



# MARCH JOINT POWERS AUTHORITY

## IMPORTANT COVID-19 NOTICE

IN AN EFFORT TO PROTECT PUBLIC HEALTH AND PREVENT THE SPREAD OF COVID-19 (CORONAVIRUS) AND TO ENABLE APPROPRIATE SOCIAL DISTANCING, THE AUTHORITY ENCOURAGES MEMBERS OF THE PUBLIC TO WATCH THE MARCH BUSINESS CENTER DESIGN IMPLEMENTATION COMMITTEE MEETING REMOTELY AND NOT TO ATTEND IN PERSON. IF YOU WOULD LIKE REMOTE ACCESS TO VIEW THE MEETING, PLEASE EMAIL THE CLERK AT [ALLEN@MARCHJPA.COM](mailto:ALLEN@MARCHJPA.COM) BY 5:00 P.M. ON MONDAY, AUGUST 23RD, 2021 AND WE WILL PROVIDE REMOTE ACCESS INSTRUCTIONS.

MEMBERS OF THE PUBLIC WHO WISH TO ATTEND IN PERSON MUST BE WEARING A MASK TO ENTER THE WESTERN MUNICIPAL WATER DISTRICT/MARCH JPA BUILDING. WITH SPACE STRICTLY LIMITED, WE STRONGLY ENCOURAGE MEMBERS OF THE PUBLIC TO UTILIZE THE REMOTE ACCESS APPROACH DESCRIBED ABOVE OR TO SIMPLY PROVIDE COMMENT ON ITEMS OF INTEREST THROUGH THE METHODS BELOW.

MEMBERS OF THE PUBLIC WHO WISH TO COMMENT ON MATTERS BEFORE THE COMMITTEE MAY ALSO PARTICIPATE IN THE FOLLOWING WAYS:

(1) COMMENTS AND CONTACT INFORMATION CAN BE EMAILED TO [ALLEN@MARCHJPA.COM](mailto:ALLEN@MARCHJPA.COM) BY 1 P.M. ON THE DAY OF THE SCHEDULED MEETING TO BE INCLUDED IN THE WRITTEN RECORD; OR

(2) A REQUEST TO SPEAK CAN BE EMAILED TO [ALLEN@MARCHJPA.COM](mailto:ALLEN@MARCHJPA.COM) AND, AT THE TIME OF THE REQUESTED AGENDA ITEM, THE CLERK WILL PLACE A PHONE CALL TO THE COMMENTER AND ALLOW THEM TO SPEAK TO THE COMMITTEE VIA SPEAKER PHONE DURING THE LIVE MEETING FOR UP TO THREE MINUTES. PLEASE INDICATE ON WHICH ITEM YOU WISH TO SPEAK.

ONLY ONE PERSON AT A TIME MAY SPEAK BY TELEPHONE AND ONLY AFTER BEING RECOGNIZED BY THE CHAIR.

PLEASE BE MINDFUL THAT THE TELECONFERENCE WILL BE RECORDED AS ANY OTHER MEETING IS RECORDED, AND ALL OTHER RULES OF PROCEDURE AND DECORUM WILL APPLY WHEN ADDRESSING THE COMMITTEE BY TELECONFERENCE. FINALLY, IT IS REQUESTED THAT ANY MEMBER OF THE PUBLIC ATTENDING WHILE ON THE TELECONFERENCE TO HAVE HIS/HER/THEIR PHONE SET ON "MUTE" TO ELIMINATE BACKGROUND NOISE OR OTHER INTERFERENCE.

