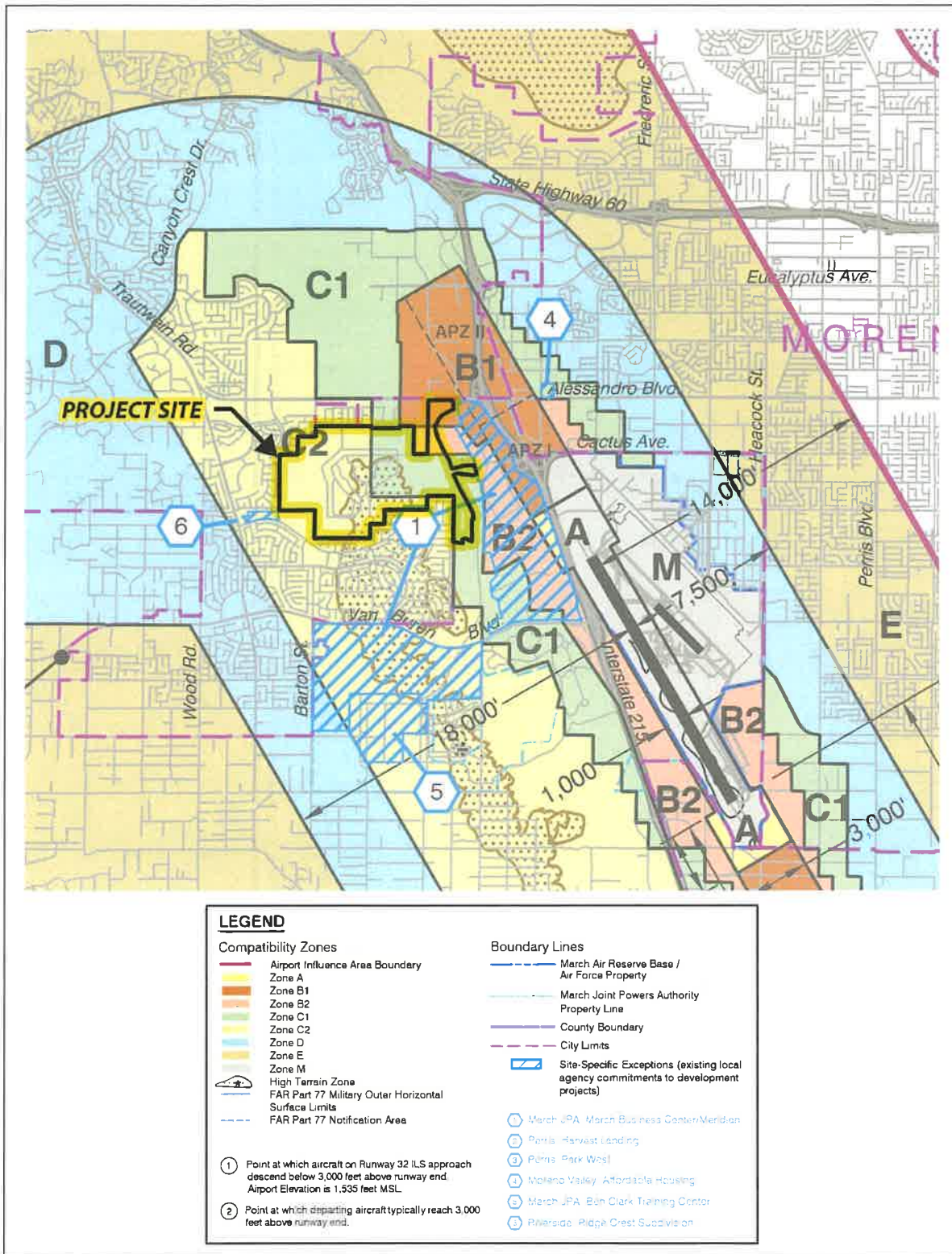
The Specific Plan area contains overlay zones within its boundaries. *Figure 2-3, MARB Land Use Compatibility Map*, depicts the location of the Inner Approach/Departure Zone (B1), High Noise Zone (B2), Primary Approach/Departure Zone (C1), and the Flight Corridor Zone (C2). To ensure consistency with the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan, this Specific Plan provides land use regulations relating to safety (both for air navigation and for people within the West Campus Upper Plateau), noise impacts, and building heights. The following paragraphs summarize these regulations.

2.5.1. Aviation Safety Regulations

Figure 2-3, MARB Land Use Compatibility Map, shows the location of the project in relationship to the various land use compatibility zones in association with the March Air Reserve Base. The project site is located within compatibility zones B1, B2, C1 and C2, with most of the project being within zones C1 and C2. Depending upon the compatibility zone, certain land uses are prohibited or discouraged due to their proximity to the airport. Any discouraged uses must be reviewed by the Riverside County Airport Land Use Commission. Additional information regarding prohibited and discouraged land uses can be found in Table MA-2 of the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan.

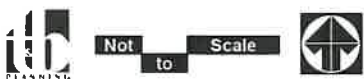
Regulations relating to the safety of air navigation are as follows:

- The Final Map shall convey an aviation easement to the JPA
- Lighting Plans for any development shall be reviewed and approved by the ALUC and the Air Force Reserve
- Uses that would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft during initial climb or final approach shall be prohibited
- Use of rooftop solar panels shall be reviewed and approved by the JPA
- Uses that generate smoke or water vapor which would affect safe air navigation shall be prohibited
- Uses that generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation shall be prohibited



Source(s): March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (11-13-2014)

Figure 2-3



MARB Land Use Compatibility Map

2.5.2. AVIATION NOISE REGULATIONS

An acoustical analysis shall be required for any noise sensitive uses before the implementation of the following uses:

- Churches and Places of Religious Assembly
- Hotel/Motel
- Museums
- Private Clubs, Lodges, and Fraternal Organizations
- Radio and television studios
- Trade Schools

This analysis will include components necessary to achieve an indoor noise reduction level of 25 to 30 decibels for each of the Project's components with noise sensitive uses, and will include all surrounding noise sources (e.g., transportation and industrial) at their ultimate design and capacity.

2.5.3. AVIATION BUILDING HEIGHTS REGULATIONS

A further limitation on site development is the height of structures in the vicinity of the runway. Federal Aviation Regulations (FAR) Part 77 defines a variety of imaginary surfaces around airports, including a horizontal surface and a conical surface. FAR Part 77 is not an absolute height limit. Instead, it is a guideline used by the FAA to identify structures that may constitute a hazard to air navigation. Any construction or alteration of greater height than an imaginary surface extending upward and outward at a 100 to 1 slope from the nearest point of the runway (see FAR §77.13.2.i) will require the preparation of FAA Notice of Proposed Construction or Alteration (form 7460-1). If a hazard to air navigation is identified, then the FAA will issue a determination of hazard to air navigation. However, the FAA does not have the authority to prevent encroachment; it is up to the local land use authority to enforce the recommendation.

Figure A-1, MARB Airspace Protection Surfaces in Appendix A is a land use compatibility map that shows FAR Part 77 surfaces adjacent to MARB. As shown in this exhibit, terrain elevations (without structures) penetrate the conical surface on the Specific Plan Area, east of Barton Street. Depending on the elevation of the finished grade and height of the proposed structure, future development in West Campus Upper Plateau Specific Plan may penetrate the Part 77 surfaces. Development proposals in West Campus Upper Plateau will file form 7460-1 as appropriate based on §77.13.2.i.

Figure 2-3, MARB Land Use Compatibility Map, shows the location of the Height Caution Zone. This zone is defined as the area where the maximum allowable building height plus the rough grading plan elevation penetrates the Part 77 surface. Within the Height Caution Zone, objects up to 50 feet tall are acceptable, and do not require ALUC review for the purposes of height factors. ALUC review will be required for any proposed object taller than 50 feet within the Height Caution Zone.

2.6. OPEN SPACE

The West Campus Upper Plateau Specific Plan will provide approximately 445 acres of Open Space – Conservation land use, representing more than ½ of the entire Specific Plan Area. The open space areas are represented in three different land use types: park, open space, and open space-conservation. As a result, the open space will provide both active and passive use opportunities, as well as habitat value and aesthetic benefit to the area.

2.6.1. PARK SITE

An approximately 10-acre park will be established in the northwestern corner of the West Campus Upper Plateau Area, west of Barton Street. This 10-acre park is intended for both active and passive use, and is conceptually designed to include two soccer fields, two basketball/pickleball courts, two exercise nodes, playground area, shaded picnic areas, restrooms, a loop trail, and parking for approximately 100 vehicles. Park use is anticipated to be from local residents and employees of businesses within the Specific Plan Area. *Figure 2-4, Conceptual Park Design*, provides a design concept for the park site.

2.6.2. OPEN SPACE AREA

An Open Space area is approximately 50 acres in size will be located in the western segment of the Specific Plan Area, west of and adjacent to Barton Street. This area will be designated for hiking trails and other passive uses. The area will generally remain in its natural state, with exception to the planned hiking trails. Two trailhead locations will be provided adjacent to and west of Barton Street. Each trailhead will provide a small parking area, benches, and information kiosks for trail users. This is further represented in *Figure 2-5, Typical Trailhead Concept*.

2.6.3. OPEN SPACE CONSERVATION AREA

The Specific Plan Area includes a Conservation Area as part of the Settlement Agreement that was entered between and among the Center for Biological Diversity, the San Bernardino Valley Audubon Society, MJPA, and LNR Riverside LLC in 2012. The Conservation Area will surround the West Campus Upper Plateau development area, providing a buffer to the surrounding residential area in the City and County of Riverside. A majority of the Conservation Area is proposed within the eastern segment of the Specific Plan Area. Wildlife crossing will be provided crossing under Cactus Avenue. *Figure 2-6a and 2-6b, Conceptual Wildlife Crossing Design*, shows the typical wildlife crossing designs, and *Figure 2-7a and 2-7b, Conceptual Wildlife Crossing Cross Section*, shows the typical wildlife crossing cross sections. Additionally, there are several existing recreational trails throughout the open space conservation area. The Project proposes to retain many of these trails and provide for continued public use. This is further represented in Specific Plan Section 5.2.



Figure 2-4

Conceptual Park Design



Specific Plan No. XXX

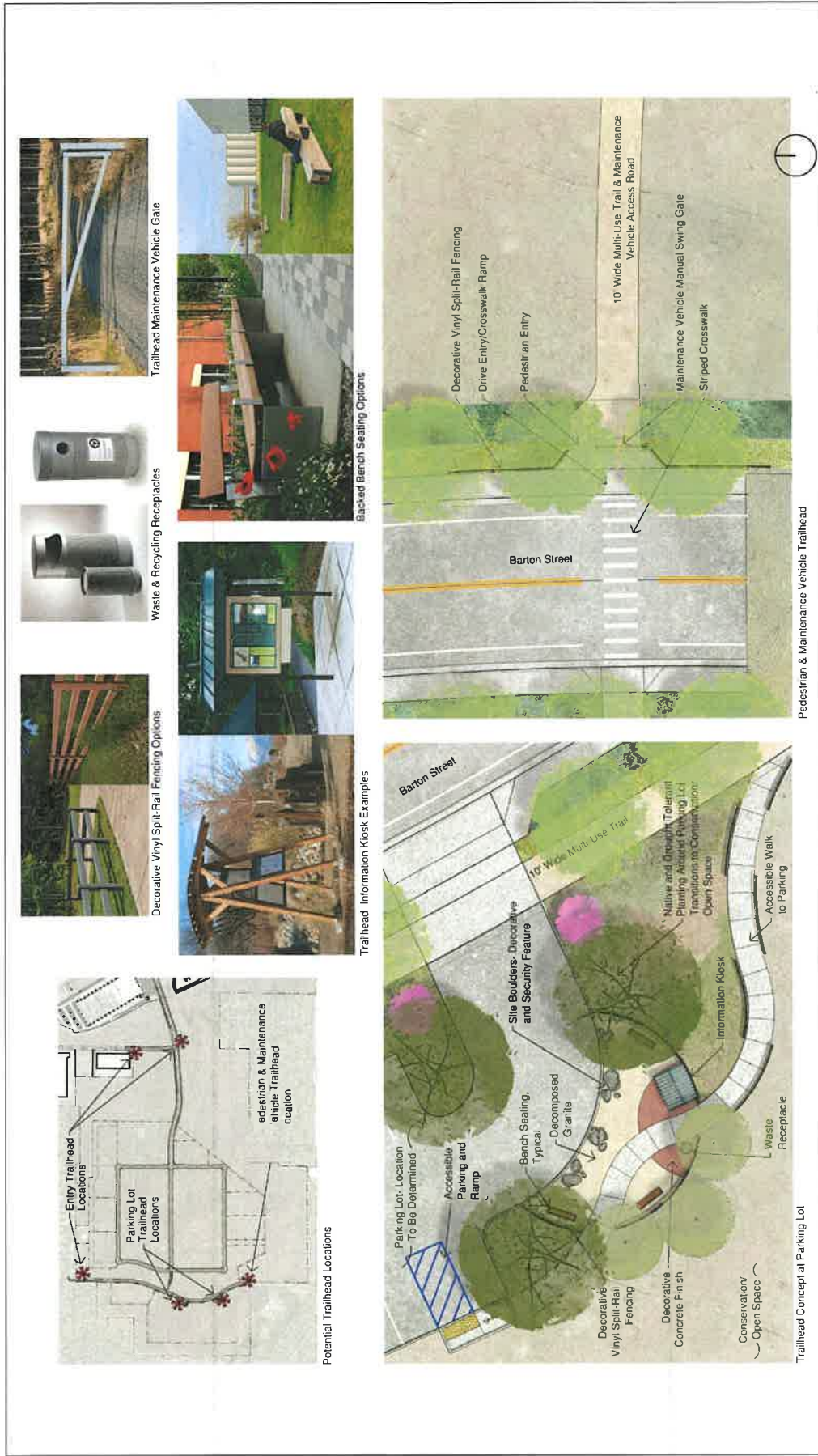
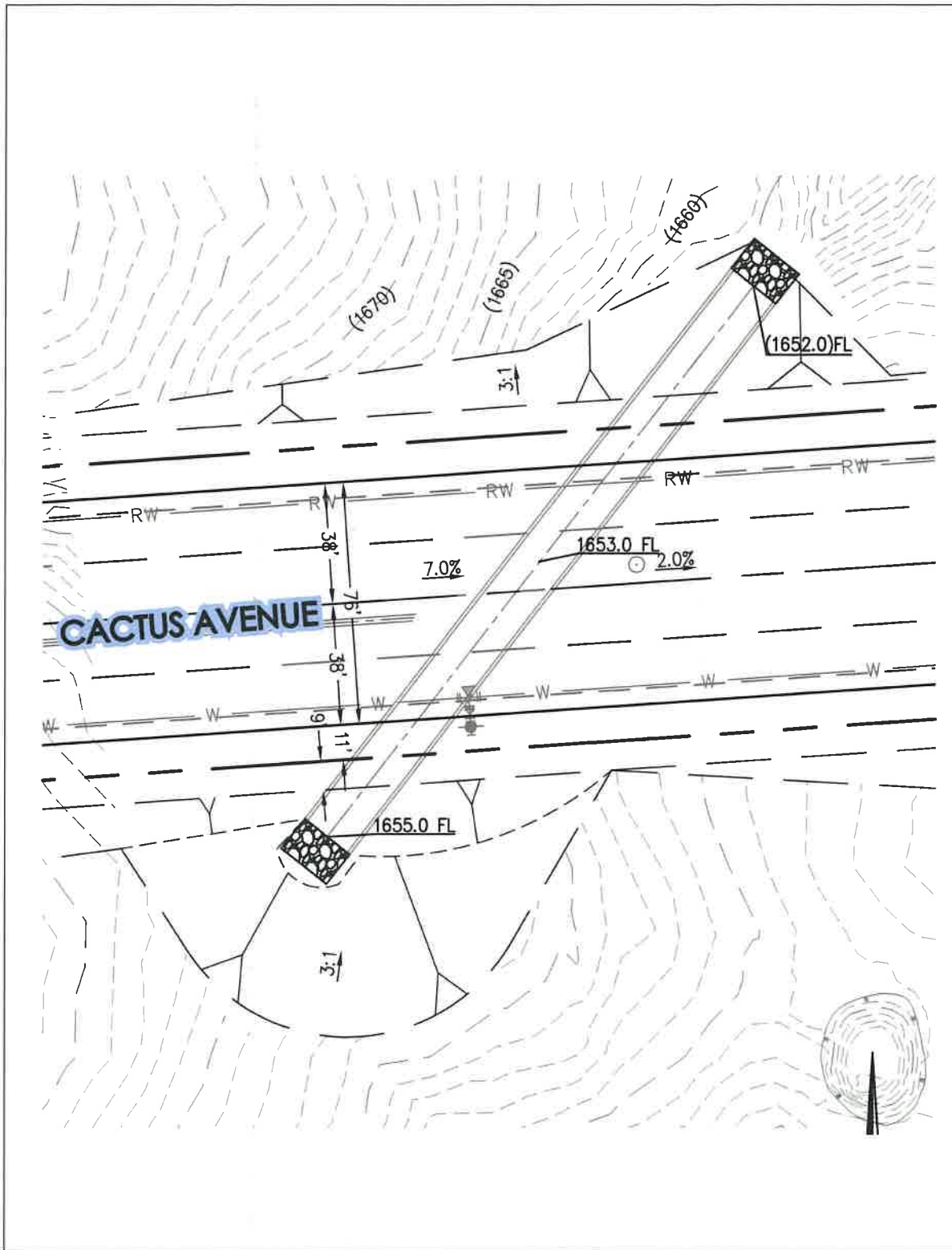


Figure 2-5

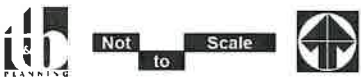
Typical Trailhead Concept



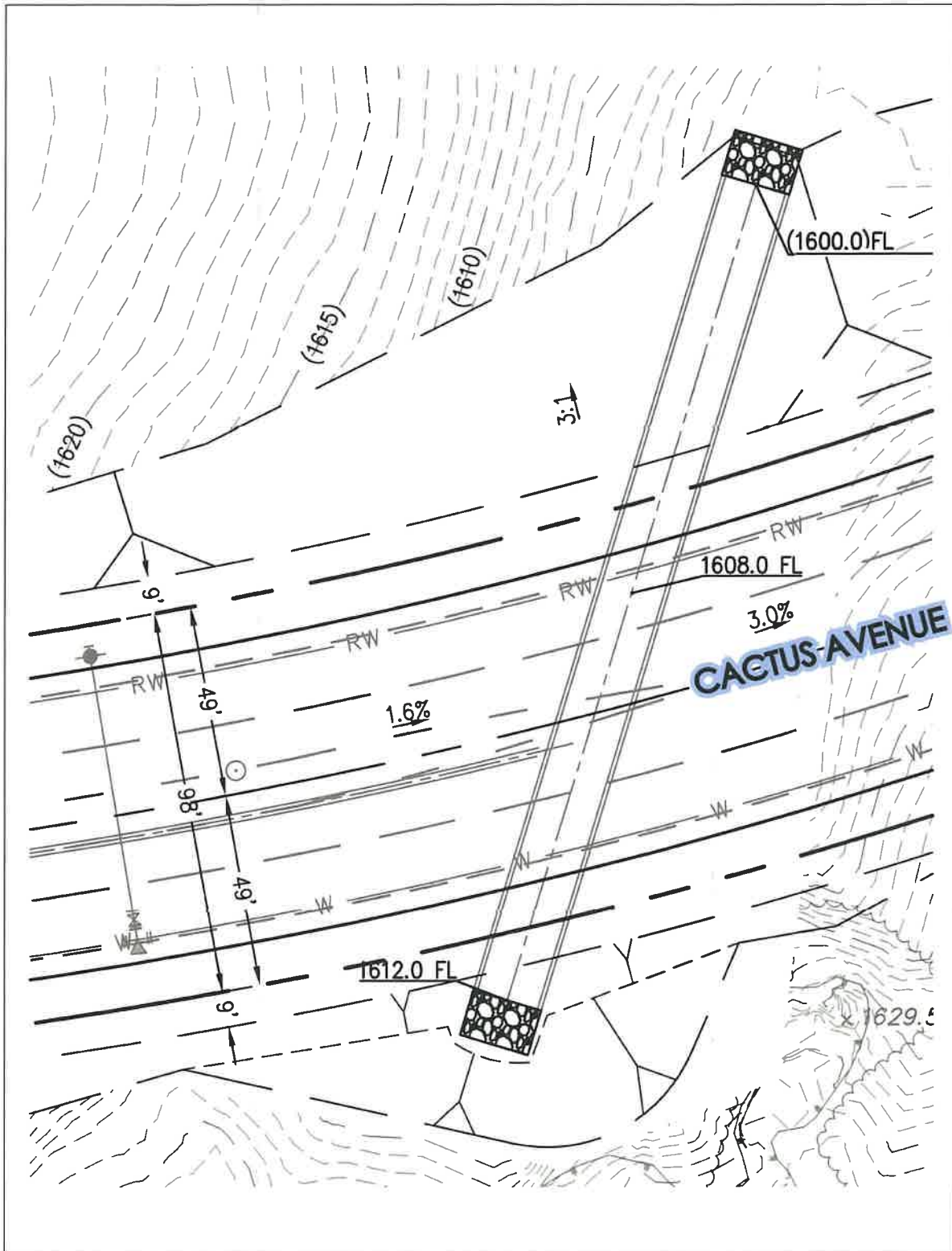


Source(s): DRC Engineering Inc (01-20-2022)

Figure 2-6A

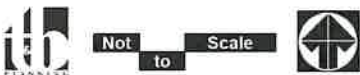


Conceptual Wildlife Crossing Design



Source(s): DRC Engineering Inc (01-20-2022)