To Join March Joint Powers Authority Commission Meeting Via Zoom:

<https://us02web.zoom.us/j/6573841741?pwd=L0oYcW0zRHFiUWw1RmtnajFMTmlPQT09>

Meeting ID: 657 384 1741    Password: 14205  
One tap mobile (from mobile or electronic device)  
+16699006833,,6573841741# US (San Jose)  
+13462487799,,6573841741# US (Houston)

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# NOTICE TO THE PUBLIC

and to the

## MEMBERS OF THE MARCH BUSINESS CENTER IMPLEMENTATION COMMITTEE

of the

### MARCH JOINT POWERS AUTHORITY

Meeting will be held

**Wednesday, August 25 – 11:00 AM**

VIA ZOOM from the March Joint Powers Authority Office  
14205 Meridian Parkway, Suite 140  
Riverside, CA 92518

Virtually via zoom, the link for the meeting is found here:

<https://us02web.zoom.us/j/6573841741?pwd=L0oycW0zRHFyUWw1RmtnajFMTmlPQT09>

I hereby certify that the foregoing notice is a full, true and correct copy of a notice sent to the following locations:

1. County of Riverside  
County Administrative Center  
4080 Lemon Street  
Riverside, CA 92501
2. City of Perris  
City Hall  
101 North D Street  
Perris, CA 92570
3. City of Riverside  
City Hall  
3900 Main Street  
Riverside, CA 92522
4. City of Moreno Valley  
City Hall  
14177 Frederick Street  
Moreno Valley, CA 92553
5. March Joint Powers Authority  
Office  
14205 Meridian Parkway, Suite 140  
Riverside, CA 92518

I hereby certify that a copy of the foregoing notice was dispatched by me on August 19, 2021, to each member of the March Business Center Implementation Committee of the March Joint Powers Authority.

*Carey L. Allen*

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Carey L. Allen, CMC, Clerk  
March Joint Powers Authority

**Meeting of the  
March Business Center Implementation Committee  
of the  
MARCH JOINT POWERS AUTHORITY**

**Wednesday, August 25, 2021 - 11:00 AM**

**VIA ZOOM from the March Joint Powers Authority Office  
14205 Meridian Parkway, Suite 140  
Riverside, CA 92518**

**AGENDA**

**1. Call to Order**

**2. Roll Call**

**3. Approval of the Minutes of the Meeting held on June 28, 2021 (Page 4)**

**4. Public Comments**

*Any person may address the Commission on any subject pertaining to the March Joint Powers Authority, March Inland Port Airport Authority, the Successor Agency to the March Joint Powers Redevelopment Agency, and March Joint Powers Utilities Authority business not listed on the Agenda during this portion of the Meeting. A limit of three (3) minutes shall be set for each person desiring to address the Commission. The JPC may not act on these matters except to refer the matters to staff or schedule the matters for a future agenda.*

**4. Discussion and Action Item**

- a) **Design Plan (DP 21-01) Building F:** A request for Ministerial Design Plan approval in compliance with the March Business Center Specific Plan (SP-1, Amendment #8) Section VII. d (b) for an application by Meridian Park LLC, for Development Plan 21-02, a package delivery facility project located on Parcels 19 and 20 of the March Business Park, South Campus. The Design Plan would grant approval for the Site Plan, Floor Plan, Landscape Plan, Building Elevations and preliminary Grading Plans and would authorize the applicant to proceed with Construction Plan submittal. The March Business Center Design Implementation Committee will also consider a determination that the Design Plan approval is exempt from the California Environmental Quality Act (CEQA) under Public Resources Code Section 21080(a) and (b)(1) and Sections 15268(a) and 15369 of the CEQA guidelines and Direct Staff to file a Notice of Exemption (Page 6)

**5. Adjournment**

**March Joint Powers Authority  
14205 Meridian Parkway, Ste 140, Riverside, CA 92518  
Phone: (951) 656-7000 FAX: (951) 653-5558**

**Meeting of the  
March Business Center Implementation Committee  
of the  
MARCH JOINT POWERS AUTHORITY**

**Monday, June 28, 2021 - 2:30 PM**

**March Joint Powers Authority Office  
Meridian Conference Room  
14205 Meridian Parkway, Suite 140  
Riverside, CA 92518**

**MEETING MINUTES**

**1. Call to Order**

Chair Melendrez called the meeting to order at 2:30 p.m.