Figure 2-6B

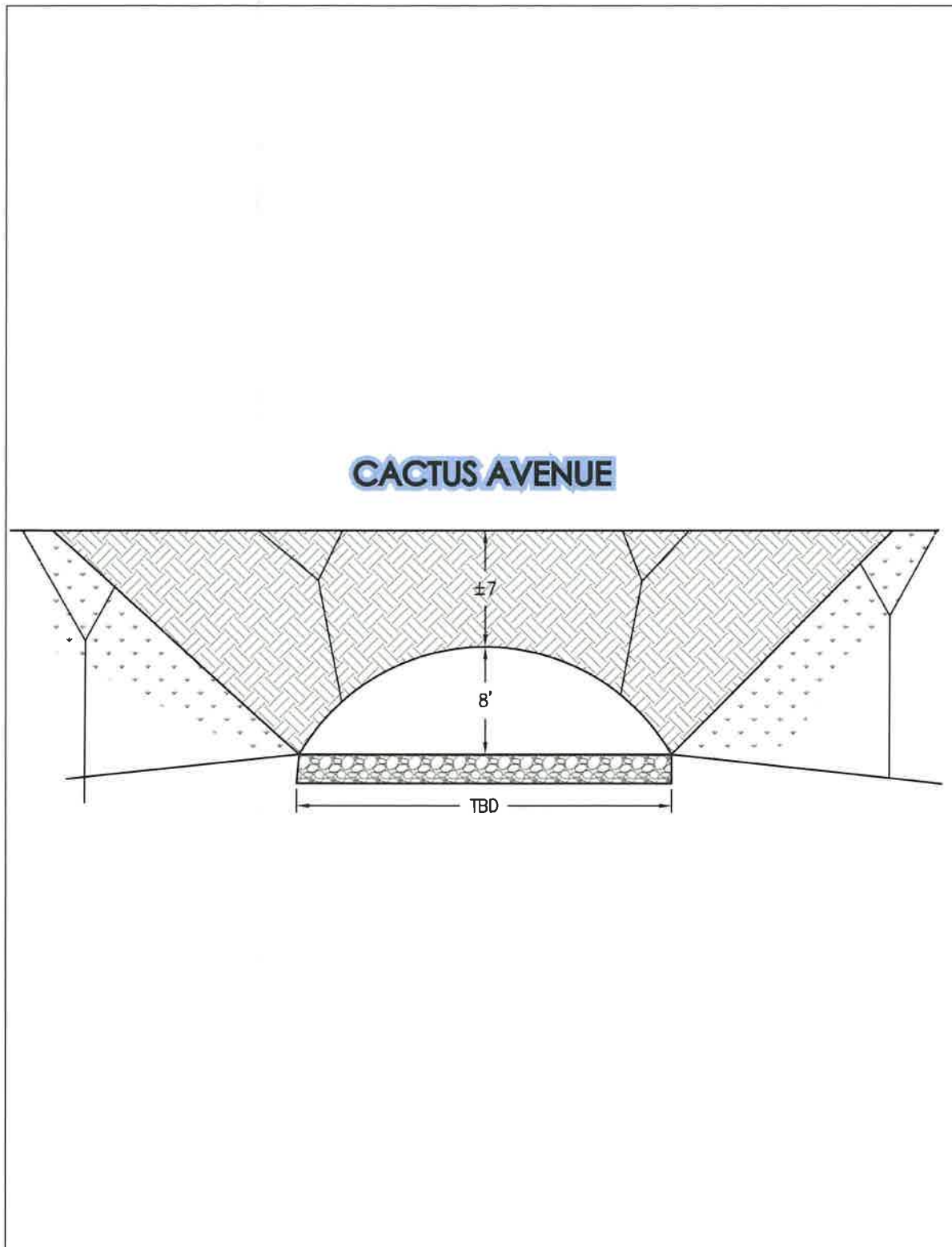


Not to Scale

Conceptual Wildlife Crossing Design

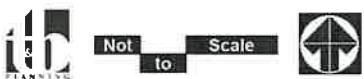
Specific Plan No. XXX

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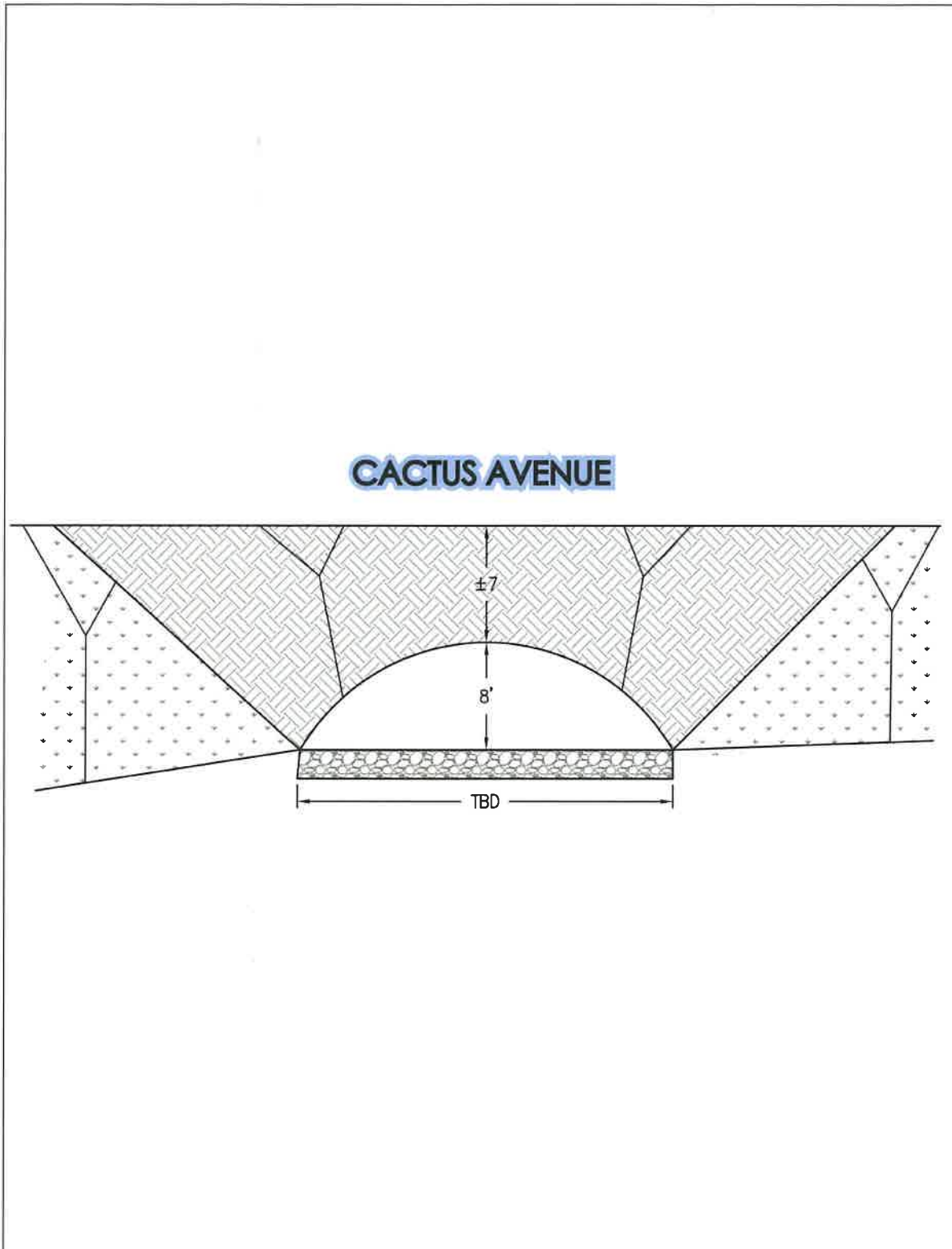
Source(s): DRC Engineering Inc (01-20-2022)

Figure 2-7A



Not to Scale

Conceptual Wildlife Crossing Cross Section



Source(s): DRC Engineering Inc (01-20-2022)

Figure 2-7B



Not to Scale



Conceptual Wildlife Crossing Cross Section

3 DEVELOPMENT REGULATIONS

3.1. PURPOSE AND INTENT

This chapter formally establishes the various uses permitted and development standards applicable to the West Campus Upper Plateau Specific Plan. The regulations provided herein work in concert with the architectural and landscape guidelines set forth in Chapter 4 (Design Guidelines) to achieve the vision of and direction for this Specific Plan.

3.2. DEFINITION OF TERMS

The meanings of words, phrases, titles, and terms shall be the same as provided for in the March Joint Powers Authority Development Code, unless otherwise identified in this Specific Plan.

3.3. APPLICABILITY

The regulations set forth in the chapter shall apply to all development plans or agreements, tract or parcel maps, site plans, or any other action requiring administrative or discretionary approval within the West Campus Upper Plateau Specific Plan Area. Whenever the development standards contained herein differ from those contained in the March Joint Powers Authority Development Code, the provisions of this Specific Plan shall take precedence. Any development standard, condition, or situation not specifically addressed herein shall be subject to the applicable requirements of the Development Code.

3.4. PERMITTED, CONDITIONAL AND ANCILLARY USES

The West Campus Upper Plateau Specific Plan area and structures/facilities thereon may be developed and/or used according to those activities listed in *Table 3-1, Permitted Uses*. Table 3-1 lists the permitted, conditionally permitted, and administratively permitted land uses for each land use district established by this Specific Plan (Industrial, Business Park, Mixed-Use, and Open Space/Park and Public Facility). A use that is not listed in Table 3-1. is a prohibited use unless otherwise allowed pursuant to the procedures described in Chapter 7, *Implementation*, or applicable interpretations and determinations established by the Development Code. The entire Specific Plan Area is located within the boundary of the March Air Reserve Base Land Use Compatibility Zones (Zones B1, B2, C1, and C2), which may prohibit or restrict certain land uses. Refer to the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan for additional information.

LAND USE TABLE

Table 3-1 is a matrix indicating the status of specific land use types within the development districts described in Chapter 2. For each specific land use, a "P" indicates that it is permitted, and a "C" indicates that a conditional use permit is required.

Table 3-1
West Campus Upper Plateau Specific Plan Land Use Table

P= Permitted; C= Conditional Use Permit; A=Ancillary; --- = Prohibited					
USES	BUSINESS PARK ^{1,2}	INDUSTRIAL ³	MIXED-USE ⁴	P/R/OS	PUBLIC FACILITY
INDUSTRIAL					
Bio-Medical Waste Treatment Facility	---	C	---	---	---
Manufacturing - Custom	P	P	C	---	---
Manufacturing - Light	P	P	C	---	---
Manufacturing - Medium	P	P	---	---	---
Manufacturing - Heavy	C	C	---	---	---
Newspaper Publishing Plants	P	P	---	---	---
Parcel Delivery Terminal	P	P	C	---	---
Research & Development	P	P	P	---	---
Trucking/Transportation Terminals	P	P	---	---	---
WHOLESALE STORAGE/DISTRIBUTION					
Public Storage/Mini-Warehouse (Indoor)	C	C	C	---	---
Business Enterprise	P	P	P	---	---
Warehouse, Storage & Distribution - Medium	P	P	C	---	---
Warehouse, Storage & Distribution - Heavy	P	P	C	---	---
E-Commerce Fulfillment Center	P	P	C	---	---
OFFICE					
Financial Institutions	P	---	P	---	---
Government	P	P	P	---	---
Medical Clinics	P	P	P	---	---
Offices, Business & Professional	P	C	P	---	---
Regional & Corporate Headquarters	P	C	P	---	---
COMMERCIAL					
Agricultural Equipment Repair Shops	C	P	---	---	---
Agricultural/Nursery Supplies & Services	C	C	P	---	---
Alcoholic Beverage Outlets	C	C	C	---	---
Animal Care/Pet Hotels	P	P	C	---	---
Assembly & Entertainment	C	C	C	---	---
Automotive Parts & Accessory Sales	---	---	P	---	---
Automotive Fleet Storage	C	C	C	---	---
Automotive Service Stations	---	---	---	---	---
Automotive/Truck Repair - Major	C	P	---	---	---
Automotive/Truck Repair - Minor	P	P	C	---	---
Building & Site Maintenance Services	P	P	P	---	---
Building Contractor's Storage Yard	P	P	C	---	---
Building Material & Equipment Sales	P	---	P	---	---
Business Supply/Equip Sales/Rentals	C	C	C	---	---
Business Support Services	P	P	P	---	---
Child Care Facilities	C	---	C	---	---
Churches & Places of Religious Assembly	C	---	C	---	---
Communication Facilities, Antennas & Satellite Dishes	C	C	C	---	---
Consumer goods, Furniture, Appliances, Equipment Sales	C	---	P	---	---
Convenience Sales	C	---	P	---	---
Energy Generation & Distribution Facilities	C	C	C	---	---
Exhibit Halls & Convention Facilities	---	---	C	---	---
Fairgrounds	---	---	---	---	---
Food And Beverage Sales	A	A	P	---	---
Funeral & Mortuary Services	C	---	P	---	---

P= Permitted; C= Conditional Use Permit; A=Ancillary; --- = Prohibited					
USES	BUSINESS PARK ^{1,2}	INDUSTRIAL ³	MIXED-USE ⁴	P/R/OS	PUBLIC FACILITY
General Retail Establishments	---	---	P	---	---
Golf Courses, Driving Ranges and Pitch & Putt Courses	---	---	---	---	---
Grocery Stores	---	---	---	---	---
Health Club	A	A	C	---	---
Heavy Equipment Sales and Rentals with Outside Merchandising	C	C	C	---	---
Horticulture Nurseries & Greenhouses	C	P	---	---	---
Hospitals, Intermediate Care Facilities & Nursing Facilities	---	---	---	---	---
Hotel/Motel	---	---	---	---	---
Instructional Studios	P	P	P	---	---
Interpretive Center	P	P	P	---	---
Laundry Services	P	P	C	---	---
Maintenance & Repair	P	P	P	---	---
Major Transmission, Relay or Communications Switching Stations	P	P	C	---	---
Museums	---	---	P	---	---
Bar & Grill	---	---	C	---	---
Open Air Markets for the Sale of Agriculture-related Products & Flowers	C	---	C	---	---
Outdoor Commercial	---	---	C	---	---
Outpatient Medical Clinic	---	---	P	---	---
Parking Facilities as a <i>Primary Use</i>	C	C	C	---	---
Personal Services	---	---	P	---	---
Petroleum Products Storage	A	A	---	---	---
Pets & Pet Supplies	---	---	C	---	---
Private Clubs, Lodges & Fraternal Organizations	---	---	C	---	---
Radio & Television Studios	P	P	P	---	---
Recreational Facilities	A	A	C	---	---
Recycling Facilities (Outdoor Storage not to Exceed Building Area)	C	P	C	---	---
Repair Services	P	P	P	---	---
Restaurants (Fast Food)	---	---	C	---	---
Restaurant (Sit Down)	A	A	P	---	---
Social Service Institutions	P	P	P	---	---
Sundries, Pharmaceutical & Convenience Sales	---	---	P	---	---
Trade Schools	C	---	C	---	---
Vehicle, Boat and Trailer Sales	C	---	C	---	---
Vehicle Storage	C	C	C	---	---
Veterinary Clinics & Animal Hospitals	C	---	P	---	---
OTHER USES					
Parks and Recreational Facilities (Public)	---	---	---	P	---
Public Utility Stations, Yards, Wells and Similar Facilities, Excluding Offices	---	---	---	P	P

¹ Within the Business Park zone, a use permit is required for uses that provide outdoor storage in excess of 10% of the primary building gross square footage.

² A Parcel Delivery Terminal is allowed within the Industrial zone on parcels that are 50 acres or larger in size.

³ Within the Industrial zone, a use permit is required for uses that provide outdoor storage in excess of the primary building gross square footage.

⁴ A maximum of 25% of the Mixed-Use district gross square footage shall be allowed for retail uses.

* All uses subject to the density/intensity standards and additional criteria set forth in the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. Certain uses listed in this table may be limited in density/intensity or prohibited as a result of the Compatibility Plan standards.

3.5. DEVELOPMENT STANDARDS

The following standards establish the development criteria that shall apply in the Business Park, Industrial, and Mixed-Use districts of this Specific Plan. The entire Specific Plan Area is within the boundary of the March Air Reserve Base Land Use Compatibility Zones, which may limit building height, land uses, and FAR based upon the land use. Refer to the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan for additional information.

No building or portion thereof shall be erected, constructed, converted, established, altered, enlarged, nor shall any legal lot or premises be used unless the legal lot or premises and building comply with the following regulations and standards:

Table 3-2 Development Standards

Dimensions	Business Park	Industrial	Mixed-Use
Minimum Lot Size	1 acre	5 acres	1 acre
Street Frontage (minimum)	200 ft.	600 ft.	200 ft.
Lot Width (minimum)	200 ft.	600 ft.	200 ft.
Minimum Yards			
Front Yard Setback	20 ft.	20 ft.	20 ft.
Interior Side Yard Setback ¹	0 ft.	0 ft.	0 ft.
Street Side Yard Setback	20ft.	20 ft.	20 ft.
Rear Yard Setback ¹	0 ft.	0 ft.	0 ft.
Building Height – Max.	50 ft. ²	50 ft. ²	40 ft. ²
Screen Wall – Max.	14 ft. ³	14 ft. ³	14 ft. ³
Floor Area Ratio - Min. ⁴	0.45	0.50	0.35
Site Landscaping – Min.	10%	10%	20%

¹Structure shall be constructed on the property line or a minimum of 3 feet from the property line.

²Increased height up to 80 feet is permitted where all building setbacks meet or exceed the proposed building height, and subject to FAA Part 77 clearance.

³Screen wall height allowed to exceed maximum when required for noise attenuation or grade differences requiring additional screen height from public right-of-way.

⁴Based upon building net floor area, excluding stairwells and elevator shafts, equipment rooms, lofts or mezzanines of warehouse buildings use for equipment and conveyor systems, and floors below the first or ground floor, except when used for human habitation.

3.5.1. Lot Development

- 1) Two adjoining lots which have a common interior side or rear lot line may be developed with zero side yard setbacks on the common lot line, provided that the opposite side yard setback is not less than 30 feet.
- 2) Any construction or alteration of greater height than an imaginary surface extending upward and outward at a 100 to 1 slope from the nearest point of the runway (see FAR §77.13.2.i) will require the preparation of FAA Notice of Proposed Construction or Alteration (form 7460-1).
- 3) Construction of objects taller than 50 feet in the Height Caution Zone, will require review by the Airport Land Use Commission.

3.5.2. Landscaping

Landscaping design for development in the West Campus Upper Plateau Specific Plan shall be consistent with the West Campus Upper Plateau Design Guidelines. A 15-foot landscaped setback, measured from the public right-of-way, will be required for all front and side yards adjacent to public streets.

3.5.3. Driveway Widths and Locations

Driveway width and spacing shall be in conformance with the MIPA Development Code or as approved by the MIPA Civil Engineer.

3.5.4. Off-Street Loading Facilities

Loading or unloading facilities shall be so sized and located so that they do not require trucks to be located in required front or street side yards during loading and unloading activities.

3.5.5. Special Regulations

All uses, except for storage, loading, and outdoor work, shall be conducted entirely within an enclosed building. Outdoor work, storage of merchandise, material, and equipment is permitted in interior side or rear yards, provided the area is completely enclosed by sight obscuring walls, fences, or a combination thereof.

Fences and Walls: The design and location of fences and walls shall be the same as set forth in the West Campus Upper Plateau Design Guidelines (Chapter 4 herein).

In addition to the above, the following regulations apply:

- 1) Chain link fences shall not be used within 100 feet of a public right-of-way. Where used, chain link fences shall be vinyl coated.
- 2) Coiled, spiraled, or rolled fencing such as razor wire or concertina wire shall not be permitted.
- 3) All walls or fences within 100 feet of public right-of-way or visible from residential development shall be painted to be consistent with the project building colors (higher walls may be necessary to screen trucks and outdoor storage, consistent with the approved screening plan).



3.5.6. Off-street Parking

It is the intent of the West Campus Upper Plateau Specific Plan to provide minimum off-street parking requirements for passenger vehicles that appropriately accommodate parking demand. Furthermore, the MIPA General Plan represents in Transportation Element Policy 2.7 that on-street parking should be de-emphasized to both increase vehicle capacity and to accommodate bicycle access. As a result, *Table 3-3, Minimum Parking Space Requirements*, identifies minimum off-street parking requirements for general industrial, manufacturing, general warehousing, and distribution operations.

Table 3-3 Minimum Passenger Vehicle Parking Space Requirements

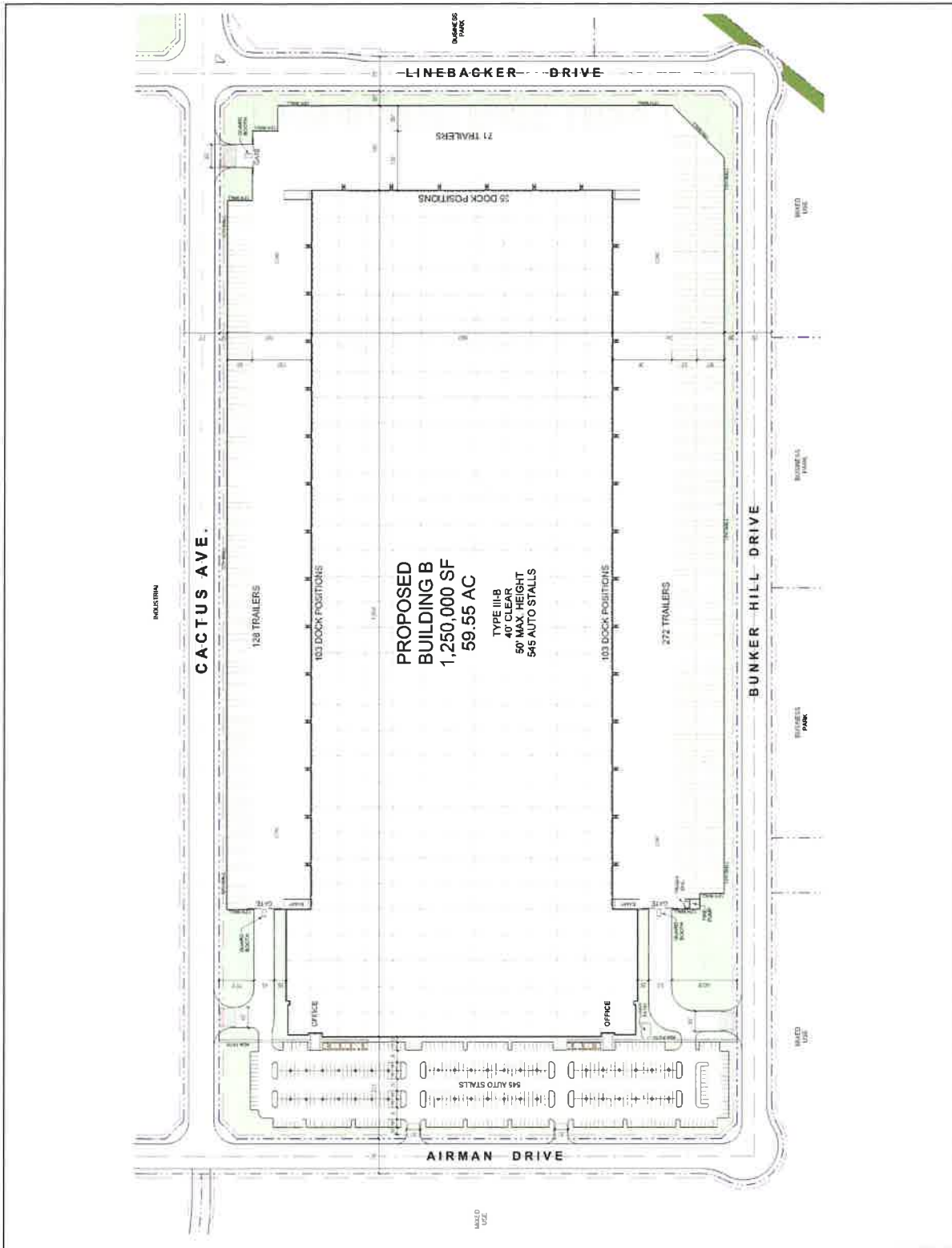
Use	Parking Spaces (per sq. ft. of Gross Floor Area) ¹
Light, Medium & Heavy Manufacturing	
First 10,000 sq. ft.	1 space per 500 sq. ft.
10,000-100,000 sq. ft.	1 space per 1,000 sq. ft.
Over 100,000 sq. ft.	1 space per 3,000 sq. ft.
Office space	1 space per 300 sq. ft.
Research & Development	1 space per 500 sq. ft.
Warehouse and Distribution	
First 20,000 sq. ft.	1 space per 1,000 sq. ft.
20,000 – 100,000 sq. ft.	1 space per 2,000 sq. ft.
Over 100,000 sq. ft.	1 space per 5,000 sq. ft.
Office space	1 space per 300 sq. ft.
Other Manufacturing and Warehouse Uses	
First 10,000 sq. ft.	1 space per 500 sq. ft.
Over 10,000 sq. ft.	1 space per 2,000 sq. ft.
Office space	1 space per 300 sq. ft.

¹Truck trailer parking spaces may count as passenger vehicle parking spaces if so noted on an implementing development's site plan

Uses not identified above shall adhere to the March Joint Powers Authority Development Code off-street parking requirements as represented in Section 9.11.040 of the March Joint Powers Authority Development Code. It is acknowledged that certain land uses will have unique parking characteristics, based on building utilization, workforce composition, and other considerations. In these cases, the MIPA Commission may review a use permit application to reduce required parking through a detailed parking analysis.

3.5.7. Conceptual Building Layouts

Figure 3-1, Conceptual Building Layout (Bldg B), and *Figure 3-2, Conceptual Building Layout (Bldg C)*, depict a preliminary layout of two sample, conceptual industrial buildings for the West Campus Upper Plateau Specific Plan. The figures show a conceptual first phase of development within the Specific Plan area and are representative of the development standards for the West Campus Upper Plateau Specific Plan.



Source(s): RGA

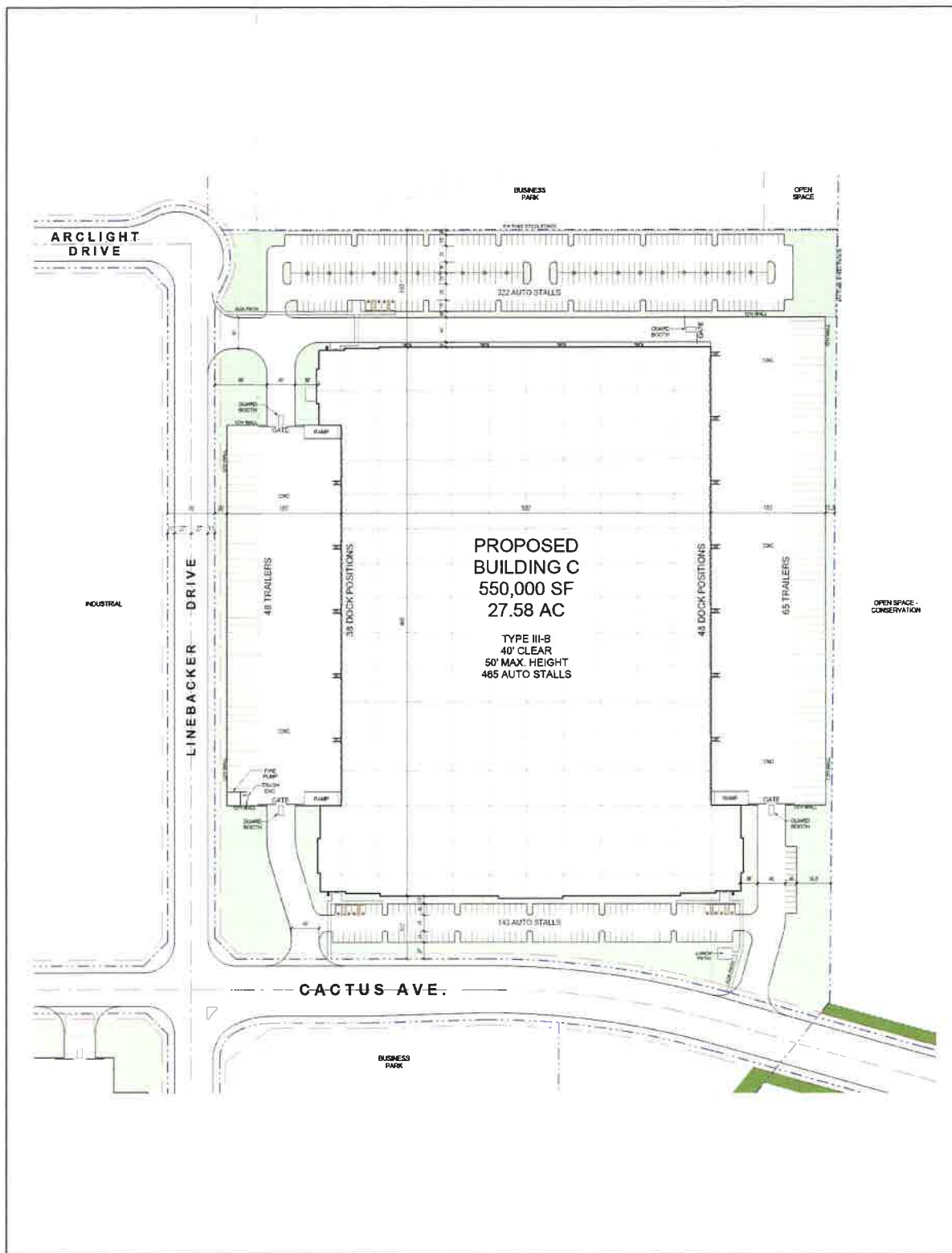
Figure 3-1



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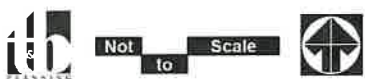


Conceptual Building Layouts (Bldg B)



Source(s): RGA

Figure 3-2



Conceptual Building Layouts (Bldg C)

Specific Plan No. XXX

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4 DESIGN GUIDELINES

4.1. PURPOSE AND INTENT

This chapter is intended to describe the quality and character of the built environment expected for the West Campus Upper Plateau Specific Plan. While design guidelines provide aesthetic direction, they're intended to be general and provide flexibility that allows creative expression during the design of future development projects. The guidelines provide criteria for architecture, energy efficiency, lighting, signage, and landscaping.

The visual identity of the West Campus Upper Plateau Specific Plan will be primarily represented through the hardscape, landscape, and signage elements of the various developments. The architectural design guidelines contained herein are represented in a manner that ensures consistent architectural expression throughout the Specific Plan Area, while allowing a degree of flexibility for individual projects.

The Design Guidelines objectives are as follows:

- To provide the MJPAA with the assurance that the West Campus Upper Plateau will develop in accordance with the quality and character described within this Specific Plan.
- To provide guidance to developers, builders, engineers, architects, landscape architects, and other professionals to achieve and maintain the desired design quality.
- To provide an aesthetic benchmark for MJPAA staff and all other decision makers in their review of the design of future implementing development projects in the Specific Plan Area.
- To provide guidelines that convey a contemporary aesthetic theme and character while allowing flexibility for practical application and creative expression.
- To encourage energy efficiency measures that can be incorporated into the site planning, design, and construction phases of the Specific Plan's implementation.
- To ensure that the Specific Plan implements the intent of the March Joint Powers Authority General Plan and Development Code.



4.2. DESIGN THEME

The West Campus Upper Plateau Specific Plan is a contemporary commerce center containing Industrial, Business Park, Mixed-Use, Parks and Open-Space, and Public Facility land uses. It will provide businesses easy access to an existing regional transportation network, be in proximity to workers, as well as proximity to the Ports of LA and Long Beach (approximately 65 miles to the southwest of the project site).

The design theme for the development areas of the specific plan features an overall contemporary aesthetic, which provides architectural styling with attractive detailing, steel accents, a light-toned color palette, and timeless features. Sign designs are to be modern, landscaping colorful and drought-tolerant, project lighting focused and directed, and design features intended to lower energy use demands while encouraging efficient building operations. Development areas are surrounded by permanently preserved open space.

4.3. ARCHITECTURAL DESIGN GUIDELINES

The architectural style of the West Campus Upper Plateau Specific Plan emphasizes building massing over structural articulation. Buildings are characterized by simple and distinct cubic masses with interlocking wall planes, colors, and materials that work together to create visual appeal. Exterior building colors are light and gray tones with use of stone, glass, or steel materials to establish focal points, such as around building entrances and near outdoor gathering spaces. Additionally, architectural designs may mix colors, materials, and textures to articulate façades and create visual appeal.

Design elements have been selected to be compatible in character, massing, and materials that result in a clean and contemporary feel. Individual creativity and identity are encouraged, but design integrity and overall design compatibility must be maintained among all buildings and planning areas to reinforce a unified image and campus-like setting within the Specific Plan Area.



4.3.1. Building Form

Building form is one of the primary elements of architecture. Numerous design aspects, including shape, mass (size), scale, proportion, and articulation, are elements of a building's "form." Building forms are especially important for building façades that are visible from Cactus Avenue at the easterly entrance into the industrial building campus area and along Barton Street.

The following guidelines apply to buildings within the West Campus Upper Plateau Specific Plan Area to ensure that development is visually consistent, appealing, and inviting. Note that building faces not visible from public roads, or publicly accessible viewing areas, are not required to adhere to the below building form guidelines.

- a. Use simple geometric shapes as the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Avoid arbitrary, complicated building forms.
- b. Long horizontal wall planes visible from a public street should include periodic changes in exterior building materials, color, decorative accents, and/or articulated features.
- c. Modulation, shift and variation of building masses between adjacent buildings visible from public streets is encouraged.
- d. Main pedestrian entrances to buildings (with the exception of service doors and emergency exit doors) should be obvious through changes in massing, color, and/or building materials.
- e. Pedestrian and ground-level building entries intended for visitor use should be recessed or covered by architectural projections, roofs, or arcades in order to provide shade and visual relief.
- f. Architectural and trim detailing on building façades should be clean, simplistic, and not overly complicated.
- g. Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.

4.3.2. Building Materials, Colors and Textures

Building materials and colors play a key role in creating a clean, contemporary visual environment. Therefore, the selected exterior materials, colors, and textures should complement one another throughout the West Campus Upper Plateau Specific Plan. Subtle variations are encouraged to provide visual interest.

- a. Appropriate primary exterior building materials include concrete and similar materials, as well as tilt-up panels. Primary materials should be accented by secondary materials including but not limited to natural or fabricated stone, Fire resistant wood siding (horizontal or vertical), and metal.
- b. Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Use of overly extraneous "themed" detailing, like oversized or excessive foam cornice caps, foam molding and window detailing is discouraged.
- c. Material changes should occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
- d. Primary exterior building colors should be light and gray tones. Darker and/or more vibrant accent colors may be provided in focal point areas, such as around building entrances and near outdoor gathering spaces. Use of colors other than light and gray tones are



- allowed specific to branding and being limited to not more than 10% of the exterior building surface area.
- e. Bright primary colors, garish use of color, and arbitrary patterns or stripes that will clash with this color palette are discouraged, except in signage logos.
 - f. Exposed downspouts, service doors and mechanical screen colors shall be the same color as the adjacent wall.
 - g. Any color banding should be vertical and not horizontal across the length of a building to deemphasize the building length and width. Short horizontal color bands are acceptable but long bands across the entire length or width of a building are discouraged.

4.3.3. Windows and Doors

The patterns of window and door openings shall correspond with the overall rhythm of the building and should be consistent in form, pattern, and color within each planning area. Guidelines for windows and doors within the West Campus Upper Plateau Specific Plan are as follows:

- a. When possible, the positioning of doors and windows on individual building façades should occur in a symmetrical and repetitive pattern to create continuity.
- b. Material or color banding shall be limited in horizontal dimension in order to de-emphasize building length.
- c. Window styles and trims shall be of similar form and finished in a consistent color on each building.
- d. Unfinished/untreated metal window or door frames are prohibited. Clear silver anodized frames are allowed.
- e. Glass shall be clear or colored with subtle reflectiveness. Silver/reflective glass is prohibited.
- f. Pedestrian entry doors to buildings shall be clearly defined by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways (except emergency exit and service doors) and tacked-on entry alcoves are discouraged.



4.4. SITE FEATURES

Several key components play a critical role in the overall project design. The design of loading dock areas, placement of equipment, and screen wall and fence placement are all integral to operations and critically important to overall site aesthetics as well.

4.4.1. Walls and Fences

Due to the nature of the land uses and substantial amount of open space and conservation area identified for the West Campus Upper Plateau Specific Plan, a tube steel fencing design is preferred for the fencing to be placed along the conservation/open space interface boundaries.

Additionally, four split rail fencing designs are identified for use along the Barton Street multi-purpose trail. These fencing types are represented in *Figure 4-1, Fence Details*.

Additionally, fences and walls are anticipated to be proposed in conjunction with the development of the individual project sites. Along building site perimeters and interior to building sites, fences and walls will be necessary. The final locations and details of these fences and walls will be determined when project sites and buildings are designed and oriented during implementation of the Specific Plan.

Screen wall may be provided around the perimeters of individual buildings sites and around loading and dock areas, trailer parking areas, and parking lots to screen on-site industrial uses from public views and public roads. The maximum height for these walls is expected to not exceed 14 feet (unless acoustical attenuation or grade differences from truck dock areas to public right-of-way necessitates greater height) and include landscaping in association with the wall when facing or viewed from a public street. In addition, landscaping within roadway rights-of-way and outside of rights-of-way serve as additional screening between on-site land uses and public roads.

The following guidelines for walls and fencing will ensure that these features complement the overall design theme of the West Campus Upper Plateau Specific Plan, are attractive from public viewing areas, scaled appropriately, durable, and integrated consistently within the Specific Plan Area.

- a. Freestanding walls and fences should not exceed a height of 14 feet, measured from the base of the wall/fence to the top of the wall/fence.
- b. Landscaping may be used for visual screening instead of walls and fences in locations where a solid physical barrier is not needed.
- c. Walls and fences in public view should be built with attractive, durable materials.
- d. Chain-link fencing is not permitted within 100 feet of a public right-of-way and shall be vinyl coated when used.
- e. Along public street frontages, long expanses of freestanding wall surfaces should be offset and/or architecturally treated to prevent monotony. Techniques to accomplish this may include, but are not limited to openings, material changes, pilasters and posts, and staggered sections.
- f. Wall and fencing materials shall be compatible with the design characteristics of the primary building for the site in which the wall or fence is located.

CONSERVATION / OPEN SPACE
PERIMETER FENCING



Tube Steel Fencing

SPLIT RAIL FENCING ALONG BARTON STREET
MULTI-PURPOSE TRAIL



Textured Matte-Finish Vinyl



Tube Steel Fencing



Textured Concrete

Figure 4-1



Not to Scale

4.4.2. Truck Courts and Loading Docks

- a. Loading doors, service docks, and equipment areas should be oriented or screened to reduce visibility from public roads and publicly accessible locations within the West Campus Upper Plateau Specific Plan. Screening may be accomplished with solid walls or fences that are compatible with the architectural expression of the building. Screening may also be accomplished by landscaping.
- b. No loading or unloading activity is permitted to take place from public streets/view.
- c. Adequate queuing distance should be provided on-site in front of security gates to avoid the circumstance of trucks stacking on public streets waiting to enter at gates.
- d. Truck and service vehicle entries should be designed to provide clear and convenient access to truck courts and loading areas such that passenger vehicle, pedestrian, and bicycle circulation is not adversely affected by truck movements.
- e. Loading bays that are utilized by refrigerated trailers should have dock seals and be equipped with plug-in electrical outlets.
- f. Conduit should be installed in truck courts in logical locations that would allow for the future installation of charging stations for electric trucks, in anticipation of this technology becoming available.

4.4.3. Ground or Wall-Mounted Equipment

- a. Ground-mounted equipment, including but not limited to mechanical or electrical equipment, emergency generators, boilers, storage tanks, risers, and electrical conduits, should be screened from public viewing areas including adjacent public roads. Screening may be accomplished with solid walls, or landscaping.
- b. Electrical equipment rooms should be located within the building envelope. Pop-outs or shed-like additions are discouraged.
- c. Wall-mounted items, such as electrical panels, should not be located on the building façade facing adjacent public roads/views. Wall-mounted items should be screened or incorporated into the architectural elements of the building so as not to be visually apparent from the street or other public areas.

4.4.4. Rooftop Equipment

- a. Rooftop equipment, including but not limited to mechanical equipment, electrical equipment, storage tanks, wireless telecommunication facilities, satellite dishes, vents, exhaust fans, smoke hatches, and mechanical ducts, shall be screened by rooftop screens or parapet walls so as not to be visible by the public.
- b. Integrate rooftop screens (i.e. parapet walls) into the architecture of the main building. Wood finished rooftop screens are prohibited.
- c. Building rooftops should be designed to support the future installation of solar panels. Solar rooftop systems shall be reviewed and approved by the March JPA with full consideration given to any potential glare impacts upon aviation operations.
- d. Roof access (via roof ladders or other means) must be located interior to the building.

4.4.5. Trash Enclosures

- a. All outdoor refuse containers shall be screened within a permanent, lockable, and durable enclosure and should be oriented to not be visible from public roads/views. The trash enclosure design shall reflect the architectural style of adjacent buildings and use similar, high-quality materials.
- b. All outdoor trash enclosures shall be constructed with solid roofs to prevent exposure of dumpster contents to rainfall and prevent polluted stormwater runoff from these structures.
- c. Refuse collection areas shall be located behind or to the side of buildings, away from the building's main entrance and public view.
- d. Buildings shall be designed to meet all applicable state, regional and local government solid waste disposal requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

4.4.6. Outdoor Lighting

Outdoor lighting within the West Campus Upper Plateau Specific Plan is an essential architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting on private property within the Specific Plan should adhere to the following:

- a. Minimize glare and "spill over" light onto public streets, adjacent properties, and Conservation Area by using downward-directed lights and/or cutoff devices on outdoor lighting fixtures, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, parking, loading, unloading, and similar areas. Where desired, illuminate trees and other landscape features by concealed uplight fixtures. Limit light spillover or trespass to one-quarter foot-candle or less, measured from within five feet of any adjacent property line.
- b. Lighting fixtures should have a similar design, materials, fixture color, and light color. Use of LED lighting is encouraged.
- c. Lights should be unbreakable plastic, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures.
- d. Neon and similar types of lighting are prohibited in all areas within the Specific Plan Area.
- e. Locate all electrical meter pedestals and light switch/control equipment in areas with minimum public visibility or screen them with appropriate plant materials.



- f. Illuminate parking lots, loading dock areas, pedestrian walkways, building entrances, and public sidewalks to the level necessary for building operation and security reasons. Dimmers and motion detectors are permitted.
- g. Along sidewalks and walkways, the use of low mounted fixtures (ground or bollard height), which reinforce the pedestrian-scaled, are encouraged.
- h. Use exterior lights to accent entrances, plazas, activity areas, and special features.
- i. High-Pressure Sodium (HPS) light fixtures are prohibited for site lighting.
- j. Lighting is prohibited that could be mistaken for airport lighting or that would create glare in the eyes of pilots of aircraft using the nearby March Air Reserve Base.

4.4.7. Signage Guidelines

Signage within the Specific Plan Area serves a variety of purposes. Signs will identify the West Campus Upper Plateau Specific Plan and its building occupants and ensure the efficient circulation of vehicle traffic within the site by identifying vehicular entry points and directing vehicles to their on-site destinations. Also, signage will enhance the vehicular and pedestrian experience through the design of wayfinding components: directories, directional signage, and destination identifiers.

As such, clear, concise, and easy-to-understand signage that is also visually appealing is vitally important for a positive worker and visitor experience. General signage design standards are as follows:

- a. Signage in association with development projects should be compatible with and complementary to the building's exterior materials, colors, and finishes.
- b. The dimensions and shape of free-standing signs and sign panels or elements mounted on building façades or marquees shall be scaled proportionately to the architecture.
- c. All signs shall be contained within the parcel to which it is applicable and shall be so oriented as to preclude hazardous obstructions to person and/or vision of pedestrians and/or vehicle operators.
- d. Building occupant identification signage shall be in keeping with the character established for the Specific Plan with variations allowed to accommodate individual user identifies/corporate branding standards.
- e. All signs are expected to be of the highest quality to pass eye-level examination and scrutiny.
- f. Prohibited sign components include the following:
 - i. Letters with exposed fastening and unfinished edges (unless architecturally consistent);
 - ii. Paper, cardboard, Styrofoam or untreated cloth;
 - iii. Visible moving parts or simulated moving parts by means of fluttering, rotation, or reflecting devices; and
 - iv. Flashing and strobing.

- g. All conductors, transformers, cabinets, housing, and other equipment for the illumination of signs shall be concealed and/or incorporated into the building architecture.
- h. Signs shall be constructed to not have exposed wiring, raceways, ballasts, conduit, transformers, or the like.
- i. Direction signs may be located at any vehicular or pedestrian decision point.
- j. Vehicular direction signs shall clearly direct to destination anchors within the West Campus Upper Plateau Specific Plan, on-site parking areas, and truck routes.
- k. Vehicular direction signs shall be consistent in size, shape, and design throughout the West Campus Upper Plateau Specific Plan.
- l. Typography on vehicular direction signs should be legible and have enough contrast to be read from an appropriate windshield viewing distance.
- m. Vehicular direction signs shall incorporate reflective vinyl copy for night-time illumination.
- n. All traffic control signs, whether on public or private property, shall conform to the California Manual on Uniform Traffic Control Devices (MUTCD).

4.5. LANDSCAPE DESIGN GUIDELINES

The West Campus Upper Plateau Specific Plan Landscape Design Guidelines establish landscape principles and standards that apply to all planning areas within the Specific Plan. The intent is to ensure that plant materials, monuments and entries, streetscapes, and other features are compatible with the overall design theme and that all implementing development projects are united under a common landscape design vocabulary. These Landscape Design Guidelines, when taken with the companion Architectural Design Guidelines provided herein, establish an identity for the Specific Plan that is contemporary, visually appealing, and contextually sensitive to the surrounding area.

Although a great deal of design information is presented herein, these Guidelines are not intended to establish a set of rigid landscaping requirements. It is recognized that these Guidelines will occasionally need to have flexibility to meet certain parcel-specific or user-identity requirements. As such, these Guidelines are intended to be flexible, and are subject to modification over time. However, any deviations from these Landscape Guidelines are to be in keeping with the spirit of the core elements of the overall theme described herein to ensure a cohesive and unified landscape concept.

The landscaping plan serves the dual purpose of providing visual appeal while also being sensitive to the environment and climate by using drought-tolerant materials. Landscaping occurs throughout the developed areas of the Specific Plan, being most prominent at main entry point, along roadways, and at building entrances and in passenger vehicle parking lots.

Entry Treatments welcome employees and visitors to the West Campus Upper Plateau Specific Plan. A major entry treatment will be provided on Cactus Avenue at the entrance to the developed project area. Secondary entry treatments will be provided on Barton Street near the northern and southern entry points to the Specific Plan Area.

Streetscape landscaping is proposed for all streets within the Specific Plan boundary, presenting a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists.

4.5.1. Plant Palette

The plant palette for the West Campus Upper Plateau Specific Plan includes colorful shrubs and groundcovers, ornamental grasses and succulents, and evergreen and deciduous trees that are commonly used throughout Southern California and the Inland Empire region, complementing the Specific Plan's design theme and setting. Many of the plant materials are water-efficient species native to the region or naturalized to the arid Southern California climate.

A list of plant materials approved for use in the Specific Plan is provided for in *Appendix B – Landscape Plant Palette*. The plants listed establish a base palette for the landscape design. Other similar plant materials may be substituted for species listed in Appendix B, provided the alternative plants are drought-tolerant and complement the Specific Plan design theme.

To prevent or reduce wildlife hazards to aircraft operations in association with the March Air Reserve Base, plant palette priority shall be given to plants listed in the Riverside County Airport Land Use Commission's "Landscaping Near Airports" brochure, which can be found on the Commission's website (www.rcaluc.org/Resources). Additionally, the general planting guidelines represented in this brochure shall also be considered and incorporated into the landscape design of projects within the West Campus Upper Plateau Specific Plan.

4.5.2. Irrigation

The following general irrigation concepts shall be considered in the design and installation of irrigation systems within the West Campus Upper Plateau Specific Plan:

- a. All landscaped areas should be equipped with a permanent, automatic, underground irrigation system. Drip systems are encouraged in all areas needing irrigation.
- b. Irrigation systems should be designed to apply water slowly, allowing plants to be deep soaked and to reduce run-off.
- c. Connect the irrigation system to the recycled water conveyance system, when possible.
- d. "Pop-up" type sprinkler heads may be used adjacent to all walks, drives, curbs (car overhangs), parking areas and public right-of-way but must be designed to prevent all run-off and overspray.
- e. The design of irrigation systems, particularly the location of controller boxes, valves, and other above-ground equipment (e.g., backflow prevention devices), shall be incorporated into the overall landscaping design. Where aboveground equipment is provided, it should be screened or not placed in public view.

4.5.3. Streetscapes

Streetscape landscaping plays an important role in helping to create a sense of place. Streetscapes serve functional purposes, including screening undesirable views from public view. Within the West Campus Upper Plateau Specific Plan, streetscapes are planted with a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists.

Figure 4-2, *Exhibit Key Map*, provides the location of landscape improvements, and the following provides conceptual streetscape landscape treatment details within Specific Plan Area:

a. *Cactus Avenue East Streetscape:*

Within the Specific Plan boundary, Cactus Avenue East has two street designs, though both utilize the same landscape plant palette as shown in *Figure 4-3, Cactus Avenue East Plant Palette*. The easterly streetscape segment runs between the eastern edge of the Specific Plan through the open space conservation area into the industrial campus. This segment consists of 4.5-foot-wide landscape parkways on both sides of the street. Parkway design includes a curb-adjacent park strip planted with deciduous or evergreen trees, and low flowering groundcovers and succulents, as well as a 6-foot-wide sidewalk.

The second segment of Cactus Avenue is entirely within the industrial campus, between Airman and Linebacker Drive. Parkway design includes a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 6-foot-wide sidewalk. Evergreen and deciduous trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade.

b. *Barton Street Streetscape:*

The Barton Street streetscape design includes a landscape plant palette as shown in *Figure 4-4, Barton Street Plant Palette*. Parkway design includes curb-adjacent 6-foot-wide sidewalks on both sides. A 10-foot-wide multi-purpose trail is provided along the western side of Barton Street allowing for passive recreational opportunities and connecting neighboring residential areas to the park site and open space area. A 5-foot-wide landscape area is designed between the sidewalk and multi-purposed trail, which will be planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents. A similar landscape treatment is designed along the east side of the street between the sidewalk and edge of right-of-way.

c. *Brown Avenue Streetscape:*

The Brown Avenue streetscape design includes a landscape plan palette represented in *Figure 4-5, Brown Avenue Plant Palette*. Parkway design includes a curb-adjacent 6-foot-wide sidewalks on both sides of the street. The remaining area between the sidewalk and edge of right-of-way will be planted with deciduous or evergreen trees, and low flowering groundcovers and succulents.

d. *Interior Streets:*

Interior streets include those within the industrial campus area, which are Arclight Drive, Cactus Avenue, Bunker Hill Drive, Airman Drive, and Linebacker Drive. Two plant palettes apply to these streets and apply depending upon north-south or east-west orientation. Thus, Arclight Drive, Cactus Avenue and Bunker Hill Drive are represented by the east-west (EW) plant palette represented in *Figure 4-6, Interior Street EW Plant Palette*, while Airman Drive and Linebacker Drive are represented by the north-south (NS) plant palette in *Figure 4-7, Interior Street NS Plant Palette*.