**2. Roll Call**

Present: Baca, Jeffries, Melendrez

**3. Approval of the Minutes of the Meeting held on August 5, 2020**

Motion to approve minutes: Baca

Second by: Jeffries

**4. Public Comments**

*Any person may address the Commission on any subject pertaining to the March Joint Powers Authority, March Inland Port Airport Authority, the Successor Agency to the March Joint Powers Redevelopment Agency, and March Joint Powers Utilities Authority business not listed on the Agenda during this portion of the Meeting. A limit of three (3) minutes shall be set for each person desiring to address the Commission. The JPC may not act on these matters except to refer the matters to staff or schedule the matters for a future agenda.*

Mathew Evans, March JPA Planning Director read a letter from Richard Drury from Lozeau Drury, LLP.

No public comments.

**4. Discussion and Action Item**

- a) **Design Plan (DP 21-01):** The proposed Meridian South Campus Building F project is located on a 26.3-acre parcel that was previously mass graded along with the overall South Campus project, located north of the intersection of Krameria Avenue and Bundy Avenue. The project consists of a proposed 517,500 square foot (SF) industrial warehouse building, 159,801 (SF) of landscaping (14% of overall site), 246 vehicle parking spaces, 119 truck/trailer parking spaces along with associated drive aisles, a 12' tall JPA Standard decorative screen wall to screen the truck courts, as well as the necessary utilities. Access to the site is achieved by two proposed driveways from Krameria Avenue, and overall access to the site is achieved from Van Buren Boulevard and Village West Drive. The site is adjacent to the Eagle Business Park to the west, vacant developable land to the east,

an existing LLMD easement and the Least Bell's Vireo Conservation Easement to the north, and Krameria Avenue/the Amazon Warehouse and vacant developable land to the south.

*Mathew Evans provided an update on this project.*

Member Jeffries asked if there cannot be any mitigation requirements. Mr. Evans answered, correct. Member Jeffries asked if this warehouse has to be approved without mitigation requirements. Mr. Evans answered, correct. Mr. Evans added that the mitigation requirements were addressed when the amendment was done back in January and technically the mitigation measures are already in place. Member Jeffries asked if the traffic enforcement was part of the mitigation. Mr. Evans answered yes, the traffic enforcement is part of those mitigation measures. Member Jeffries stated that it was approved in January but was never signed and put into place so that mitigation does not exist as required.

Timothy Reeves, Lewis Companies stated that it was his understanding that there were a few items left to be negotiated between the JPA, the County and the CHP. Then it was going to be signed. Mr. Reeves added that this is the first he's heard about it in a month. Mr. Reeves continued to say that from the JPA stand-point, he believes that it was going to be worked out between the three parties. Member Jeffries stated that the issue is all of the bleed-over that is occurring on the side streets. He added that Van Buren and Cajalco Roads have both become truck routes and they were not designed for that. Without mitigation in place to attempt to divert that traffic, land use issues have been created. Mr. Reeves asked if a building permit could be pulled until that agreement is in place for this particular building. Mr. Evans answered that if the Implementation Committee wants to do a condition of approval, they can certainly do that. Mr. Reeves asked Member Jeffries what would make him comfortable. Member Jeffries answered that he is hesitant to put his name on anything until he knows what the truth is. Chair Melendrez stated that he has similar concerns as Member Jeffries.

The JPA staff paused the meeting for a brief discussion. Upon further open discussion, March JPA Interim Director, Lori Stone stated that CHP is enforcing traffic at the moment without the agreement and they are comfortable that they will get the agreement signed.

Chair Melendrez asked how the stacking will be handled. Mr. Reeves stated that are three gates and the main gates are right-turn in. There is enough space for up to ten trucks to be stacked before getting to the street. Both gates allow for two to three truck stacking. Mr. Reeves stated that they will promote the eastern entrance as there is the most space on the east side. Mr. Reeves added that the east side was analyzed from a CEQA standpoint in January. They analyzed the highest impact the building would have.

No public comments.

Motion to approve Discussion and Action Item 4(a).

Motion: Baca

Second: Melendrez

Ayes: Baca, Melendrez

Noes: Jeffries

## **5. Adjournment**

The meeting adjourned at 3:07 p.m.

**TO: MARCH BUSINESS CENTER IMPLEMENTATION COMMITTEE**

**FROM: MATHEW EVANS, PLANNING DIRECTOR**

**DATE: AUGUST 25, 2021**

**SUBJECT: A REQUEST FOR MINISTERIAL DESIGN PLAN APPROVAL IN COMPLIANCE WITH THE MARCH BUSINESS CENTER SPECIFIC PLAN (SP-1, AMENDMENT #8) SECTION VII. D (B) FOR AN APPLICATION BY MERIDIAN PARK LLC, FOR DEVELOPMENT PLAN 21-02, A PACKAGE DELIVERY FACILITY PROJECT LOCATED ON PARCELS 19 AND 20 OF THE MARCH BUSINESS PARK, SOUTH CAMPUS. THE DESIGN PLAN WOULD GRANT APPROVAL FOR THE SITE PLAN, FLOOR PLAN, LANDSCAPE PLAN, BUILDING ELEVATIONS AND PRELIMINARY GRADING PLANS AND WOULD AUTHORIZE THE APPLICANT TO PROCEED WITH CONSTRUCTION PLAN SUBMITTAL. THE MARCH BUSINESS CENTER DESIGN IMPLEMENTATION COMMITTEE WILL ALSO CONSIDER A DETERMINATION THAT THE DESIGN PLAN APPROVAL IS EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) UNDER PUBLIC RESOURCES CODE SECTION 21080(A) AND (B)(1) AND SECTIONS 15268(A) AND 15369 OF THE CEQA GUIDELINES AND DIRECT STAFF TO FILE A NOTICE OF EXEMPTION**

Recommended Motions:

1. The March Business Center Design Implementation Committee (“Committee”) finds that the application is consistent with the requirements of the March Business Center Design Guidelines (“Design Guidelines”), the March Business Center Specific Plan (SP-1, A8) (“Specific Plan”), and the Certified Focused Environmental Impact Report and Mitigation Measures for the March Business Center Specific Plan (SCH #2002071089) (“Specific Plan FEIR”).
2. The Committee adopts the findings identified in this Staff Report.
3. The Committee has reviewed the proposed Design Plan, March Business Center Specific Plan Land Use Plan, applicant’s narrative, March Business Center Design Implementation Committee Checklist, Design Guidelines, March Business Center Development Standards, Specific Plan, and Specific Plan FEIR, staff report, and other related land use and environmental documents, based on that review, and approves Design Plan 21-02, subject to the Standard Conditions of Approval for the project.

4. The Committee's review is limited to determining whether Design Plan 21-02 is consistent with the Design Guidelines, Specific Plan, and the Specific Plan FEIR. As such, approval of Design Plan 21-02 is a ministerial action. The Committee finds that the approval is thus not a "project" subject to the California Environmental Quality Act ("CEQA") because CEQA only applies to discretionary activities. (Pub. Res. Code § 21080(a).) Further, even if the approval is a project, the Committee finds that the approval is exempt from CEQA because CEQA does not apply to ministerial activities. (Pub. Resources Code § 21080(b)(1); CEQA Guidelines §§ 15268(a) and 15369.) The Committee directs staff to file a Notice of Exemption consistent with CEQA and the Authority's CEQA Guidelines.

**Applicant:** Meridian Park, LLC.

**Background:**

The Committee was created on February 26, 2003, through the adoption of the March Business Center Specific Plan and incorporated into the Meridian Specific Plan (which is the amendment to the original March Business Center Specific Plan) through the adoption of Ordinance #JPA 10-02. As identified in Section VII. D of Specific Plan, the Committee reviews Design Review Applications such as the proposed Design Plan 21-02 through a ministerial review process.