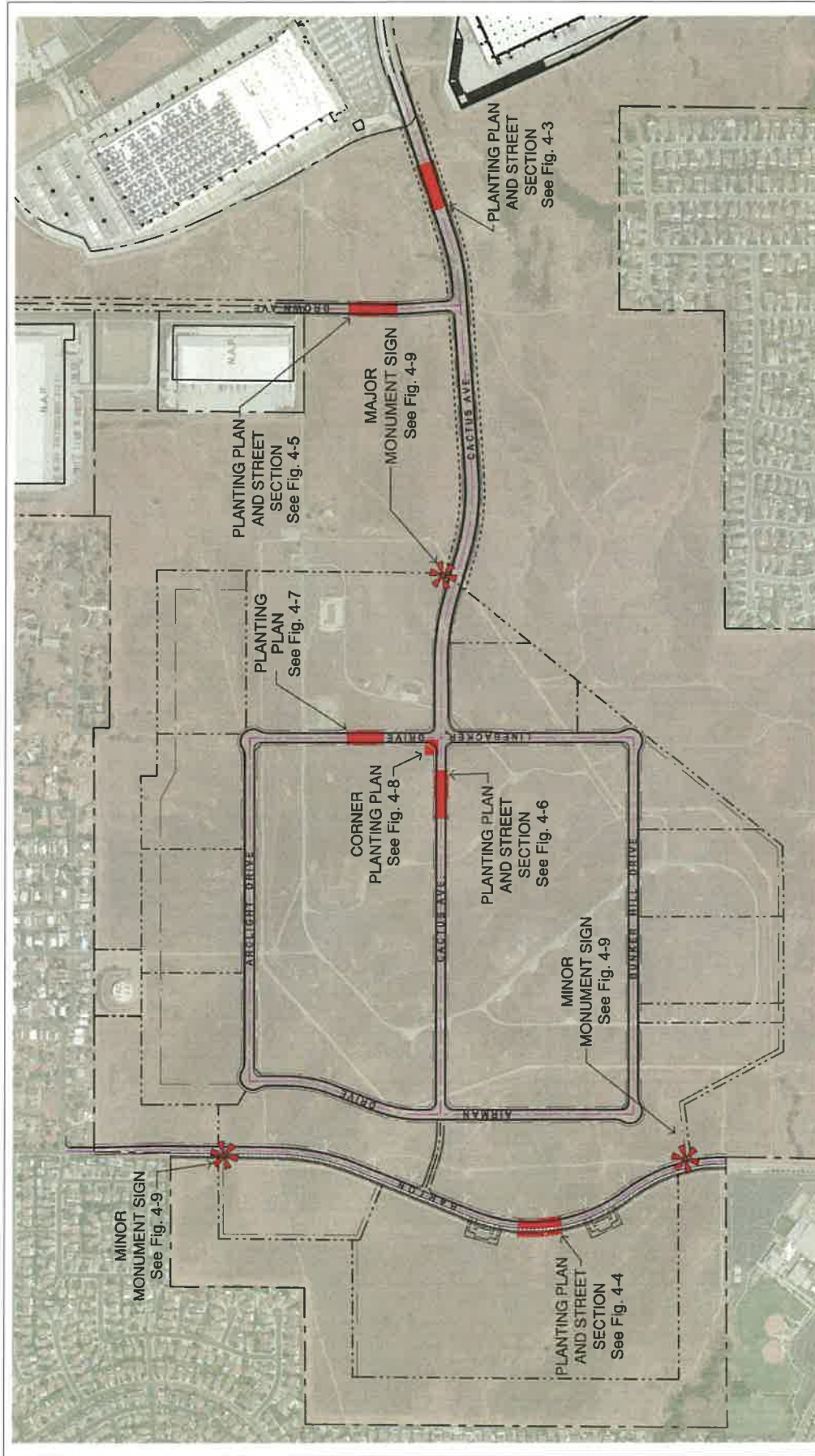


Figure 4-2

Source: HATCH & ASSOCIATES, INC. (01-14-2022)



Specific Plan No. XXX

TREES



Tuscarora Grape Myrtle
Lagerstroemia x 'Tuscarora'
Deciduous



Aleppo Pine
Pinus halepensis
Evergreen

SHRUBS



Pigeon Point Coyote Bush
Baccharis pilularis 'Pigeon Point'



Little John Callistemon
Callistemon viminalis 'Little John'



Magenta Rock Rose
Cistus x pulverulentus 'Sunset'



Purple-leaved Hop-bush
Dodonaea viscosa 'Purpurea'



Atlas fescue
Festuca mairei



Pink Creeping Myoporium
Myoporum parvifolium 'Pink'



Autumn Sage
Salvia greggii

Figure 4-3



Specific Plan No. XXX

TREES



Goldenrain Tree
Koelreuteria paniculata
Deciduous

SHRUBS



Blue Grama Grass
Bouteloua gracilis



Creeping Wild Rye
Elymus triticoides



Atlas fescue
Festuca mairei



Gray Rush
Juncus patens



Bush Monkey Flower
Mimulus aurantiacus



Deer Grass
Muhlenbergia rigens



Black Sage
Salvia mellifera



White Sage
Salvia apina

Figure 4-4



Specific Plan No. XXX

Barton Street Plant Palette

Page 4-15



TREES

London Plane Tree
Platanus x acerifolia



SHRUBS

Blonde Ambition Blue Grama Grass
Bouteloua gracilis 'Blonde Ambition'



Creeping Wild Rye
Elymus triticoides



California Gray Rush
Juncus patens



Pink Creeping Myoporum
Myoporum parvifolium 'Pink'



Autumn Sage
Salvia greggii

Figure 4-5



Specific Plan No. XXX

Brown Avenue Plant Palette

Page 4-16

TREES



Saratoga Laurel
Laurus nobilis 'Saratoga'

SHRUBS



Blonde Ambition Blue Grama Grass
Bouteloua gracilis 'Blonde Ambition'



Magenta Rock Rose
Cistus x pulverulentus 'Sunset'



Creeping Wild Rye
Elymus triticoides



California Gray Rush
Juncus patens



Dwarf Olive
Olea europea 'Little Ollie'



Dwarf Red Fountain Grass
Pennisetum s. 'Eaton Canyon'



Blue Chalk Stick
Senecio serpens

Figure 4-6



Specific Plan No. XXX

Interior Street EW Plant Palette

TREES



Raywood Ash
Fraxinus oxycarpa 'Raywood'
Deciduous

SHRUBS



Blue Grama Grass
Bouteloua gracilis



Little Rev Flax Lily
Dianella revoluta 'Little Rev'



Creeping Wild Rye
Elymus triticoides



California Gray Rush
Juncus patens



Otto Quast Spanish Lavender
Lavandula stoechas 'Otto Quast'



Texas Ranger
Leucophyllum f. 'Green Cloud'



Little Bunny Miniature Fountain Grass
Pennisetum alopecuroides 'Little Bunny'

Figure 4-7



Specific Plan No. XXX

4.5.4. Entries and Monuments

The West Campus Upper Plateau Specific Plan provides for a standard street corner planting design and two-tiered hierarchy of monument signage. The entry and corner treatments are designed to provide distinctive visual statements and encourage the Specific Plan's contemporary aesthetic. All hardscape and landscape features at entry and monument locations shall provide adequate line of sight for motorists. Monumentation shall not be located within the public street right-of-way.

The typical corner landscape planting design represented in *Figure 4-8, Typical Corner Planting*, is designed to be a prominent representation of the quality and distinctiveness of the West Campus Upper Plateau Specific Plan and reinforce the general architecture and landscape theme. Entry and corner treatments should provide design flexibility to respond to physical contexts and unique circumstances of specific tenants and may differ slightly from that represented herein. However, all entry and corner treatments within the Specific Plan shall be consistent with the overall theme and character. *Figure 4-9, Monument Signage*, provides examples of typical major and minor monuments.

a. Major Monument:

A single Major Entry Monument is to be located on the north side of Cactus Avenue at the entrance to the industrial campus, providing entry identity for those entering the campus. The monument sign will stand approximately six feet at its highest point and be approximately 30 feet in width, including wing walls. Sign design is contemporary theme and will include finish and colors complementary to the overall design theme of the Specific Plan. Associated landscaping will be consistent with the Cactus Avenue planting plan, ensuring that plantings provide appropriate visual draw and support to the entry monument sign.

b. Minor Monument:

Two minor monuments are to be located on Barton Street with one placed on the east side of Barton Street just north of and inside the Specific Plan boundary and the other placed on the west side of Barton Street just south of and inside the boundary. Each monument sign is to identify arrival into the Specific Plan Area. Monument sign design will be generally consistent with the major monument sign, being contemporary in appearance and of finish and colors complementary to the overall design theme of the Specific Plan. The signs will stand approximately five feet in height and 12 feet in width. Associated landscaping will be consistent with the Barton Street planting plan, ensuring that plantings provide appropriate visual draw and support to the entry monument sign.

TREES



Western Redbud
Cercis occidentalis



Holly Oak
Quercus ilex



California Gray Rush
Juncus patens



Blonde Ambition Blue Grama Grass
Bouteloua gracilis 'Blonde Ambition'



Sageleaf Rockrose
Cistus salvifolius 'prostratus'



Creeping Wild Rye
Elymus triticoides



Texas Yucca - Yellow
Hesperaloe parviflora 'Yellow'



Blonde Ambition Blue Grama Grass
Bouteloua gracilis 'Blonde Ambition'

PLANTING LEGEND (WUCOLS REGION 4)

SYMBOL	ABBREVIATION	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS	WATER USE
TREES						
○	CERCIS	<i>occidentalis</i>	WESTERN REDBUD	36" BOX	MULTI-TRUNK	LOW/30%
○	QUERCUS	<i>ilex</i>	HOLLY OAK	36" BOX	STANDARD	LOW/30%
SHRUBS						
○	CIS SAL	<i>CISTUS salvifolius 'prostratus'</i>	SAGELEAF ROCKROSE	5 GALLON MIN.		LOW/20%
○	HES PAR	<i>HESPERALOE parviflora 'Yellow'</i>	TEXAS YUCCA- YELLOW	5 GALLON MIN.		LOW/20%
DRAINAGE SWALE GRASSES						
■	BOU GRA	<i>BOUTELOUA gracilis</i>	BLUE GRAMA GRASS	5 GALLON		LOW/20%
■	ELY TRI	<i>ELYMUS triticoides</i>	CREEPING WILD RYE	5 GALLON		LOW/20%
■	JUN PAT	<i>JUNCUS patens</i>	GRAY RUSH	5 GALLON		LOW/20%

NOTE

1. SALVAGE BUILDERS FOUND DURING CONSTRUCTION TO USE AS DECORATIVE ELEMENTS AT ALL CORNER PLANTERS.

PLAN VIEW

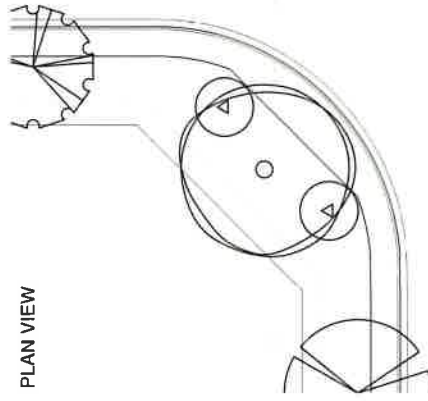
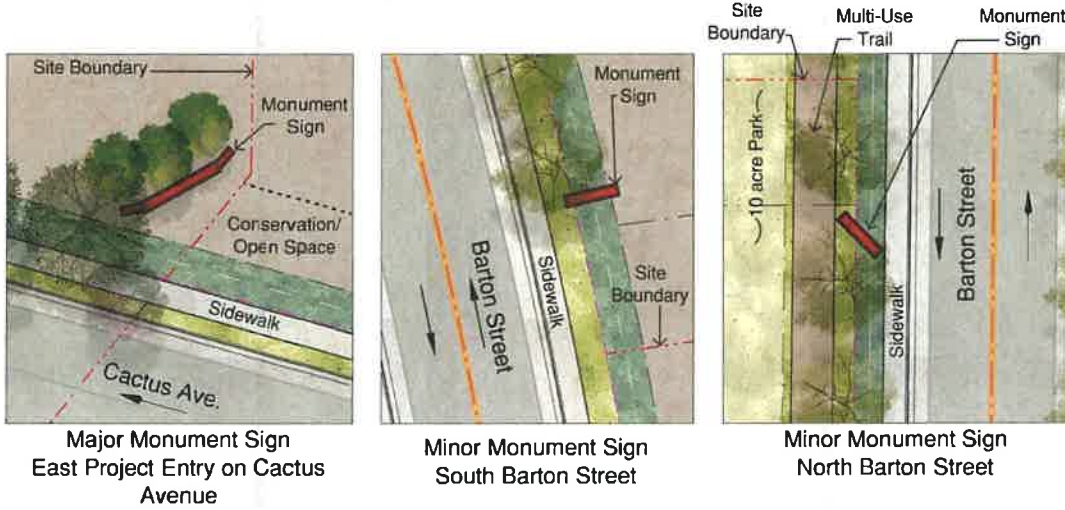


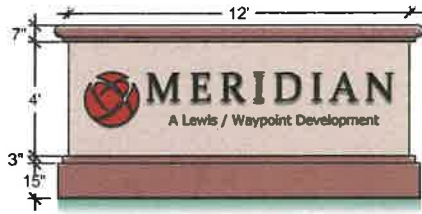
Figure 4-8



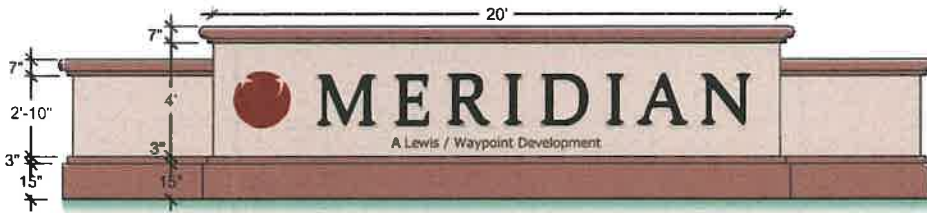
Typical Corner Planting



MONUMENT SIGN EXAMPLES



12' Long Minor Monument Signs at Barton Street, North and South Entries



20' Long Major Monument Sign with Wing Walls at Cactus Avenue Site Entry

Figure 4-9



Not to Scale

4.5.5. Open Space Areas

Most of the land within the West Campus Upper Plateau Specific Plan is identified as open space and open space-conservation. With exception to passive use activity proposed within the open space area west of Barton Street, the open space areas are primarily intended to maintain landscaping and an overall aesthetic consistent with the current undeveloped environment.

The passive use activity to be provided for and encouraged in the open space area west of Barton Street is of benefit to the neighboring residents, employees, and visitors to the Specific Plan. A limited number of amenities are to be provided, namely consisting of two trail parking areas and meandering decomposed granite or native material walkways and trails, which is further described in Section 2.6.

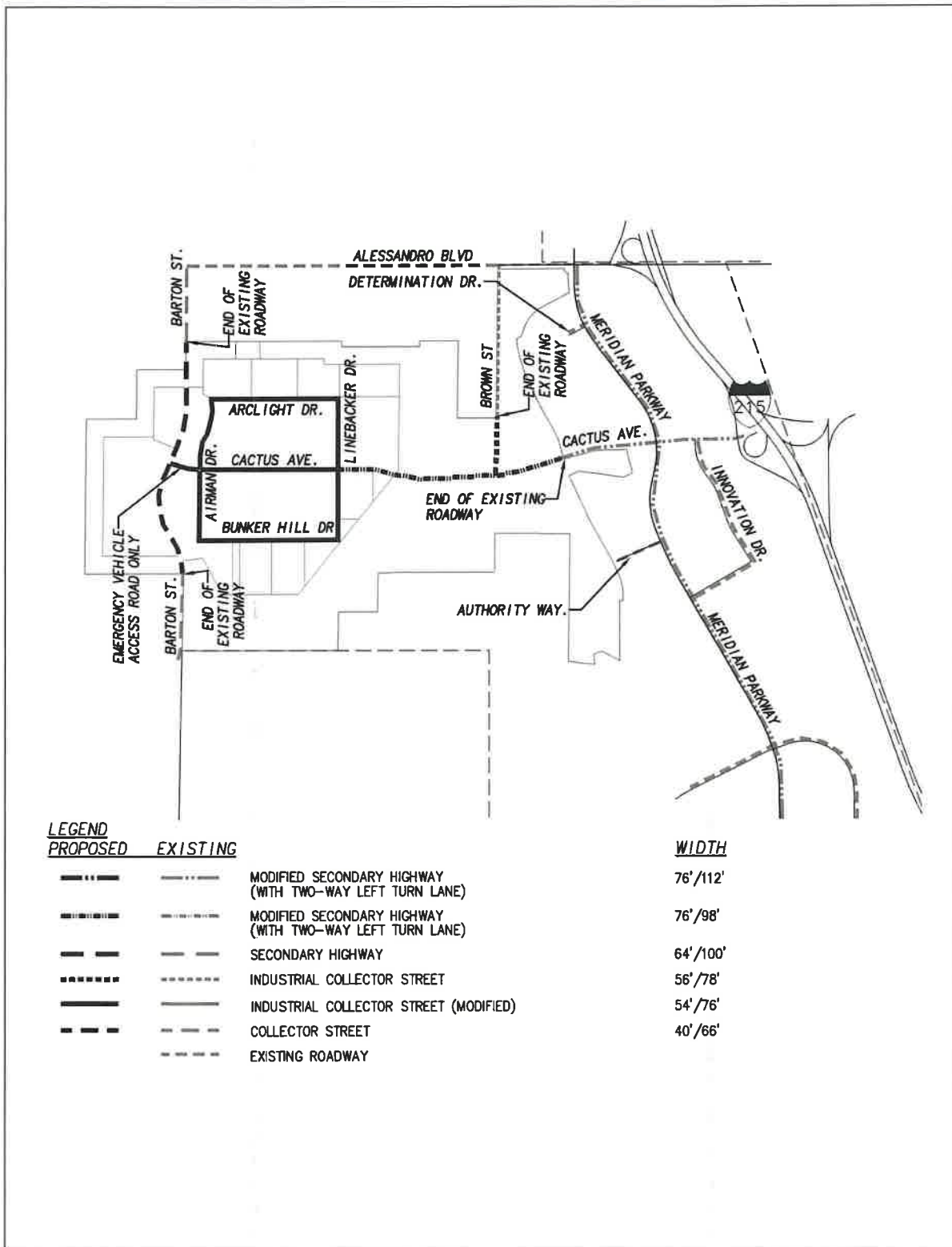
5 TRANSPORTATION

5.1 TRAFFIC CIRCULATION PLAN

The West Campus Upper Plateau Circulation Plan provides convenient, direct, and safe access for employees, visitors, and goods movement to and from the Specific Plan Area. This is achieved through a roadway network consisting of a hierarchy of local, collector and arterial streets providing access to and from the parcels comprising the West Campus Upper Plateau Specific Plan Area. The internal street network will consist of public roadways maintained by the County of Riverside. Off-site transportation improvements will be provided as deemed necessary by the MJPA to ensure there is sufficient capacity to accommodate future traffic. Improvements associated with each development phase will be assured to the satisfaction of the MJPA prior to the occupancy of that phase.

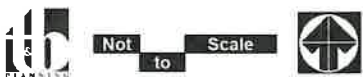
5.1.1 VEHICULAR CIRCULATION

Access to and from the West Campus Upper Plateau Specific Plan Area will be provided via three roadways: Cactus Avenue, Brown Avenue and Barton Street. This is depicted in *Figure 5-1, Circulation Plan*. Cactus Avenue will serve as the primary serving roadway, connecting with I-215 located approximately 1 mile east of the Specific Plan boundary. Brown Avenue will serve as a secondary access to the industrial center, connecting with Alessandro Boulevard to the north. Barton Street will be restricted to providing access to the westerly segment of the Specific Plan Area only to the park, open space and mixed-use areas on the east and west sides of Barton Street. Barton Street will connect with the existing street network to the north and south of the Specific Plan boundary, serving as a local connector pursuant to the City of Riverside General Plan Circulation Element. In order to prevent truck trips from impacting neighboring residential streets, Barton Street will not have any connection with Cactus Avenue, strategically preventing direct vehicular access to the industrial campus area, except for emergency the inclusion of an emergency vehicle access road between Cactus Avenue and Barton Street.



Source(s):

Figure 5-1



Circulation Plan

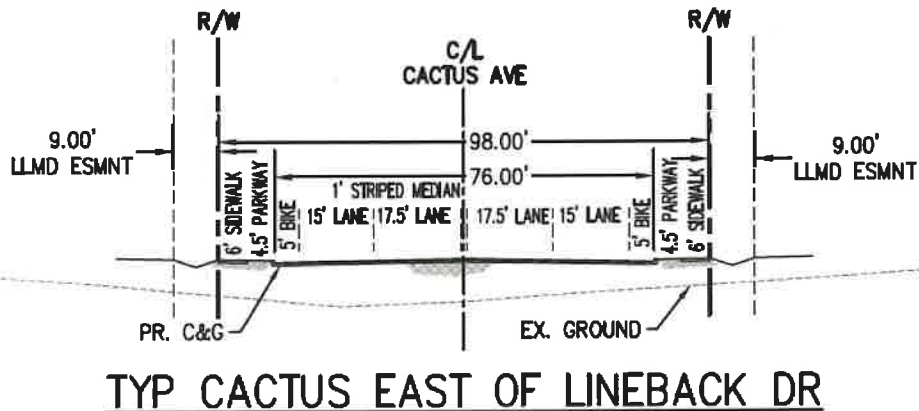
The following information further describes and illustrates the vehicular circulation network.

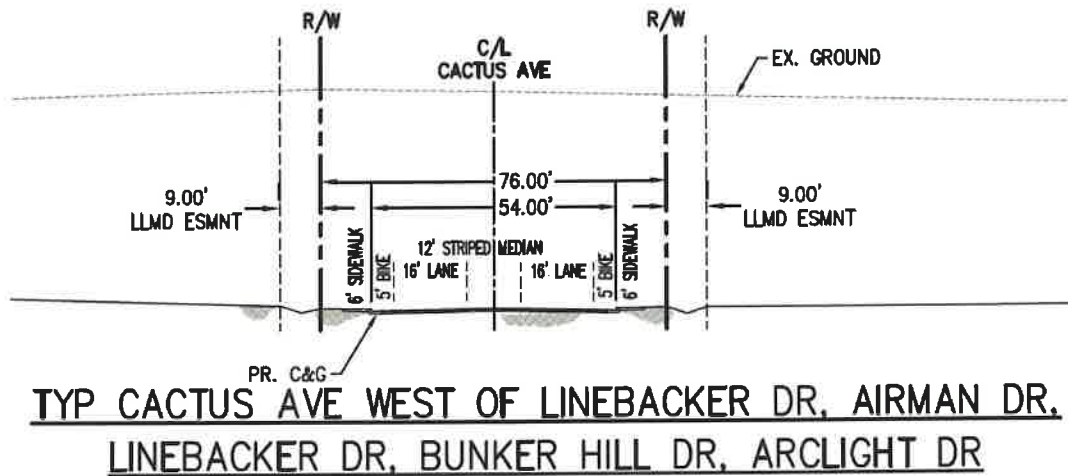
a. *Cactus Avenue:*

Cactus Avenue serves as the main access to the West Campus Upper Plateau Specific Plan. The public roadway will connect directly with the existing segment of Cactus Avenue to the east of the Specific Plan boundary, providing direct access to Meridian Parkway, I-215, and points further east. This roadway will consist of two design segments, serving as a modified secondary highway from the Specific Plan boundary westerly to Linebacker Drive, then serving as a modified industrial collector street within the industrial campus area.

The modified secondary highway segment will consist of a 98-foot wide right of way and 76 feet of curb-to-curb pavement width providing for a 17.5-foot and 15-foot travel lane in each direction, and a 5-foot bike lane in each direction. Additionally, a 6-foot sidewalk and 4.5-foot landscape parkway will also be provided within the street right of way.

The modified industrial collector segment of Cactus Avenue will consist of a 76-foot wide right of way with 54 feet of curb-to-curb pavement width providing for a single 16-foot travel lane in each direction, a 12-foot striped median and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. The remaining right-of-way and an abutting 9-foot Lighting and Landscaping Maintenance District (LLMD) easement will provide for a 14-foot-wide abutting landscape parkway buffer.

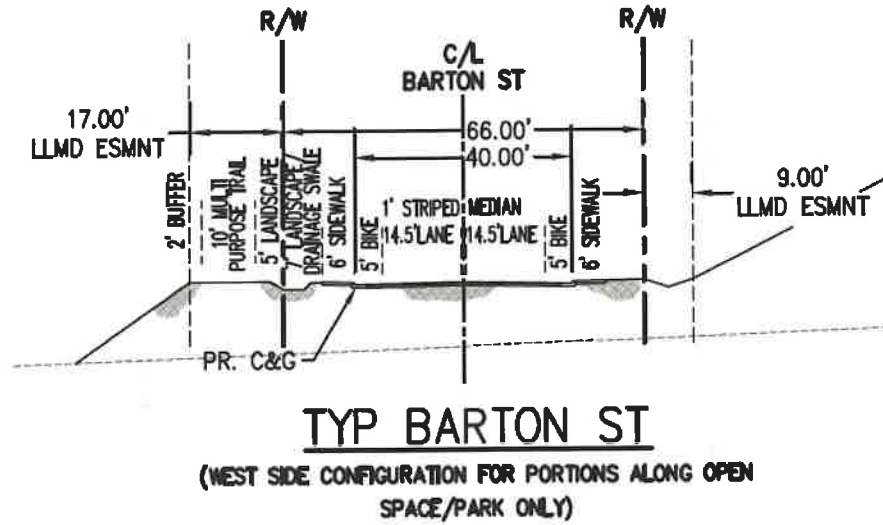




b. *Barton Street:*

Barton Street provides access to the westerly segment of the Specific Plan Area, serving the park, open space recreation area, and the western edge of the main Mixed-Use area. Barton Street will connect with the existing City of Riverside street network to the north and south of the Specific Plan boundary. This will allow for local access between the established residential neighborhoods and commercial areas in the Mission Grove community to the north and Orangecrest community to the south. Barton Street will not connect with Cactus Avenue, preventing direct vehicular access to and from the industrial campus area, except for emergency vehicles.