The Committee's review of the proposed development does not involve the exercise of judgment, discretion, or deliberation, but is merely a ministerial determination of whether the proposed plan complies with the Design Guidelines, Specific Plan and Specific Plan FEIR. Project denial by the Committee is appealable to the March Joint Powers Commission (JPC) in conformance with Section 9.02.240 of the March Joint Powers Authority Development Code.

In *Health First vs. March Joint Powers Authority* (2009) 174 Cal.App.4th 1135, the Court of Appeal upheld the validity of this ministerial review process and the checklist, both of which are utilized for this application.

On June 28, 2021, the Committee approved Design Plan (DP) 21-01 for the Meridian South Campus, Building F for Parcel 19. The project included a 517,000 SF industrial warehouse building located on 26.3-acres. This proposed project, if approved, will supersede the approval of DP 21-01.

**TAC Review:**

Design Plan (DP) 21-02 was presented to the Technical Advisory Committee (TAC) on August 4, 2021, as a Courtesy Review. Because the Design Plan requires approval by the March Business Center Design Implementation Committee and not the JPC, no formal action by TAC is required. Staff presented the item to TAC and solicited questions or comments from TAC members. There were no questions asked of staff, and a comment in support for the project was made by the City of Perris TAC representative.



## Development Overview

### Project Location:

The Meridian South DJT6 Building and Development (“Project”) is located on a 44.8-acre industrial zoned site that was previously mass graded along with the overall Meridian Park South Campus project and located north of the intersection of Krameria Avenue and Bundy Avenue. The Project site is adjacent to the Eagle Business Park to the west, vacant developable land to the east, an existing LLMD easement and the Least Bell’s Vireo Conservation Easement to the north, and Krameria Avenue/Amazon Warehouse, vacant developable land, and parkland to the south. Access to the site is achieved by four proposed driveways from Krameria Avenue, and overall access to the site is achieved from Van Buren Boulevard and Village West Drive. The proposed project is located on Parcels 19 and 20 of Parcel Map 37878 (APN’s 294-100-034, 294-120-047, 294-120-046 and 294-100-034) (see Figure 1 below).

**FIGURE 1: PROJECT LOCATION**



### Project Description:

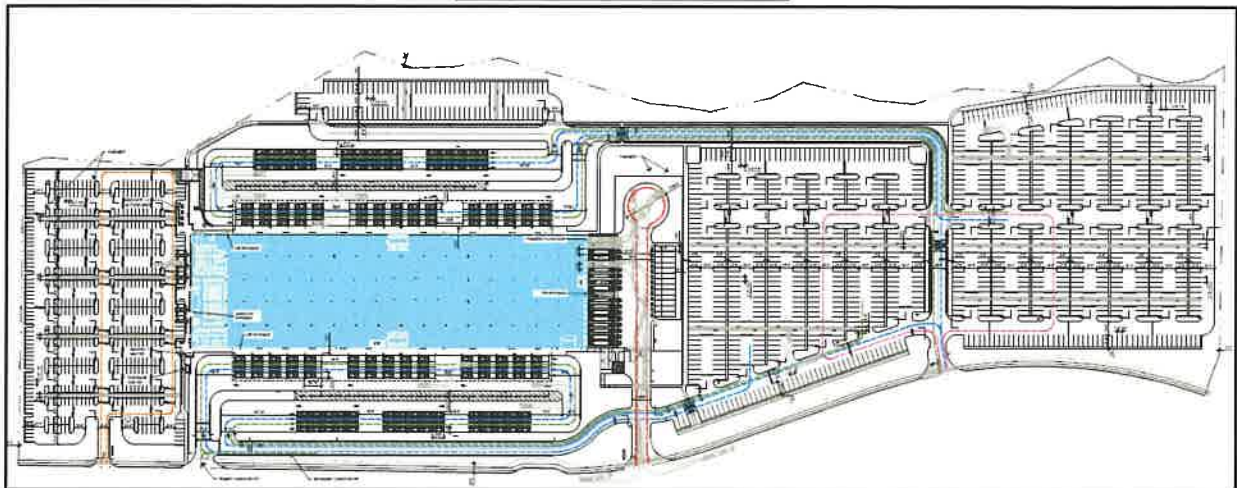
Today the proposed Project consists of a 219,000 square foot delivery station facility with 16 dock doors and a building height of 48 feet to the top of the parapets. The Project proposes 17% landscaping coverage (10% required), 449 vehicle parking spaces, including 7 ADA spaces and 2 ADA-Van spaces, 1024 van parking spaces, 6 bicycle parking spaces, and 13 truck/trailer parking stalls along with associated drive aisles, as well as necessary utilities. It should be noted that the proposed building represents a reduction of approximately 300,000 SF from the originally approved building F, which was located on one parcel consisting of 22 acres.

The proposed Project has four access points on Krameria Avenue, two located to the west of Bundy Avenue, one aligning with Bundy Avenue, and one to the east on Bundy Avenue.



- The westerly driveway (Driveway 1) is approximately 25-feet wide and would provide access into the delivery station employee parking field (passenger cars only).
- Driveway 2 is approximately 30-feet wide and would provide access to exiting delivery vehicles which are only loading on the south side of the building.
- Driveway 3 is 40-feet wide and is proposed to align with the intersection of Bundy Avenue and Krameria Avenue, creating a 4-leg intersection. Driveway 3 would serve both inbound and outbound trucks only.
- Driveway 4 is the easterly driveway on Krameria Avenue and is approximately 30-feet wide. It is anticipated to serve all inbound delivery vans loading on the north and south sides of the building and departing delivery vans that are loaded on the north side of the building. This driveway will also serve delivery van drivers who park their personal vehicles on-site (see Figure 2, Site Plan, below).

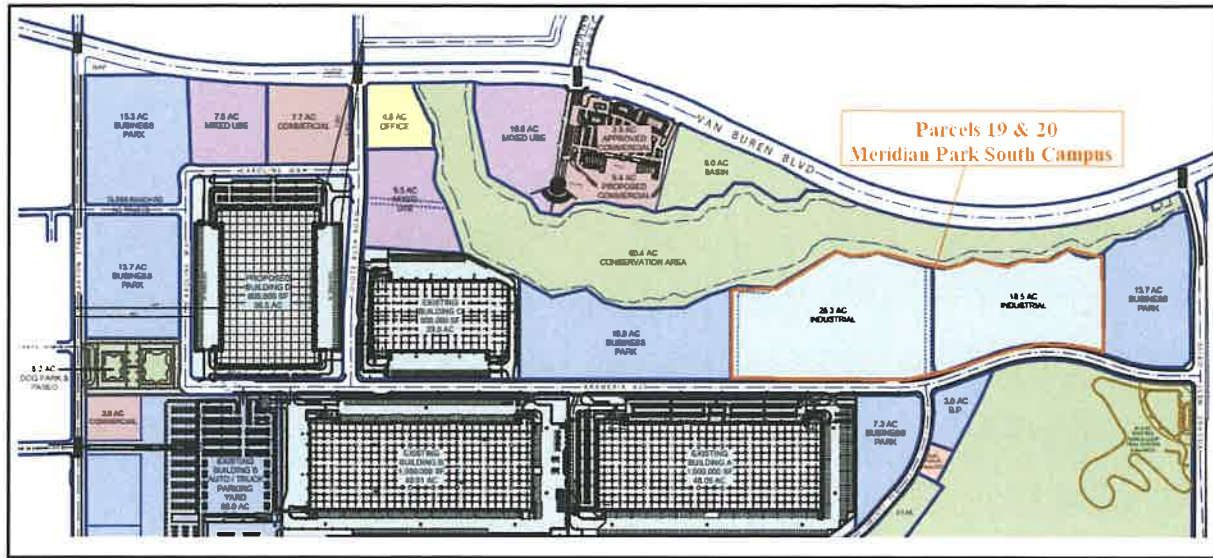
**FIGURE 2: SITE PLAN**



### **Zoning and Land-Use**

The project is located on Parcels 19 and 20 within the South Campus of the March Business Center Specific Plan (SP 1, A8) and is comprised of 44-8 acres. The General Plan, Zone and Land-Use designations for the site is “Industrial” as identified within the March Business Center Specific Plan (SP-1) Amendment 8 (A8). The proposed delivery station facility is an “Approved” use within the Industrial Zone designation of the SP and is classified within the SP Zoning Matrix as a “Parcel Delivery Terminal” (see Figure 3 below).