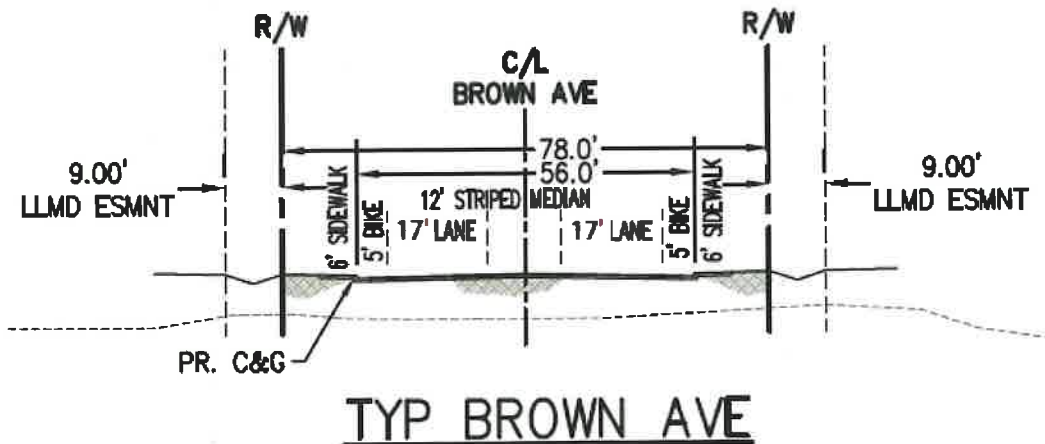
This roadway will be a 66-foot Collector design classification pursuant to the City of Riverside General Plan Circulation Element, consisting of a 66-foot wide right of way with 40 feet of curb-to-curb pavement width providing for a single 14.5-foot travel lane, a 1' striped median, and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. In addition, a 17-foot wide LLMD easement will exist along the west side of the roadway, providing for a 10-foot-wide multi-purposed trail, as well as a 5-foot landscape buffer that will be associated with a 7-foot-wide landscape buffer and drainage swale located within the street right-of-way. The multi-purpose trail is consistent with the City of Riverside's Trails Master Plan.



c. Brown Avenue:

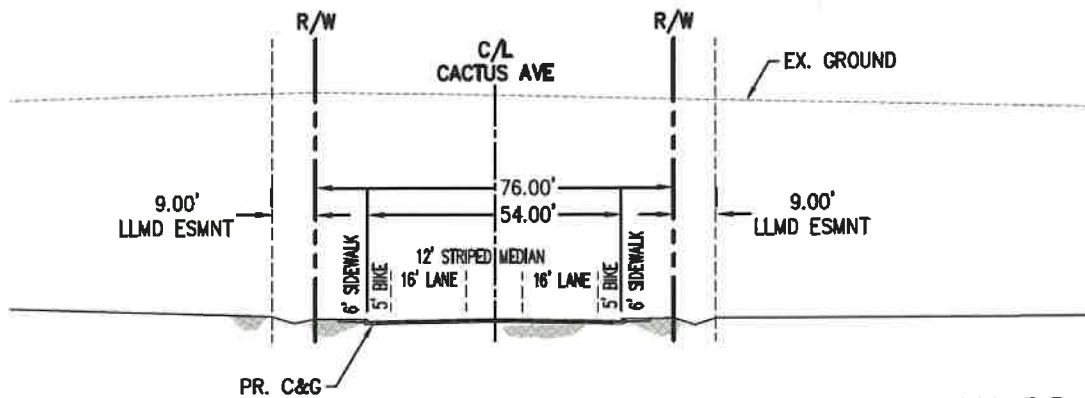
Brown Avenue serves as a secondary access to the Specific Plan Area, providing connection between Cactus Avenue and Alessandro Boulevard to the north. Though most vehicular trips serving the industrial campus area of the Specific Plan are expected to utilize Cactus Avenue and connect with Meridian Parkway and I-215, Brown Avenue will provide an alternative ingress/egress point to these connections, as well as westerly connections via Alessandro Boulevard.

The roadway will be an industrial collector street design, consisting of a 78-foot-wide right-of-way with 56 feet of curb-to-curb pavement width providing for a single 17-foot travel lane in each direction, a 12-foot striped median and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. The remaining right-of-way and an abutting 9-foot LLM D easement will provide for a 14-foot-wide abutting landscape parkway buffer.



d. Industrial Campus Interior Streets (Arclight Drive, Airman Drive, Bunker Hill Drive, Cactus Avenue, Linebacker Drive):

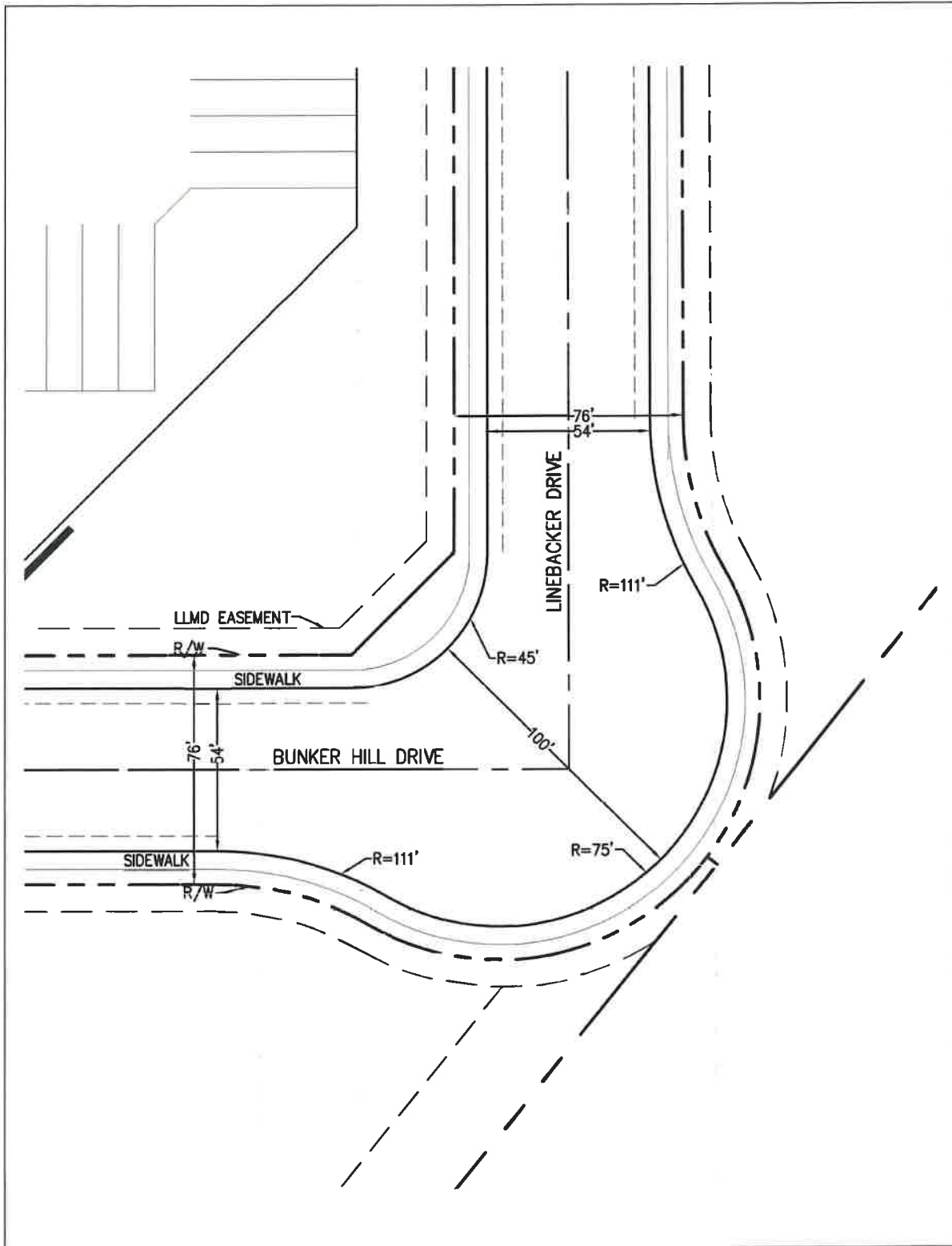
The industrial campus interior streets provide direct access between the industrial campus area and Cactus Avenue. The roadway will be a modified industrial collector street design, consisting of a 76-foot-wide right-of-way with 54 feet of curb-to-curb pavement width providing for a single 16-foot travel lane in each direction, a 12-foot striped median and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. The remaining right-of-way and an abutting 9-foot LLMD easement will provide for a 14-foot-wide abutting landscape parkway buffer.



**TYP CACTUS AVE WEST OF LINEBACKER DR, AIRMAN DR,
LINEBACKER DR, BUNKER HILL DR, ARCLIGHT DR**

e. Industrial Campus Interior Knuckle Design:

The industrial campus interior knuckle represents the condition at the north to south and east to west roadway transition points between the campus interior streets. The interior knuckles will provide a 100-foot-wide knuckle radius to accommodate for truck turning. For example, *Figure 5-2, Knuckle Concept Designs*, provides a conceptual knuckle design between Linebacker Drive and Bunker Hill Drive.



Source(s): DRC Engineering

Figure 5-2



Not to Scale

Knuckle Concept Designs

5.1.2 TRANSPORTATION DEMAND MANAGEMENT

While the West Campus Upper Plateau will provide a regional transportation benefit, much of the traffic accessing the site will be concentrated in peak commuting hours causing potential congestion. Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non-"drive alone" modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel. The following TDM measures are recommended:

1. The MIPA shall coordinate with the RCTC as the project Transportation Management Agency (TMA). The purpose of the TMA will be to:
 - Provide information on employee matching for carpools and van pools
 - Identify park and ride lot locations
 - Provide information on and encourage transit use
2. Each employer with more than 250 full-time employees shall submit a TDM plan to the JPA. The TDM plan shall address the following:
 - Designate a TDM coordinator
 - Provide a space (e.g., kiosk, bulletin board, etc.) for rideshare information
 - Provide preferential parking for carpools
 - Identify bus routes and bicycle facilities in the vicinity
 - Provide flexible working hours and/or a telecommuting program (to the extent feasible)
 - Bicycle storage facilities
 - Showers and locker room (optional)

5.1.3 TRUCK TRAFFIC

Industrial, business park, warehousing, and related uses typically generate a higher volume of truck traffic than other types of uses. The large size and acceleration/deceleration characteristics of trucks have a disproportionate impact on transportation capacity, as compared to passenger vehicles. In order to reduce the impacts of trucks on neighboring residential serving streets, Brown Street and Cactus Avenue will serve as the project site access for trucks. Truck traffic is discouraged from using Barton Street with no direct vehicular connection to Cactus Avenue being provided. *Figure 5-3, Truck Route*, represents streets identified for truck activity.

The project will cooperate with the City of Riverside to support measures to restrict the use of residential collector streets and secondary highways by trucks. Design of pavement sections will provide a structural depth sufficient for anticipated truck traffic. Key access intersections shall be designed to accommodate truck turns.

5.2 NON-AUTOMOBILE CIRCULATION

5.2.1 TRANSIT SERVICE

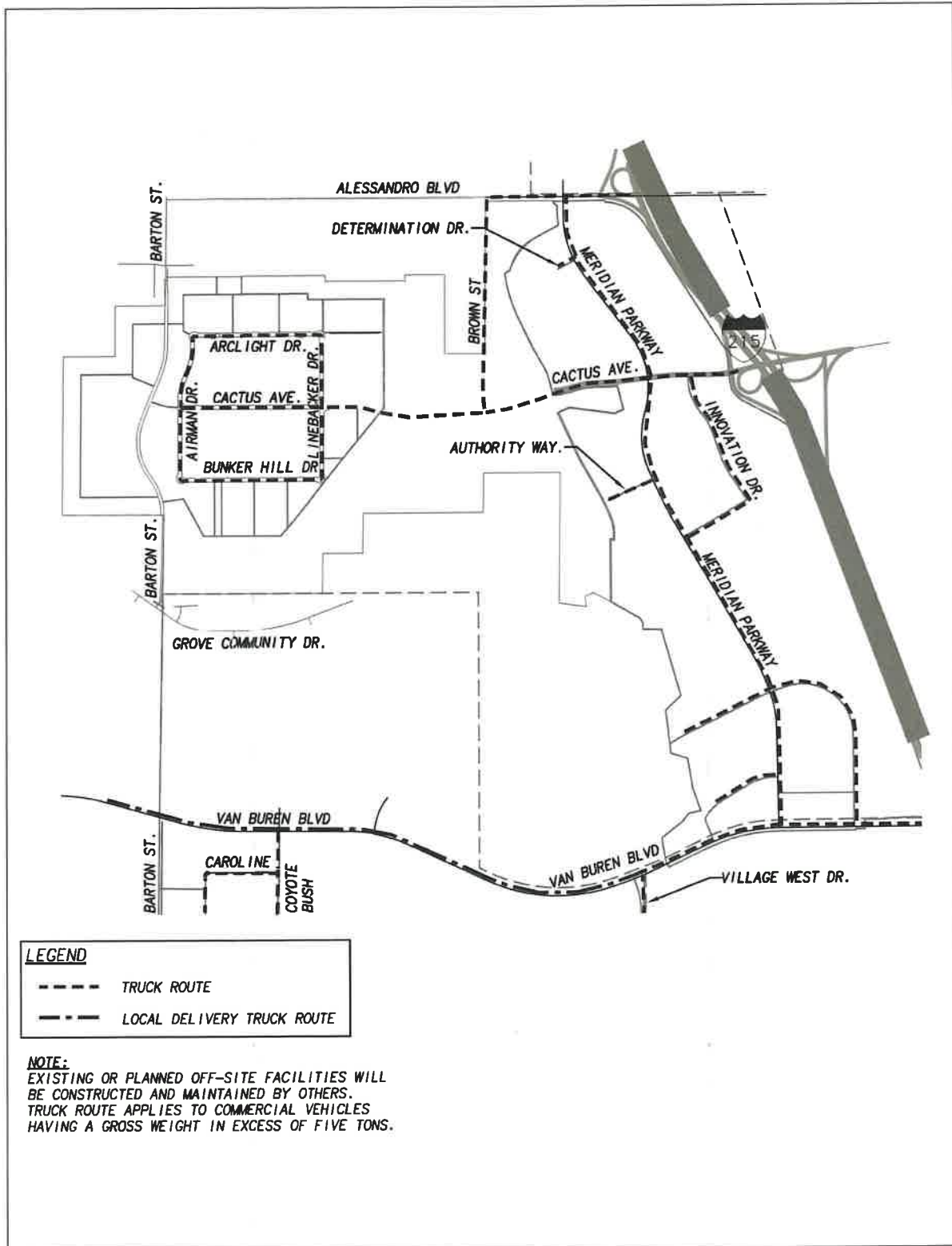
The West Campus Upper Plateau Specific Plan Area is within the Riverside Transit Agency service boundary. Bus transit service is currently provided near the Specific Plan, along Alessandro Boulevard and Orange Terrace Parkway. Additionally, a Metrolink rail stations is located on Meridian Parkway approximately 1.5 miles from the industrial campus area. As a result of the proximity to existing rail transit service and the anticipation of future bus transit service within the

Specific Plan Area, bus improvements, such as bus turnouts, bus stops, and terminals should be considered as part of the conditions of development for land uses that have a large number of employees.

5.2.2 BICYCLE/PEDESTRIAN ACCESS

Bicycle and pedestrian linkages will help implement the trip reduction strategies outlined in the Transportation Demand Management section as well as provide recreational opportunities for employees and visitors to the Project. The proposed network will consist of Bike Lanes (Class II facilities), which are designated by signs and traverse the shoulder of the roadway, a 10-foot-wide multi-purpose trail, and recreational trails. Roads within the Specific Plan Area will all contain 5' wide Class II bicycle lanes and 6' wide sidewalks. A 10-foot-wide multi-use trail will be built along the western side of Barton Street. Additionally, several existing recreational trails will be retained and maintained throughout the open-space and open-space conservation areas. *Figure 5-4, Non-motorized Path of Travel Schematic*, shows the path of travel for bikes, sidewalks, and trails throughout the Specific Plan Area. The linkages were identified based on the following criteria:

1. The network was defined based on 1) connectivity to the Metrolink station east of the project area, and 2) likely routes of travel between West Campus Upper Plateau recreational amenities and existing residential development in proximity to the project area.
2. The proposed routes will provide linkages to bicycle and pedestrian facilities identified by adjacent jurisdictions.
3. Class II facilities should be provided on internal streets to facilitate access to project land uses.



Source(s):

Figure 5-3



Not to Scale



Truck Route

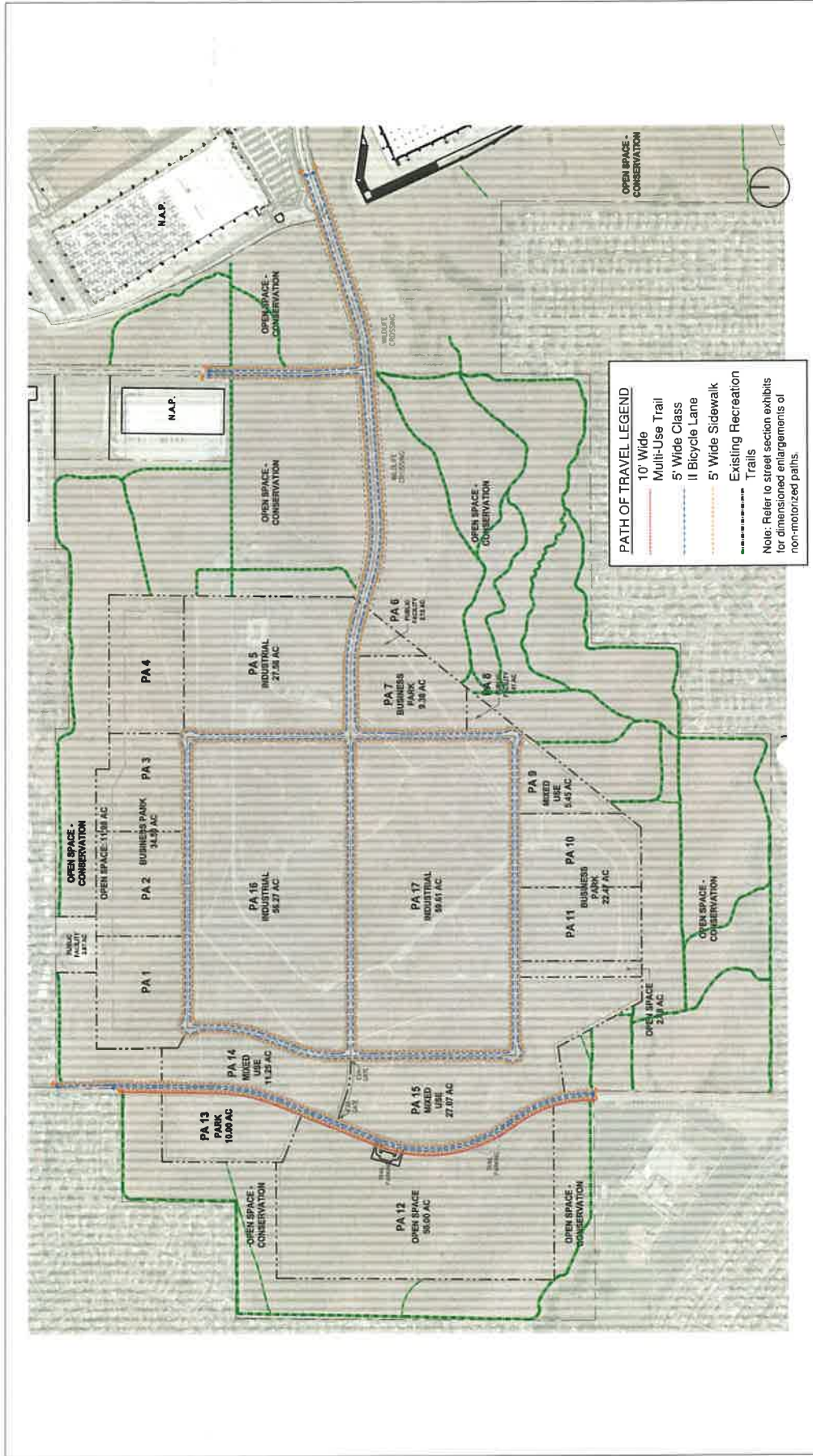


Figure 5-4

Non-Motorized Circulation Plan

Source(s): Hitch & Associates, Inc. (02.03.2022)



Specific Plan No. XXX

6 INFRASTRUCTURE AND GRADING

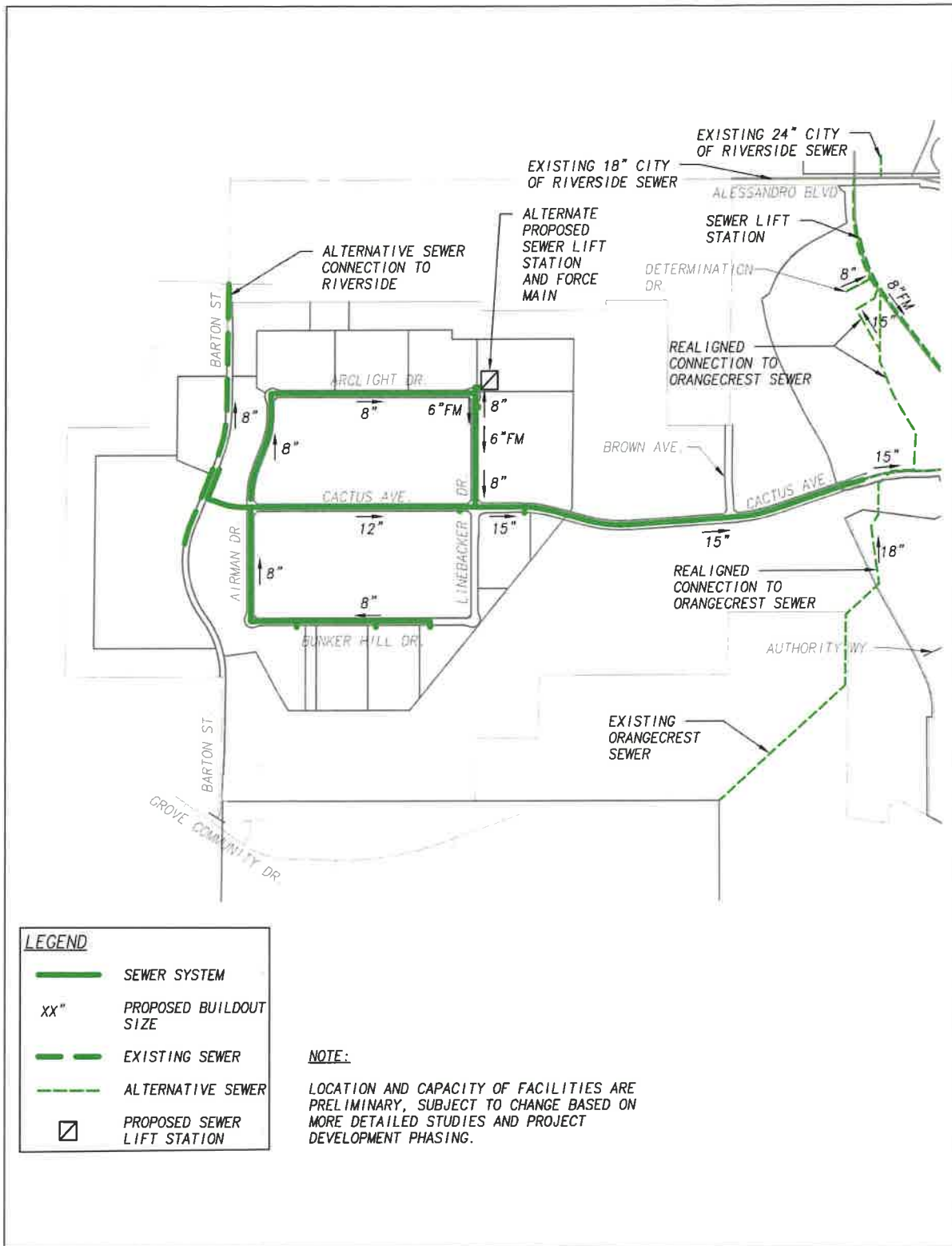
6.1 EXISTING INFRASTRUCTURE

The West Campus Upper Plateau Specific Plan area consists of vacant land with minimal municipal utilities or services. Public facilities, services, and infrastructure will be provided concurrently with the appropriate phase of project development.

6.2 SEWER SERVICE AND FACILITIES

Sanitary sewer service for the Specific Plan Area is provided by the Western Municipal Water District (WMWD). Currently, existing 15-inch City of Riverside Sewer lines are located within the Cactus Avenue right-of-way to the east of the Specific Plan boundary, which connects to an 8-inch force main sewer line in Meridian Parkway. Existing 8-inch sewer lines are located within the Barton Street right-of-way to the northwest and southwest of the Specific Plan area in the City of Riverside. The proposed sewer service plan represents the primary sewer connection being provided via Cactus Avenue and flowing to the east. A sewer connection is also identified via Barton Street to the north to the City of Riverside Sewer system, which would serve as an alternative should the park and open space lot be infeasible to sewer to the east to Cactus due to grade elevations. *Figure 6-1, Sewer System*, shows sewer facilities improvements for the Specific Plan area.

The Specific Plan area requires the planning, design, and construction of the sewer systems, which include: installing a sewer lift station on the northeastern corner of Arclight Drive and Linebacker Drive (as needed dependent upon final grading and pad elevations); installing an 8-inch sewer line in Arclight Drive, Airman Drive, and Bunker Hill Drive; installing a 12-inch sewer line in Cactus Avenue east of Airman Drive and west of Linebacker Drive; installing a 15-inch sewer line in Cactus Avenue east of Linebacker Drive; and installing an 8-inch sewer line in Linebacker Drive north of Cactus Avenue to connect with the 15-inch sewer line in Cactus Avenue. The 15-inch WMWD sewer lines currently stubbed at the terminus of the Cactus Avenue cul-de-sac.