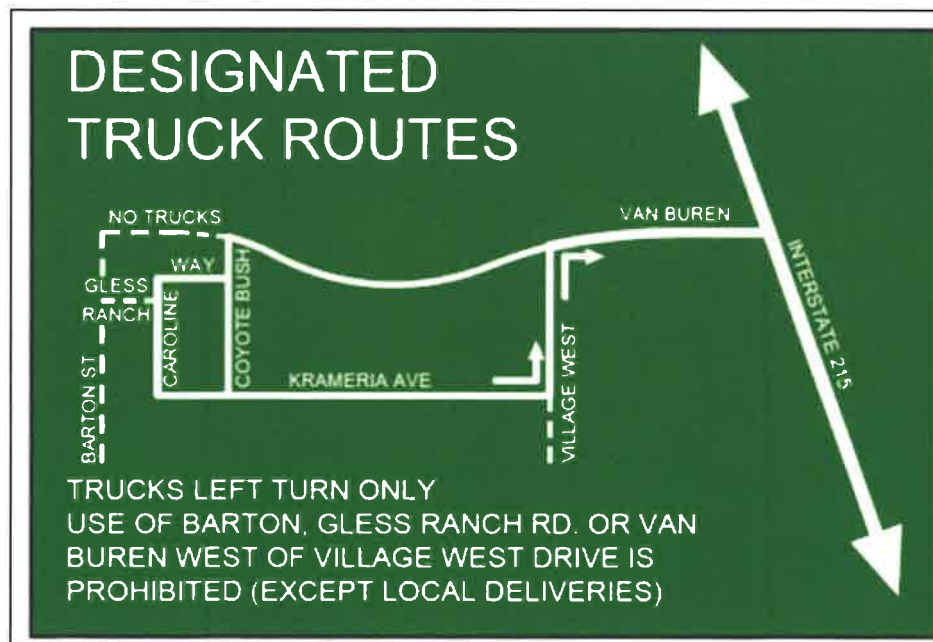
### **FIGURE 3: LAND-USE DESIGNATION**



**Traffic:**

A Traffic Consistency Memo was previously reviewed and approved by the March JPA Traffic Consultant for the Building F project. A revised traffic consistency memo is currently under review by the JPA Traffic Consultant. The new project represents a significant reduction in truck traffic to and from the site. Truck traffic to the site is expected from I-215 to the east, Village West Drive to the north and Krameria Avenue. Truck traffic from the site is required to use Krameria Avenue eastbound only, Village West Drive northbound only, and Van Buren Boulevard eastbound only.

### FIGURE 5 – DESIGNATED TRUCK ROUTE SIGN



## **Building Elevations**

Staff has reviewed the proposed Building Elevations for consistency with the March Business Center Design Guidelines and has found the elevations to be consistent with those requirements. The proposed Building Elevations include one (1) 219,000 square foot delivery station facility with 16 dock doors and a total building height of 48 feet to the top of the parapets. The previous project included one (1) 517,500 square feet with a building height of 47 feet to the top of the parapet, a proposed internal mezzanine and 68 dock-high and 12 grade-level doors for trucks. The overall look of the proposed building is consistent with other buildings within the South Campus and Meridian Park developments. The single-tower end includes an Anodized Aluminum storefront with Solarcool Pacifica – Clear glazing. The remainder of the building is tilt-up concrete construction with four (4) different color variations (Attachment A2 – also see Figure 4 below).