Source(s): DRC Engineering (01-22-2022)

Figure 6-1



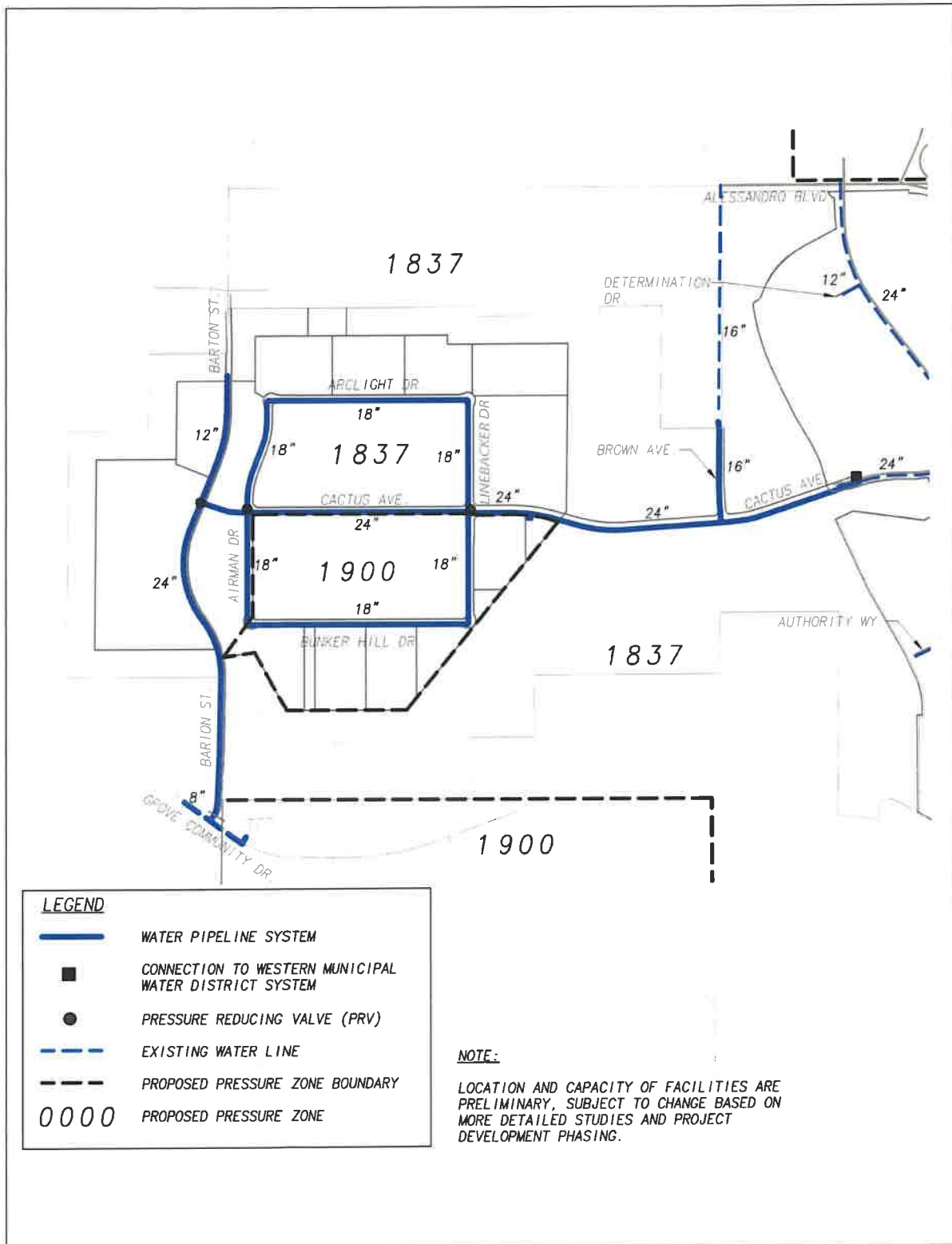
Sewer System

6.3 POTABLE WATER SERVICE

When March Air Force Base was an active-duty military installation, it consumed 2.14 million gallons of water a day for both domestic and irrigation uses. Potable water delivered to the Specific Plan Area is supplied by the Western Municipal Water District (WMWD) via a 24-inch distribution main operated by the WMWD. *Figure 6-2, Potable Water System*, illustrates project water supply facilities. All potable water facilities, including water mains, zone transitions, pressure pumps and reducers, storage facilities, will be operated and maintained by WMWD.

Currently, an existing 24-inch WMWD water line is located within the Cactus Avenue right-of-way to the east of the Specific Plan Area, and an existing 16-inch WMWD waterline is located within the Brown Avenue right-of-way. An 8-inch water line is located in Grove Community Drive to the south of the Specific Plan Area. The proposed potable water plan would connect to the 24-inch WMWD waterline via Cactus Avenue and loop to Barton Road.

The Specific Plan area requires the planning, design, and construction of the potable water systems, which include: installing pressure reducing valves (PRV) on the intersection of Linebacker Drive and Cactus Avenue, Airman Drive and Cactus Drive, and Barton Street and Cactus Drive; installing 18-inch water line in Arclight Drive, Airman Drive, Bunker Hill Drive, and Linebacker Drive; installing 12-inch water line in Barton Street north of Cactus Avenue; installing 24-inch water line in Barton Street south of Cactus Avenue; and installing 24-inch water line in Cactus Avenue. The 24-inch water line in Cactus Avenue will connect to the existing potable water facilities currently stubbed at the terminus of the Cactus Avenue cul-de-sac.



Source(s): DRC Engineering (01-22-2022)

Figure 6-2



Not to Scale



Potable Water System

6.4 RECLAIMED WATER

Reclaimed water service for the Specific Plan area will be provided by the Western Municipal Water District (WMWD). Existing 12-inch reclaimed water lines are located within the Cactus Avenue right-of-way to the east of the Specific Plan area. The proposed reclaimed water plan would connect to the existing Cactus Avenue service line. *Figure 6-3, Reclaimed Water System*, shows reclaimed water facilities improvement in the Specific Plan area.

The Specific Plan area requires the planning, design, and construction of the reclaimed water systems, which include the installing of 12-inch reclaimed water lines in Barton Street, Airman Drive, Arclight Drive, Cactus Avenue, Bunker Hill Drive, and Linebacker Drive. The 12-inch reclaimed water line in Cactus Avenue will connect to the existing reclaimed water facilities maintained by the WMWD.

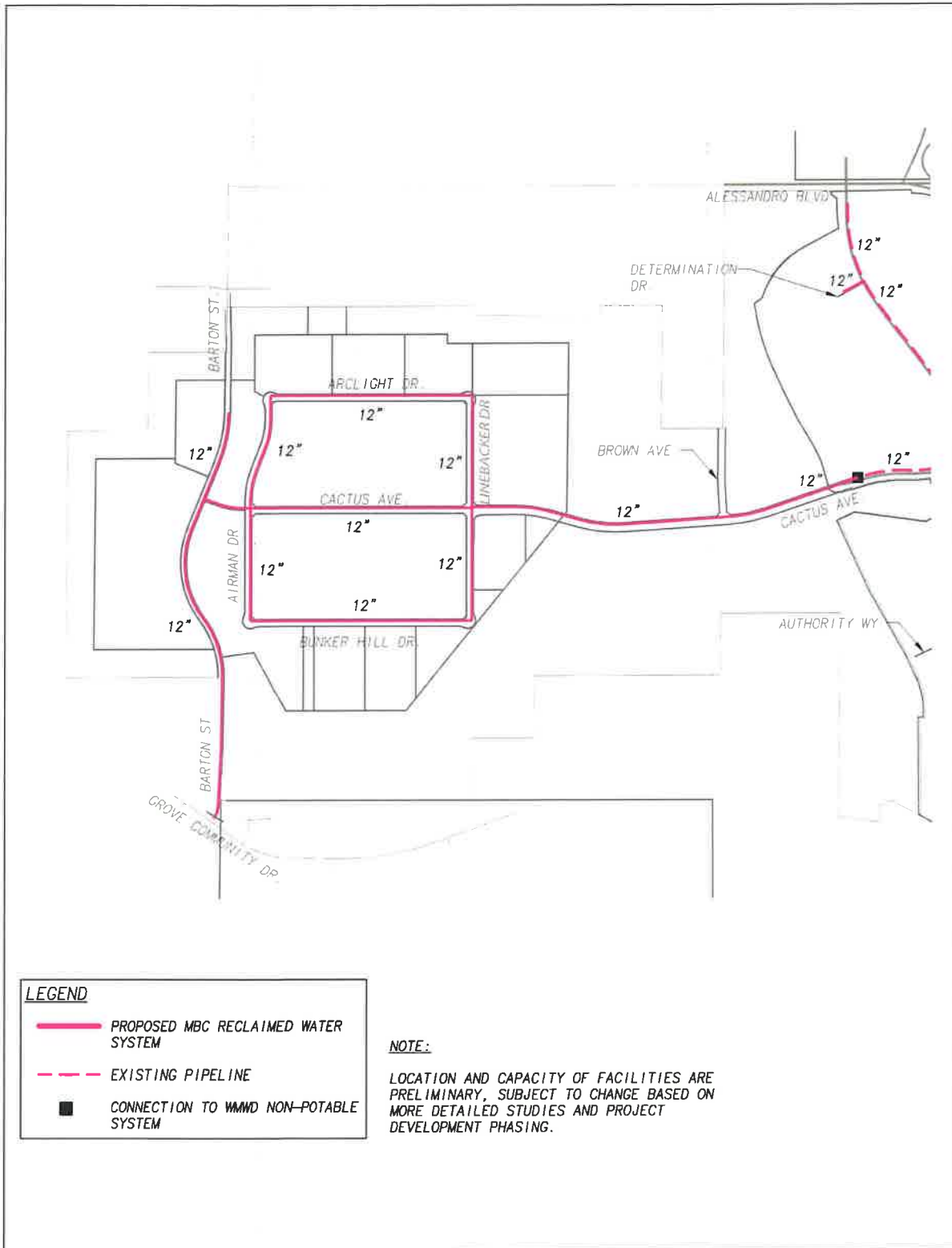
6.5 STORM WATER MANAGEMENT

The West Campus Upper Plateau storm water management plan is shown on *Figure 6-4, Storm Drain System*. Storm water in the northeastern portion of the Project area will be detained and ultimately flow to a detention basin on Alessandro Boulevard via an open channel. Storm water in the other parts of the Specific Plan Area will be detained and flow through a storm drain system and ultimately discharged to existing native flow lines across the boundary of the Specific Plan Area to match historical drainage patterns. Discharge points will be required to detain and mitigate flows to 90% of pre-developed flows to ensure there are no downstream erosion issues.

As indicated on *Figure 4-6*, various storm drain will in installed within the Specific Plan area. Storm drain improvement includes the following: a 24-inch storm drain system in the intersection of Cactus Avenue and Brown Avenue; a 24-inch storm drain system in Cactus Avenue east of Linebacker Drive and west of Brown Avenue; a 24-inch storm drain system in Cactus Avenue that stretches from Barton Street to the Public Facility land use on the eastern side of the Specific Plan area; a 24-inch storm drain system in Linebacker Drive south of Cactus Avenue; an 18-inch storm drain system that expands to 36-inches in Linebacker Drive north of Cactus Avenue extending to the northern side of the Specific Plan area; an 18-inch storm drain system that expands to 24-inches in Arclight Drive extending to the northern side of the Specific Plan Area; a 24-inch storm drain system in Airman Drive that extends to the northern side of the Specific Plan Area; a 24-inch storm drain system in Bunker Hill Drive extending to Airman Drive to the south of Cactus Avenue; and a storm drain system ranging from 18 to 36 inches in Barton Street extending to the northern, southern, and western border of the Specific Plan area. The storm drain system would ultimately connect with various open native channels and carry storm water off the Specific Plan Area consistent with historic drainage patterns.

6.6 GAS AND DRY UTILITIES

Southern California Gas Company and Southern California Edison will provide natural gas and electricity to the West Campus Upper Plateau Specific Plan area, respectively. As shown in *Figure 6-5, Gas Backbone*, *Figure 6-6, Electrical Backbone*, *Figure 6-7, Telephone Backbone*, and *Figure 6-8, Cable TV Backbone*, primary gas and dry utility lines will be installed to connect to existing gas and dry utility lines at Cactus Avenue to the east of the Specific Plan Area, and loop to Barton Street. Telephone/fiber service will be provided by Frontier Communications, and Cable TV will be provided by Spectrum Communications.



Source(s): DRC Engineering (01-22-2022)

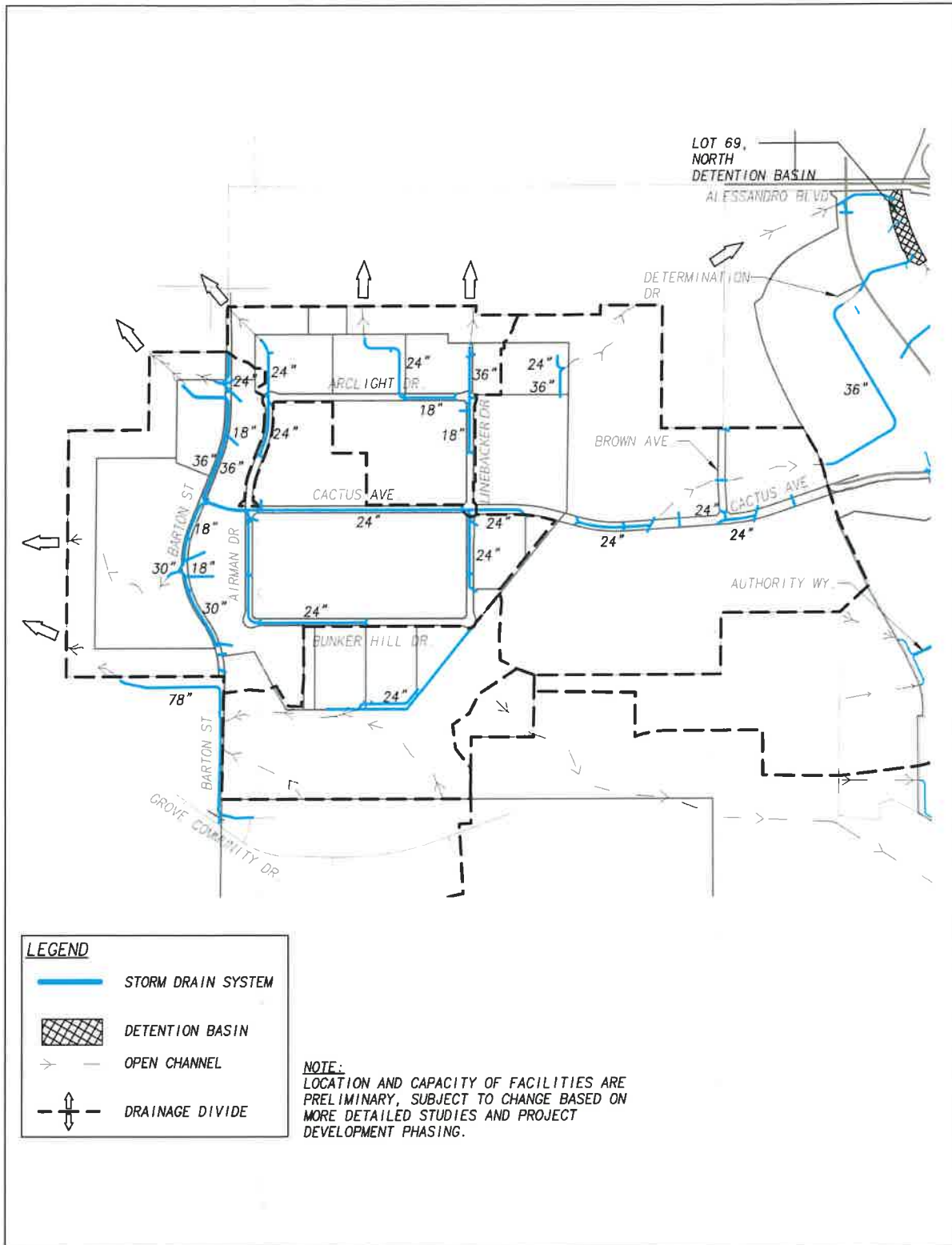
Figure 6-3



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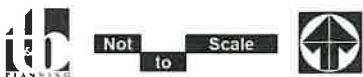


Reclaimed Water System

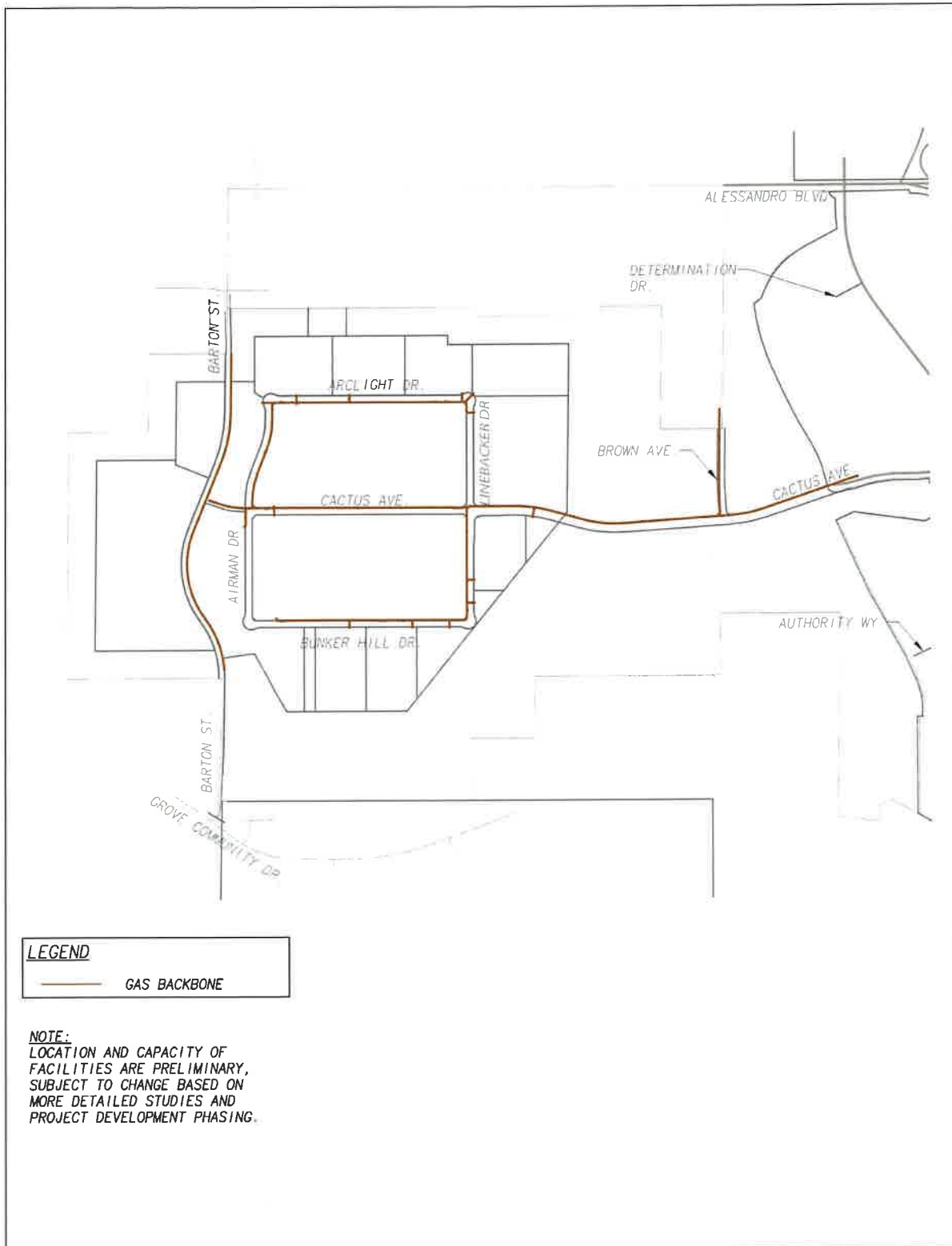


Source(s): DRC Engineering (01-22-2022)

Figure 6-4



Storm Drain System



Source(s): DRC Engineering (01-22-2022)

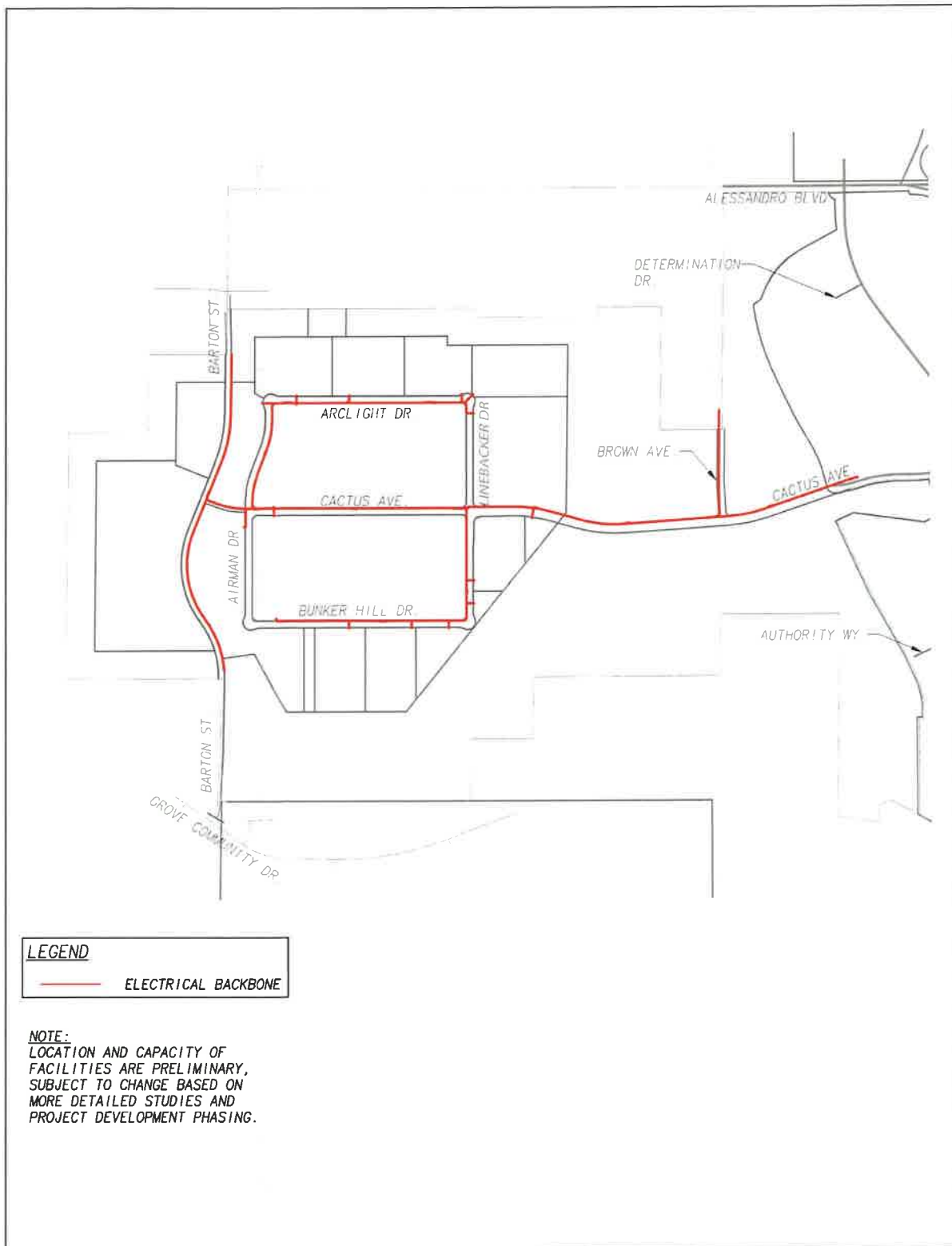
Figure 6-5



Not to Scale

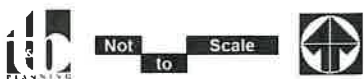


Gas Backbone

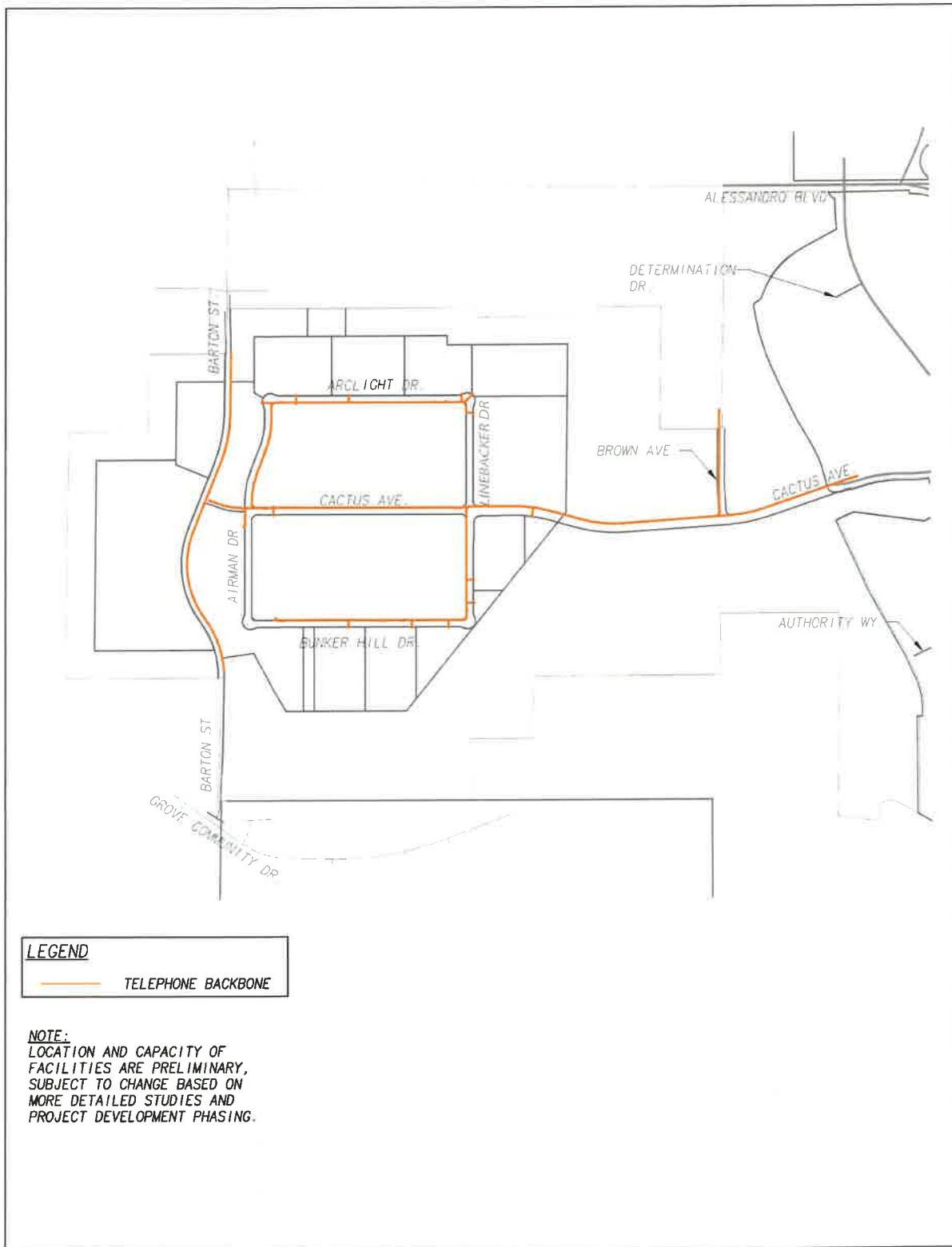


Source(s): DRC Engineering (01-22-2022)

Figure 6-6

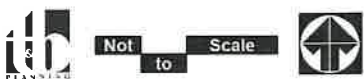


Electrical Backbone

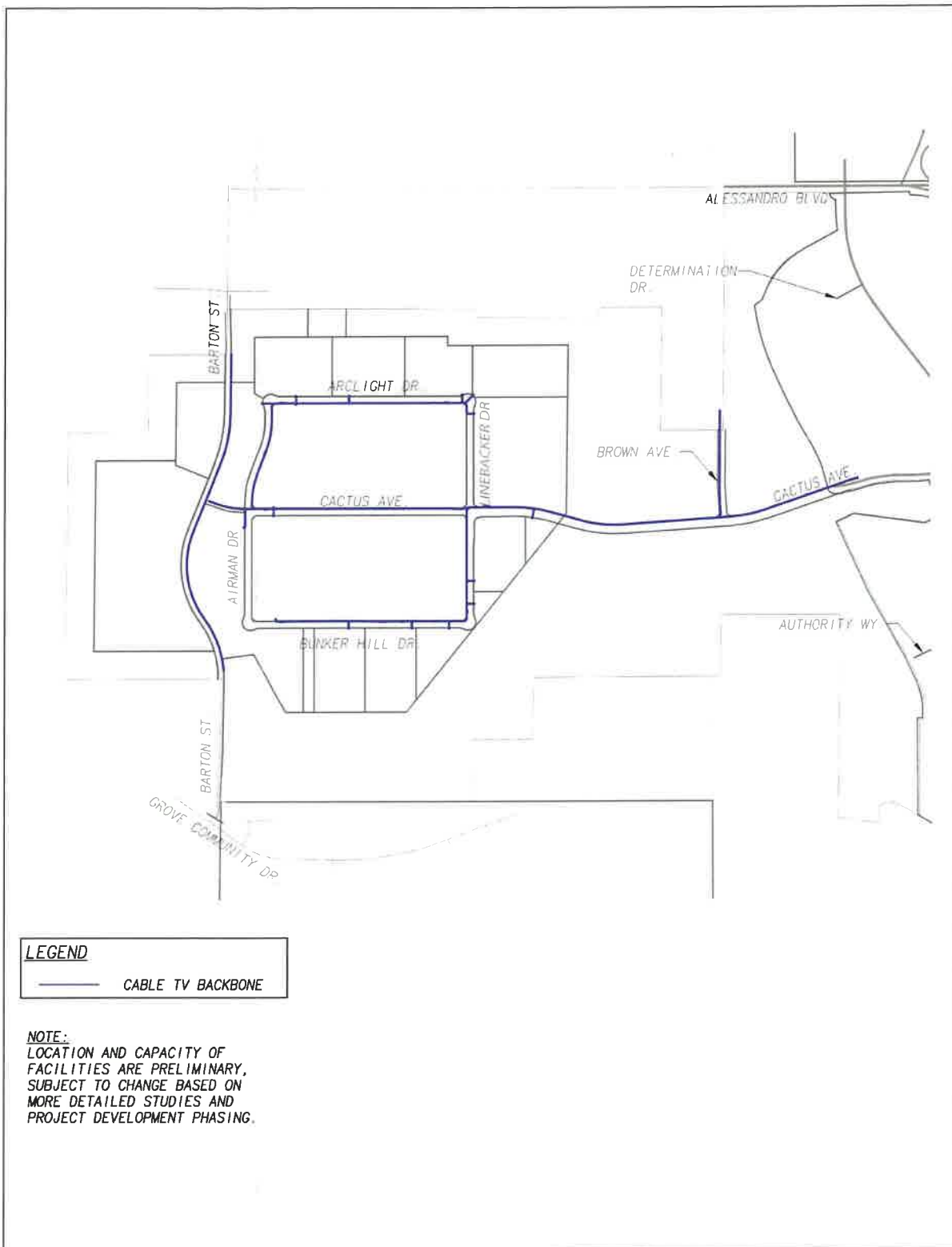


Source(s): DRC Engineering (01-22-2022)

Figure 6-7

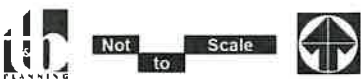


Telephone Backbone



Source(s): DRC Engineering (01-22-2022)

Figure 6-8



Cable TV Backbone

6.7 SOLID WASTE

Solid waste provider for the West Campus Upper Plateau Specific Plan is Burrtec Waste. Solid waste is disposed of at the El Sobrante, Lambs Canyon, and Badlands landfills. In order to reduce the amount of material generated by the Specific Plan, the West Campus Upper Plateau will comply with the requirements of the County of Riverside's Source Reduction and Recycling Element (SRRE).

6.8 GRADING

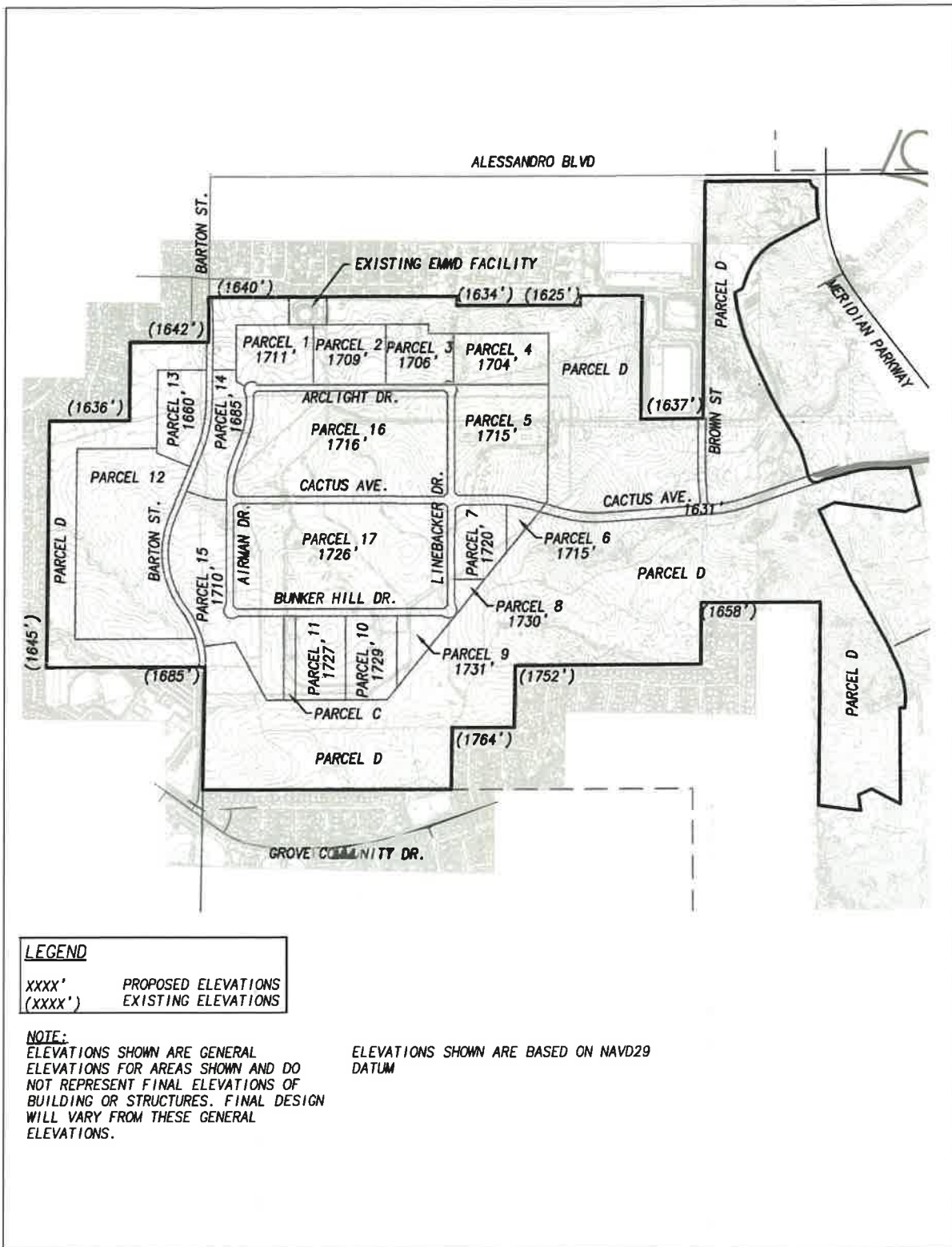
A conceptual grading design will be required for each Tentative Map application consistent with the March JPA Development Code. Grading designs will implement the goals and policies of the March JPA General Plan. *Figure 6-9, Conceptual Grading Exhibit*, shows the proposed grading for each individual parcel in the West Campus Upper Plateau Specific Plan area.

6.8.1 GRADING PLAN DEVELOPMENT STANDARDS

- All grading activities shall be in substantial conformance with the approved tentative map or development permit and shall implement any grading-related mitigation measures outlined in the accompanying EIR for the West Campus Upper Plateau.
- Prior to any development within any parcel of the Specific Plan, an overall grading plan for the portion in process shall be submitted for approval by the MJPA. The grading plan for each parcel shall be used as a guideline for subsequent grading plans for individual stages of development
- All streets shall have a gradient not exceeding use minimums and maximums established by the County of Riverside or as approved by the MJPA.
- A precise grading plan shall be prepared prior to any on-site grading for individual projects.
- The project developer/applicant shall be responsible for installation and maintenance of all planting and irrigation systems on manufactured slopes until those responsibilities are assumed by a Landscape Maintenance District or other parties.
- To the extent that is feasible, the overall shape, height, and gradient of any cut and fill slope shall be designed to be consistent with the existing natural contours and scale of the natural terrain.
- Potential brow ditches, terrace drains, or other minor swales, determined necessary at future stages of project review, shall be concealed, as feasible and possible, with landscape plantings, earth berms and similar features.
- Graded but undeveloped pads shall be maintained weed-free, appropriate erosion control measures within ninety (90) days of completion of grading, unless building permits are obtained from the MJPA. Appropriate desiltation basins are required for graded areas.
- Cut and fill slopes shall be constructed at inclinations of no steeper than two horizontal feet to one vertical foot, unless otherwise approved by the MJPA. Variable slope ratios will be used to avoid abrupt changes from the pads to the slopes.
- All newly created slopes exceeding 10 feet in vertical height shall be landscaped with a permanent irrigation system approved by the MJPA prior to final acceptance. Landscaping shall be consistent with the Specific Plan landscape design guidelines represented in Chapter 4.

- Grading shall not be permitted to commence prior to approval of grading permits for any proposed development. Mass grading will only occur for those areas undergoing development, or for those areas specifically identified as borrow or disposal sites.
- Grading operations within the confines of the Specific Plan area shall conform to all applicable MJPA Development Code standards.
- Project grading design shall make reasonable efforts to balance cut and fill on site to avoid the need for excessive importing or exporting of soil.

Manufactured slopes greater than 10 feet in vertical height, together with landscaping and irrigation systems, will be maintained by assessment district or owner's association. These slope areas will be entirely within a separate lot or easement. Irrigation systems maintained by the assessment district or owner's association will be separate from private systems. All slopes less than 10 feet in vertical height will be maintained by each project consistent with the MJPA Development Code.



Source(s):

Figure 6-9



Conceptual Grading Exhibit

7 IMPLEMENTATION

7.1 SEVERABILITY

This Specific Plan document enables the March Joint Powers Authority (MJPA) to facilitate the processing and approval of development plans and implementing permits to build out the West Campus Upper Plateau Specific Plan area. If any regulation, condition, program, or portion of this Specific Plan is held invalid or unenforceable, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such portions or provisions shall not affect the validity and enforceability of the remaining provisions contained herein.

7.2 APPLICABILITY

Approval of the West Campus Upper Plateau Specific Plan indicates acceptance by the MJPA of a general framework for the development of the Specific Plan property. Part of that framework establishes specific development standards that constitute the zoning regulations for the Specific Plan (refer to *Chapter 3, Development Regulations*). The provisions contained herein are intended to regulate development within the Specific Plan area.

Development within the West Campus Upper Plateau Specific Plan boundary shall be implemented through the MJPA's approval of tentative and final parcel maps and the Development Review process as established in the March Joint Power Authority's Development Code. The implementation process described herein provides the mechanisms for review and approval of development projects within the West Campus Upper Plateau Specific Plan.

7.3 INTERPRETATION

Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan shall be resolved by the MJPA's Planning Director, or his/her designee, in a manner consistent with the goals, policies, purpose, and intent established in this Specific Plan.

7.4 DEVELOPMENT REVIEW PROCESS

7.4.1. Subdivision Maps

Approval of future tentative subdivision maps within the West Campus Upper Plateau Specific Plan may occur concurrently with or subsequently to the adoption of the Specific Plan. All tentative and final subdivision maps shall be reviewed and approved pursuant to the applicable provisions of the MJPA and consistent with the applicable provisions established within the Land Use, Infrastructure, Design Guidelines, and Development Regulations chapters of this Specific Plan.

7.4.2. Development Plan Review

All development within the Specific Plan property shall be subject to the Development Review Process established in the MJPA's Development Code. Adoption of this Specific Plan by the Joint Powers Authority includes the design guidelines contained in Chapter 4, which shall be the design criteria by which development projects with the Specific Plan shall be reviewed during Development Plan Review. Topics on which these design guidelines are silent, the applicable design guidelines contained within the Authority's Development Code shall apply. The design guidelines are intended to be flexible in nature while establishing rudimentary evaluation criteria for the review by the Authority of development projects during design review.

7.4.3. Conditional Use Permits

Uses specified as conditionally permitted uses within Chapter 3, Development Regulations, of this Specific Plan shall be reviewed and approved by the MIPA pursuant to the requirements of the MIPA Development Code, Section 9.02.060, "Conditional Use Permits."

7.4.4. Variances

Administrative variances with respect to setback dimensions, lot coverage, building height, and fence height shall be reviewed pursuant to the Section 9.02.090 "Administrative Variances" of the MIPA Development Code. All other variances shall be considered in accordance with Section 9.02.100 "Variances" of the MIPA Development Code.

7.4.5. Development Agreement

Due to the scale and complexity of the Project, a Development Agreement is proposed to vest the Project entitlements and fees, ensure financing of public improvements required by the conditions of approval, and provide certain Community Benefits including compliance with the terms of the 2012 Settlement Agreement, and provision of new public benefits, including, but not limited to, expansion of employment opportunities for area residents.

7.5 SUBSTANTIAL CONFORMANCE

All development under the Specific Plan is subject to a Substantial Conformance Determination, considered and approved ministerially by the Planning Director or designee. The Substantial Conformance Determination is also a mechanism that allows for the approval of ministerial minor modifications for development under the Specific Plan. The MIPA recognizes that modifications to the text and exhibits of this document may be needed over time. Upon direction by the MIPA Planning Department, certain modifications to text, exhibits, and/or development standards and design guidelines may not require a formal Specific Plan Amendment (i.e., through public hearing) and occur ministerially. The following minor modifications to this document do not require a formal Specific Plan Amendment and are subject to review and approval by the Planning Director. The Planning Director shall have the discretion to defer any request for modification to the March Joint Powers Commission, either for ministerial direction and guidance, or determination that the required change requires a formal amendment as discussed below in Section 7.6. Ministerial substantial conformance decisions are not subject to CEQA compliance.

- Expansions or reductions of the net acreage covered by a given Planning Area.
- A decrease in development intensity/density (building square footage).
- Modification of design criteria such as architectural details, landscape treatments, fencing, lighting, and entry treatments.
- Changes to the Phasing Plan, provided infrastructure is available to serve the phase.
- Implementation of alternative landscape materials, wall and fence materials, entry monument design, corner treatments, and streetscape design that are generally consistent with the conceptual design guidelines contained within this Specific Plan.
- Modifications to Architectural Design Guidelines, such as variation of architectural style and variations in materials and colors.
- Final infrastructure facility sizing, and precise location of dry utilities, water, sewer, and storm drainage improvements as approved by the Planning Director or applicable utility agency.
- Roadway ROW design when the changes are warranted and approved by the Planning Director.

- Revisions to exhibits which do not substantially change the intent of the Specific Plan.
- Modification, deletions, and additions to the list of permitted and conditional uses.
- Specific modifications of a similar nature to those listed above which are deemed minor by the Planning Director, which are in keeping with the intent of this Specific Plan and which are in conformance with the MJPA General Plan.

7.6 FORMAL AMENDMENTS TO THE SPECIFIC PLAN

All modifications to this document which do not meet the criteria of a Substantial Conformance (as defined in the previous subsection) shall be deemed to require a formal Specific Plan Amendment. This document was prepared pursuant to California Government Code §65450, et. seq. Amendments shall be processed in accordance with the applicable requirements of the law, which include §65450, et. seq. of the California Government Code.

Formal Specific Plan Amendments shall be subject to the review and approval of the March Joint Powers Commission. As required by the California Government Code, all government agencies significantly affected by the proposed Amendment shall be notified of the proposed action prior to the approval. In addition, and as required by CEQA, formal Specific Plan Amendments shall be appropriately reviewed in accordance with the State CEQA Guidelines.

Any formal Specific Plan Amendment initiated by an applicant requires preliminary review by the Planning Director, filing of an official application and required materials supporting the Amendment, submittal of a fee deposit, and March Joint Powers Commission review and final decision.

7.7 APPEALS

Appeals of any determination of the Planning Director may be made by the applicant or any other aggrieved party by filing an application on forms provided by the MJPA and accompanied by the appropriate filing fee, where applicable, within ten (10) days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Section 9.02.240 "Appeals" of the MJPA Development Code.

7.8 COMPLIANCE WITH MITIGATION MONITORING AND REPORTING PROGRAM

Certification of an EIR shall be required prior to the approval of the Specific Plan. Development within the West Campus Upper Plateau Specific Plan shall comply with all approved mitigation measures as described in the MMRP included as part of the EIR.

The financing of construction, operation, and maintenance of public improvements, facilities, and public services shall include funding through a combination of financing mechanisms. Prior to the recordation of final maps, a final determination shall be made by MJPA staff and confirmed by the Planning Director and MJPA Engineer regarding the responsibility for construction and maintenance of public facilities, whether publicly or privately maintained.

Implementation of the West Campus Upper Plateau Specific Plan may involve financing options including, but not limited to, the following:

7.8.1 Facilities and Services

Construction of public improvements and facilities and the provision of public services may be financed through private capital investment, a Community Facilities District (CFD), or other special district, pursuant to the Mello-Roos Community Facilities District Act of 1982.

7.8.2 Operation and Maintenance

Options for operation and maintenance of public improvements and facilities include, but are not limited to, the following:

- Individual private property owners
- Private Property Owners Association
- LLMD (Landscaping and Lighting Maintenance District) or CFD (will be at the JPA's sole discretion)

7.9 MAINTENANCE PLAN

The public and private improvements constructed within the West Campus Upper Plateau Specific Plan shall be maintained through a combination of public and private entities as described in Table 7-1, *Maintenance Responsibilities*. Table 7-1 provides a list of maintenance entity options that may fund and/or maintain facilities within the Specific Plan. A Property Owners Association (POA) shall be established for the maintenance of common area landscape improvements and private roadways within areas of the Specific Plan. For areas in public ownership (such as public roadway ROWs), municipal maintenance districts may fund the maintenance of these areas.

Table 7-1 Maintenance Responsibilities

Facility	MJPA and/or CFD/LLMD	Property Owners Association	Property Owner or Occupant
Roadways (Cactus Avenue, Barton Street, Airman Drive, Arclight Drive, Bunker Hill Drive)			
Curb-to-curb improvements	✓		
Parkways within public right-of-way (ROW) or LLMD	✓		
Neighborhood Edges, Master Plan Trails and Medians	✓		
Landscape buffer located outside public ROW/LLMD		✓	
Off-street parking areas			✓
Traffic control signs – in the public ROW/LLMD	✓		
Traffic control signs – not in the public ROW/LLMD		✓	
Streetlights – in the public ROW	✓		
10-acre public park	✓		
Streetlights – not in the public ROW/LLMD		✓	
Public water, sewer, and storm drain improvements within public ROW (excluding laterals)	✓		
On-site landscaping and irrigation		✓	✓
Common open space		✓	
Walls and fences		✓	✓
Corner and Entry Monuments		✓	

Facility	MJPA and/or CFD/LLMD	Property Owners Association	Property Owner or Occupant
Tenant Signage		✓	✓
Off-street lighting		✓	✓
Offsite Storm Water Drainage/Water Quality Facilities – within the public ROW/LLMD	✓		
Onsite Storm Water Drainage/Water Quality Facilities (swales, basins, biotreatment filters, etc)		✓	
Fiber Optic communication system in the public ROW	✓		

8 CONSISTENCY WITH THE GENERAL PLAN

8.1 OVERVIEW

The West Campus Upper Plateau Specific Plan is based upon the goals and policies set forth in the March JPA General Plan. This section addresses conformance of the West Campus Upper Plateau Specific Plan to the General Plan on a general or conceptual basis.

8.2 GENERAL PLAN ELEMENTS

1. Land Use

Goal 1: *Land Use Plan provides for a balanced mix of land uses that contribute to the regional setting, and capitalize on the assets of the Planning Area, while insuring compatibility throughout the Planning Area with the regional plans.*

Consistency: Development of the West Campus Upper Plateau will occur in a logical pattern of growth, compatible with adjacent land uses and regional plans. The project will provide an employment center in a portion of the County that is largely residential. This will improve the balance of population and employment in the project vicinity, providing opportunity for residents to work locally, rather than commute to surrounding areas and counties.

Goal 2: *Locate land uses to minimize land use conflict or creating competing land uses and achieve maximum land use compatibility while improving or maintaining the desired integrity of the Planning Area and subregion.*

Consistency: The land use summary in Table 3-1 provides a mixture of compatible land uses that may be developed in the Specific Plan area. Incompatible or competing land uses will not be allowed in the Specific Plan area.

Goal 3: *Manage growth and development to avoid adverse environmental and fiscal effects.*

Consistency: Development of the project will be phased to the assurance of required infrastructure and services.

Goal 4: *Develop an identity and foster quality development within the Planning Area.*

Consistency: The West Campus Upper Plateau Design Guidelines establish architectural, signage, parking, and landscaping standards that will develop a project identity and foster quality development.

Goal 5: *Maximize and enhance the tax base and generation of jobs through new, reuse and joint use opportunities.*

Consistency: The West Campus Upper Plateau will be an employment center providing substantial enhancement to the tax base.

Goal 6: *Support the continued Military Mission of March Air Reserve Base, and preservation of the airfield from incompatible land use encroachment.*

Consistency: The industrial, mixed use, and recreational activities anticipated from development of the project site will be consistent with other existing uses within the March JPA boundary. These uses will support the Military Mission of the March ARB.

Goal 7: *Maximize the development potential as a regional Intermodal Transportation facility to support both passenger and freight-related air services.*

Consistency: The West Campus Upper Plateau will develop warehouse, storage, and distribution facilities that can serve as a regional Intermodal Transportation facility to support passenger and freight-related air services.

Goal 8: *Preserve the natural beauty, minimize degradation of the March JPA Planning Area, and provide enhancement of environmental resources, and scenic vistas.*

Consistency: The West Campus Upper Plateau will preserve approximately 445 acres of open space surrounding the Specific Plan Area, preserving environmental resources and scenic vistas of the March JPA Planning Area.

Goal 9: *Preserve the integrity of the historic and cultural resources of the Planning Area and provide for their enhancement.*

Consistency: Most of the project site previously served as the ammunition storage area for the former March AFB. The West Campus Upper Plateau Specific Plan proposes to embrace this more recent historic use of the site by preserving one of the numerous ammunition bunkers located on the project site.

Goal 10: *Avoid undue burdening of infrastructure, public facilities, and services by requiring new development to contribute to the improvement and development of the March JPA Planning Area.*

Consistency: Future development within the West Campus Upper Plateau Specific Plan project area will be required to develop infrastructure and public facilities, as well as provide fair share financial contributions necessary to ensure no adverse impact or undue burdening to public infrastructure or services occur.

Goal 11: *Plan for the location of convenient and adequate public services to serve the existing and future development of March JPA Planning Area.*

Consistency: All public facility connections are located adjacent to the site, and adequate capacity has been deemed available by the responsive agencies. Service facility letters were obtained from these agencies and their comments/recommendations have been incorporated into the project accordingly.

Goal 12: *Ensure, plan, and provide adequate infrastructure for all facility reuse and new development, including but not limited to, integrated infrastructure planning, financing, and implementation.*

Consistency: Development of the project will be phased to the assurance of required infrastructure and services. The Specific Plan accommodates a number of financing strategies to fund public improvement.

Goal 13: *Secure adequate water supply system capable of meeting normal and emergency demands for existing and future land uses.*

Consistency: As described in Chapter 6, *Infrastructure and Grading*, the water supply system will have sufficient capacity to accommodate projected normal and emergency needs.

Goal 14: *Establish, extend, maintain, and finance a safe and efficient wastewater collection, treatment and disposal system which maximizes treatment and water recharges, minimizes water use, and prevents groundwater contamination.*

Consistency: As described in Chapter 6, *Infrastructure and Grading*, the West Campus Upper Plateau will provide the necessary facilities to establish a wastewater collection, treatment, and disposal system.

Goal 15: *In compliance with state law, ensure solid waste collection, siting, and construction of transfer and/or disposal facilities, operation of waste reduction and recycling programs, and household hazardous waste disposal programs and education are consistent with the County Solid Waste Management Plan.*

Consistency: Development within the West Campus Upper Plateau Specific Plan area will comply with the requirements of the County of Riverside's Source reduction and Recycling Element (SRRE).

Goal 16: *Adequate supplies of natural gas and electricity from utility purveyors and the availability of communications services shall be provided within the March JPA Planning Area.*

Consistency: All public facilities connections are located adjacent to the site, and adequate capacity has been deemed available by the responsive agencies. Service facility letters were obtained from these agencies and their comments/recommendations have been incorporated into the project accordingly.

Goal 17: *Adequate flood control facilities shall be provided prior to, or concurrent with, development in order to protect the lives and property within the March JPA Planning Area.*

Consistency: As described in Chapter 6, *Infrastructure and Grading*, the West Campus Upper Plateau will provide drainage facilities to provide adequate flood control.

2. Transportation

Goal 1: *Establish and provide for a comprehensive transportation system that captures the assets and opportunities of the planning area, existing transportation facilities, and planned transportation facilities for the future growth and development of the planning area and sub-region.*

Consistency: The West Campus Upper Plateau Specific Plan identifies and proposes to develop a comprehensive street network that will seamlessly connect and integrate with the existing roadway network and transportation improvements planned for the area and region.

Goal 2: *Build and maintain a transportation system which capitalizes on the multi-faceted elements of transportation planning and systems, designed to meet the needs of the planning area, while minimizing negative effects on air quality, the environment and adjacent land uses and jurisdictions.*

Consistency: The West Campus Upper Plateau Specific Plan proposed the development of a multi-faceted transportation network, providing functional and convenient vehicular

circulation, ease of access and use by pedestrians and cyclists, as well as utilizing existing and future transit opportunities afforded by the Riverside Transit Authority and Metrolink.

Goal 3: *Develop a transportation system that is safe, convenient, efficient and provides adequate capacity to meet local and regional demands.*

Consistency: The West Campus Upper Plateau will construct an internal street network and provide transportation capacity improvement to existing facilities off-site based on future demand. Transportation improvements will be constructed in phases based on project development and projected background traffic growth.

Goal 4: *Provide a balanced transportation system that ensures the safe and efficient movement of people and goods throughout the planning area, while minimizing the use of land for transportation facilities.*

Consistency: The West Campus Upper Plateau internal streets are sized to accommodate projected future vehicular traffic in a safe and efficient manner.

Goal 5: *Plan and encourage land use patterns and designs which enhance opportunities for non-vehicular circulation and improve trip reduction strategies.*

Consistency: Site plans for individual buildings shall be reviewed to ensure that pedestrian, bicycle and transit access is facilitated. A bicycle and pedestrian circulation network is planned for the project area and will be provided.

Goal 6: *Establish vehicular access control policies in order to maintain and insure the effectiveness and capacity of arterial roadways.*

Consistency: The project's internal roadways will be designed in accordance with the "County Road Improvement Standards and Specifications," published by the County of Riverside, and take into account additional requirements established in the Riverside County Integrated Plan.

Goal 7: *Facilitate and develop transportation demand management and transportation systems management programs, and use of alternate transportation modes.*

Consistency: Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non- "drive alone" modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel.

Goal 8: *Adequate, affordable, equitably distributed and energy efficient public and mass transit services which promote the mobility to, from, and within the planning area shall be provided.*

Consistency: The project area is served by the Riverside Transit Authority. Current bus service is adjacent to the site. In addition, the Moreno Valley Metrolink station is approximately 1.5 miles from the industrial campus portion of the project site. Future development will include improvements to encourage and support bus service within the project, particularly in the industrial campus area.

Goal 9: *Develop measures which will reduce the number or vehicle-miles traveled during peak travel periods.*

Consistency: The West Campus Upper Plateau will improve job/housing balance by providing an employment center in an area that is largely residential. This will allow residents to work locally, rather commuting to Los Angeles or Orange Counties. Job/housing balance will help reduce vehicle miles of travel.

Goal 10: *Regulate the travel of trucks on March JPA Planning Area streets.*

Consistency: The project is designed to accommodate truck traffic. In addition, trucks will be required to travel on designated routes as they traverse the West Campus Upper Plateau internal streets and connect to the

Goal 11: *Adequate off-street parking for all land uses shall be provided which requires adequate on-site parking to prevent spill over on the adjacent street system.*

Consistency: The West Campus Upper Plateau Specific Plan provides parking ratios that will limit the potential for parking spillover.

Goal 12: *Plan for and seek to establish an area-wide system of bicycling trails, with linkages within the planning area and adjacent jurisdictions, and in compliance with sub-regional plans.*

Consistency: The West Campus Upper Plateau Specific Plan roadway network will provide for bicycle lanes on all streets, as well as establish a 10-foot-wide multi-purpose trail along the west side of Barton Street. This will be of benefit to the employees and visitors to the Specific Plan, as well as provide connectivity for an improved local and regional bicycling network.

Goal 14: *Goods movement through the San Jacinto Rail Branchline shall be capitalized.*

Consistency: The West Campus Upper Plateau is located west of the BNSF railway, and goods movement within the Specific Plan Area can utilize the San Jacinto Rail Branchline.

Goal 15: *In accordance with state and federal law, promote and provide mobility for the disabled.*

Consistency: Development plans and public improvement plans shall take into account and ensure compliance with all applicable accessibility requirements of the Americans with Disability Act (ADA).

3. Noise/Air Quality

Goal 1: *Ensure that land uses are protected from excessive and unwanted noise.*

Consistency: Project development shall be consistent with the land uses limitations established in the AICUZ study and the Riverside County Airport Land Use Plan.

Goal 2: *Minimize incompatible noise level exposures throughout the Planning Area, and where possible, mitigate the effect of noise incompatibilities to provide a safe and healthy environment.*

Consistency: Project development shall be consistent with the land uses limitations established in the AICUZ study and the Riverside County Airport Land Use Plan.

Goal 3: *Work toward the reduction of noise impacts from vehicular traffic, and aviation and rail operations.*

Consistency: The project shall implement all noise mitigation provisions established in the project EIR.

Goal 4: *Promote alternative modes of travel.*

Consistency: The transportation network that will be developed to serve the project will include improvements to encourage pedestrian and bicycle activity, as well as use of transit opportunities being provided by the Riverside Transit Authority and Metrolink.

Goal 5: *Reduce emissions associated with vehicle miles traveled by enhancing the jobs/housing balance of the subregion of western Riverside County.*

Consistency: The West Campus Upper Plateau improve job/housing balance in Western Riverside County by providing an employment center in an area that is largely residential. This will provide an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Job/housing balance will help reduce vehicle miles of travel, resulting in reduced emissions.

Goal 6: *Reduce air pollution through proper land use, transportation and energy use planning.*

Consistency: The West Campus Upper Plateau Specific Plan will provide access using a variety of transportation modes, including bicycle and pedestrian activity. The project's Transportation Demand Management (TDM) strategy will accommodate the shift of some trips from "drive-alone" to transit or non-motorized modes of travel.

Goal 7: *Pursue reduced emissions for stationary and mobile sources through the use and implementation of new and advancing technologies.*

Consistency: Where feasible and appropriate, development of West Campus Upper Plateau shall accommodate the use of advancing technologies, such as alternate fueled vehicles and other innovations that would provide air quality benefits.

Goal 8: *Maximize the effectiveness of air quality control programs through coordination with other governmental entities*

Consistency: Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including compliance with the South Coast Air Quality Management District, improvement of the air quality in South Coast Air Basin, and cooperation with neighboring jurisdictions.

Goal 9: *Reduce emissions associated with vehicle/engine use.*

Consistency: The West Campus Upper Plateau improve jobs/housing balance in western Riverside County by providing an employment center in an area that is largely residential. This will provide an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Job/housing balance will help reduce vehicle miles of travel. In addition, site improvements are encouraged that provide opportunity for utilization of electric vehicles.

Goal 10: *Reduce emissions associated with energy consumption.*

Consistency: Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including the use of the energy-efficient equipment and design, implementation of energy conservation features and recycling programs, and support of drought-resistant vegetation.

Goal 11: *Reduce Air pollution emissions and impacts through siting and building design.*

Consistency: Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including the use of low polluting construction materials and coatings and separation of sensitive receptors from toxic and carbon monoxide emissions.

Goal 12: *Reduce fugitive dust and particulate matter emissions.*

Consistency: Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including the implementation of fugitive dust reduction techniques, support of efficient street cleaning equipment, maintenance of the natural topography, and compliance with the South Coast Air Quality Management District.

4. Housing

The March JPA General Plan did not identify any housing opportunities within the March JPA Planning Area due to land use compatibility issues related to the continued military activities of the Air Force Reserves and aviation operations. The General Plan also identifies the need to focus on the reestablishment of jobs lost due to base realignment, and the housing rich environment of Western Riverside County. The West Campus Upper Plateau Specific Plan maintains consistency with the General Plan's absence of a residential land use designation within the Planning Area.

5. Resource Management

Goal 1: *Conserve and protect surface water, groundwater, and imported water resources.*

Consistency: The project will be constructed to minimize impacts to the existing drainage channels. The landscape plan includes drought tolerant plants. Irrigation will be moisture sensitive to limit irrigation during times of heavy rain.

Goal 2: *Control flooding to reduce major losses of life and property.*

Consistency: The West Campus Upper Plateau Specific Plan includes future improvements that properly capture, control, and maintain stormwater as required by state, regional, and local standards. These improvements will help control flooding

Goal 3: *Conserve and protect significant landforms, important watershed areas, mineral resources and soil conditions.*

Consistency: The project EIR has been prepared to assess and, if appropriate, mitigate project impacts upon geology, soils, and hydrology.

Goal 4: *Conserve energy resources through use of available energy technology and conservation practices.*

Consistency: The West Campus Upper Plateau shall comply with applicable regulations relating to energy conservation.

Goal 5: *Conserve and protect significant stands of mature trees, native vegetation, and habitat within the planning area.*

Consistency: The Specific Plan will preserve approximately 445 acres of open space surrounding the Specific Plan Area, conserving and protecting significant stands of habitat within the March JPA planning area.

Goal 6: *Provide an effective and efficient waste management system for solid and hazardous wastes that is financially and environmentally responsible.*

Consistency: The West Campus Upper Plateau shall comply with appropriate and applicable regulations and standards with respect to the management of solid and hazardous waste.

Goal 7: *Promote cultural awareness through preservation of the planning area's historic, archaeological, and paleontological resources.*

Consistency: The project EIR has been prepared to assess and, if appropriate, mitigate project impacts upon historic, archaeological, and paleontological resources within the project boundary.

Goal 8: *Develop and maintain recreational facilities as economically feasible, and that meet the needs of the community for recreational activities, relaxation, and social interaction.*

Consistency: The project will include a 10-acre park and a 50-acre open space area for hiking and other passive recreational uses. These facilities will be utilized by employees of the West Campus Upper Plateau, surrounding residents, and visitors.

Goal 9: *Create a network of open space areas and linkages throughout the Planning Area that serves to preserve natural resources, protect health and safety, contributes to the character of the community, provide active and passive recreational use, as well as visual and physical relief from urban development.*

Consistency: The West Campus Upper Plateau Specific Plan provides for approximately 445 acres of land dedicated for park, open space, and conservation use. This will not only help provide transitional buffering between existing residential and proposed industrial, business park and mixed-use activities, but also allow for passive recreational use and habitat.

Goal 10: *Establish standards for scenic corridors, trails and vistas that contributes to the quality of the planning area.*

Consistency: The Specific Plan project site will provide a 10-acre park and 50-acre open space area for passive use via a trail system. Additionally, a multi-purpose trail is proposed along the western side of Barton Street that will serve as access to the park and open space areas in addition to through trips. The project site design will provide opportunity to take advantage of territorial and scenic views afforded from this location.

6. Safety/Risk Management

Goal 1: *Minimize injury and loss of life, property damage, and other impacts caused by seismic shaking, fault rapture, ground failure, and landslides.*

Consistency: A geological reconnaissance has been conducted for the property. That study revealed that there are no active or inactive faults crossing the property and that the property is suitable for development.

Goal 2: *Minimize grading and otherwise changing the natural topography, while protecting the public safety and property from geologic hazards.*

Consistency: Grading within the Specific Plan area is designed to minimize impacts to the existing topography. The project will incorporate grading development standards and recommendations, which will minimize any potential geotechnical and site development constraints that occur on-site.

Goal 3: *Minimize injury, loss of life, property damage, and economic and social disruption caused by flood hazards.*

Consistency: The West Campus Upper Plateau will provide a number of drainage facilities to ensure flood hazards associated with the project site are managed in accordance with applicable, state, regional and local requirements.

Goal 4: *Reduce threats to public safety and protect property from wildland and urban fire hazards.*

Consistency: The West Campus Upper Plateau shall comply with applicable regulations and guidelines relating to brush management and fire protections services.

Goal 5: *Reduce the potential for hazardous material exposure or contamination in the Planning Area.*

Consistency: The West Campus Upper Plateau shall comply with regulations and guidelines relating to hazardous material exposure/contamination.

Goal 6: *Ensure to the fullest extent practical that, in the event of a major disaster, critical structures and facilities remain safe and functional.*

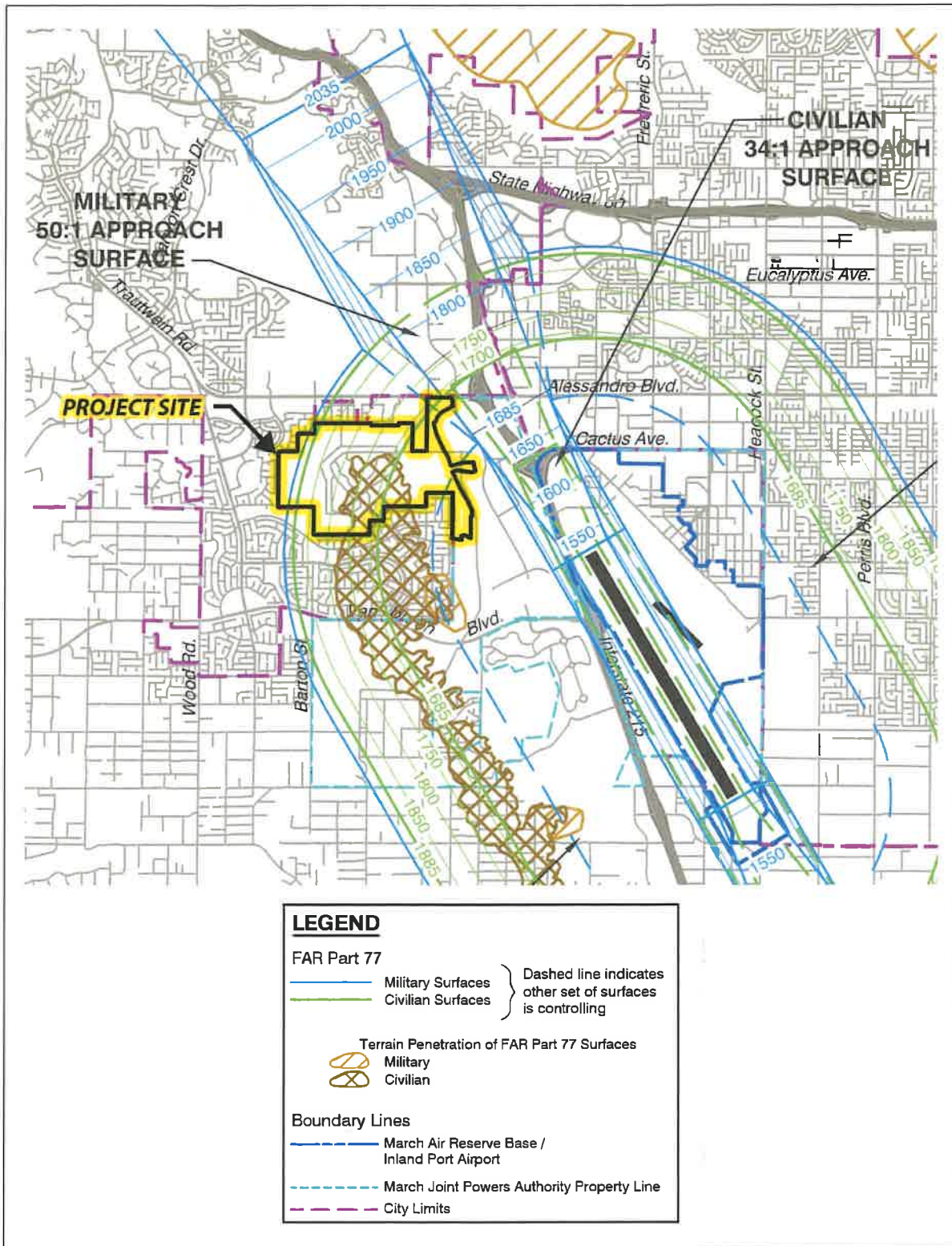
Consistency: The West Campus Upper Plateau shall comply with regulations and guidelines relating to the functionality of critical structures in the event of a major disaster.

Goal 7: *Reduce the possible risk of upset, injury and loss of life, property damage, and other impacts associated with an aviation facility.*

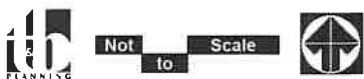
Consistency: The West Campus Upper Plateau is designed to incorporate appropriate uses within the development-limited areas as defined in the Air Installation Compatible Use Zone (AICUZ) Study done in 1998. The project will also comply with the Airport Land Use Plan.

Goal 8: *Plan for emergency response and recovery from natural and urban disasters.*

Consistency: The West Campus Upper Plateau shall comply with appropriate and applicable regulations and guidelines relating to emergency response and recovery from natural and urban disasters.



Source(s): March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (11-13-2014)



Not to Scale

MARB Airspace Protection Surfaces

Specific Plan No. XXX

Appendix A

4a 151

APPENDIX A - LANDSCAPE PLANT PALETTE

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>
<u>TREES</u>	
<i>Arbutus unedo</i>	Strawberry Tree
<i>Arbutus 'Marina'</i>	Marina Strawberry Tree
<i>Caesalpinia cacalaco</i>	Cascalote
<i>Callistemon viminalis</i>	Weeping Bottlebrush
<i>Cercidium 'Desert Museum'</i>	Desert Museum Palo Verde
<i>Cercidium floridum</i>	Blue Palo Verde
<i>Cercis occidentalis</i>	Western Redbud
<i>Cercis canadensis 'Hearts of Gold'</i>	Hearts of Gold Redbud
<i>Chilopsis linearis 'Burgundy Lace'</i>	Burgundy Lace Desert Willow
<i>Chitalpa tashkentensis</i>	Chitalpa
<i>Cinnamomum camphora</i>	Camphor Tree
<i>Eriobotrya japonica</i>	Loquat
<i>Eriobotrya deflexa</i>	Bronze Loquat
<i>Erythrina caffra</i>	Kaffirboom Coral Tree
<i>Fraxinus oxycarpa 'Raywood'</i>	Raywood Ash
<i>Geijera parviflora</i>	Australian Willow
<i>Ginkgo biloba</i>	Maidenhair Tree
<i>Juglans californica</i>	California Walnut
<i>Juglans hindsii</i>	California Black Walnut
<i>Koelreuteria paniculata</i>	Goldenrain Tree
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Laurus nobilis 'Saratoga'</i>	Saratoga Laurel
<i>Lophostemon confertus</i>	Brisbane Box
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Olea europaea 'Wilsonii', 'Swan Hill'</i>	Fruitless Olive Tree
<i>Pinus edulis</i>	Colorado Pinyon
<i>Pinus halepensis</i>	Aleppo Pine
<i>Pistacia chinensis</i>	Chinese Pistache
<i>Platanus x acerifolia</i>	London Plane Tree
<i>Platanus racemosa 'Bloodgood'</i>	Bloodgood London Plane Tree
<i>Prosopis chilensis</i>	Thornless Chilean Mesquite
<i>Pyrus calleryana 'Aristocrat'</i>	Aristocrat Pear
<i>Stenocarpus sinuatus</i>	Firewheel Tree
<i>Tabebuia impetiginosa</i>	Pink Trumpet Tree
<i>Tecoma stans</i>	Yellow Bells
<i>Quercus spp.</i>	Oak
<i>Ulmus parvifolia 'True Green'</i>	True Green Evergreen Elm

BOTANICAL NAMECOMMON NAMESHRUBS

<i>Achillea filipendulina</i>	Fern Leaf Yarrow
<i>Achillea millefolium</i>	Yarrow
<i>Agave</i> spp.	Agave
<i>Aloe</i> spp.	Aloe
<i>Anigozanthos</i>	Kangaroo Paw
<i>Anisacanthus quadrafidus</i> var. <i>Wrightii</i>	Flame Acanthus
<i>Aristida purpurea</i>	Purple Three Awn
<i>Bouteloua gracilis</i>	Blue Grama Grass
<i>Bulbine frutescens</i>	Stalked Bulbine
<i>Calliandra californica</i>	Baja Fairy Duster
<i>Calliandra eriophylla</i>	Pink Fairy Duster
<i>Callistemon viminalis</i> 'Little John'	Little John Callistemon
<i>Carex tumulicola</i>	Berkeley Sedge
<i>Ceanothus</i> spp.	California Lilac
<i>Cistus</i> x <i>pulverulentus</i> 'Sunset'	Sunset Rockrose
<i>Cistus</i> x <i>salviifolius</i>	Sageleaf Rockrose
<i>Correa pulchella</i>	Pink Australian Fuchsia
<i>Dalea capitata</i>	Lemon Dalea
<i>Dianella revoluta</i> 'Little Rev'	Little Rev Flax Lily
<i>Dianella tasmanica</i>	Variiegated Flax Lily
<i>Dietes bicolor</i>	Fortnight Lily
<i>Dodonaea viscosa</i> 'Purpurea'	Purple Hopseed
<i>Dudleya pulverulento</i>	Chalk Liveforever
<i>Elaeagnus pungens</i>	Silverthorn
<i>Epilobium canum</i>	California Fuchsia
<i>Eriophyllum confertiflorum</i>	Golden Yarrow
<i>Euphorbia rigida</i>	Silver Spurge
<i>Festuca mairei</i>	Atlas Fescue
<i>Hesperaloe parviflora</i>	Texas Yucca
<i>Junus patens</i>	Gray Rush
<i>Justicia californica</i>	Chuparosa
<i>Kniphofia uvaria</i>	Torch Lily
<i>Lantana</i> spp.	Lantana
<i>Lavandula</i> spp.	Lavender
<i>Leucophyllum frutescens</i> + cvs	Texas Ranger
<i>Leymus condensatus</i> 'Canyon Prince'	Canyon Prince Wild Rye
<i>Lupinus albifrons</i>	Silver Bush Lupine
<i>Nolina parryi</i>	Parry's Bear Grass

BOTANICAL NAMECOMMON NAMESHRUBS

Olea europea 'Little Ollie'	Dwarf Olive
Penstemon spp.	Penstemon
Phlomis fruticosa	Jerusalem Sage
Phormium spp.	New Zealand Flax
Pittosporum spp.	Pittosporum
Rhaphiolepis spp.	Indian Hawthorn
Romneya coulteri	Matilija Poppy
Rosemarinus spp.	Rosemary
Santolina spp.	Lavender Cotton
Senna artimisioides	Feathery Cassia
Stachys byzantina	Lamb's Ear
Trichostema lanatum	Woolly Blue Curls
Westringia fruticosa	Coast Rosemary
Yucca filamentosa	Yucca

GROUNDCOVERS

Acacia redolens 'Low Boy'	Prostrate Acacia
Carex flacca	Blue Sedge
Carex praegracilis	Clustered Field Sedge
Dalea greggii	Trailing Indigo Bush
Dymondia margaretae	Silver Carpet
Elymus triticoides	Creeping Wild Rye
Lantana montevidensis	Trailing Lantana
Lantana x 'New Gold'	New Gold Lantana
Myoporum parvifolium + cvs	Creeping Myoporum
Rosmarinus officinalis 'Prostrate'	Creeping Rosemary
Senecio spp.	Blue Chalksticks

** In an effort to reduce wildlife hazards to aircraft operations, plant palette priority shall be given to plants listed in the Riverside County Airport Land Use Commission's "Landscaping Near Airports" brochure. See Specific Plan Section 4.5.1 - Plant Palette for additional information.*