**FIGURE 4 – BUILDING ELEVATIONS**





### **Landscaping**

Landscaping for the site consists of approximately 358,138 square feet or 17%(+) of the overall site, which exceeds the minimum requirement of 10%. The previously approved project proposed 14%. Staff has reviewed the landscaping and has found that the conceptual landscape plans are also consistent with the March Business Center Specific Plan guidelines. Including the landscape pallet and plant materials. Landscaped areas are largely located along Krameria Avenue to the south and adjacent to the LLLMD to the north. LLMD landscaping along Krameria Avenue will be separated from the onsite landscaping using cement mow-curb. Landscaping is also provided within the interior of the site, including within the parking lots, along the drive aisles, and the side-yard setback areas (see Figure 6 below). The truck trailer courtyard area to the east of the building will be required to be screened by a Meridian Standard 12' screen wall, and the remainder of the property will be screened by a Meridian standard wrought-iron and stack stone pillar fence.

**FIGURE 6 – LANDSCAPE PLAN**



### **ALUC Review**

According to ALUC, this project is located within Zone C2 (High Terrain Zone) of March Air Reserve Base ALUCP. The project is also located within the March JPA: March Business Center/Meridian exception area. According to the MJP documentation, this site is located in area which has a current or extended development agreement. Furthermore, because the project is not proposing any legislative actions, ALUC review is not required in this case.

### **Surrounding Area/Land Use:**

Location	GP Designation/Zoning	Existing Land Use
North	Open Space	March JPA LLMD and the LBV Conservation Easement
West	Business Park	4 existing shell buildings (currently under construction)

<b>South</b>	<b>Business Park</b>	<b>Vacant land/Existing Amazon warehouse building</b>
<b>East</b>	<b>Industrial</b>	<b>Vacant land</b>

### **Compliance with Development Standards:**

The proposed project is located within the March Business Center Specific Plan and is subject to the development standards within the SP and Design Guidelines. Figure

<b>Proposed Development</b>		
<b>Development Standard</b>	<b>Minimum/Maximum Required</b>	<b>Proposed Development</b>
<b>Gross Site Area</b>	30,000 square feet (minimum)	44.8 acres
<b>Number of Buildings (range in size)</b>	One (1)	One (1) building totaling: 219,000 sf
<b>Floor Area Ratio (Maximum)</b>	50% or 975,744 sf	11% (+/-) or 219,000 sf
<b>Setbacks (both Buildings)</b>	Front: 20 feet Interior side: 0 ft. Street side: 20 ft. Rear: 25 ft.	Front: 200+ ft. Interior side: 200+ ft. - 500+ ft Street side: No Street Adjacent Rear: 200+ ft.
<b>Landscape Required (Minimum)</b>	10% or 1,951 sf	17% or 358,138 sf (excludes LLMD easement area).
<b>Building Height (Maximum)</b>	50 ft./ 2 stories	48 ft to top of parapet roof
<b>Parking</b>	258 stalls	258 stalls – does not include 1,024 van parking stalls or van queuing or loading.

### **Development Analysis:**

*Site Planning:* The proposed development meets all the minimum standards identified in the Specific Plan. Specifically, the proposed building height, setbacks, floor area ratio and landscape

requirements are compliant with the Specific Plan. Employee parking, carpool, bicycle, and ADA parking spaces provided on the proposed site plan meet the minimum parking standards required for a mix of Light Manufacturing and Wholesale Storage and Distribution uses. FAA Part 77 calculations were submitted to the Federal Aviation Administration (FAA) pursuant to 49 U.S.C. Section 44718 and Title 14 of the Code of Federal Regulations (Part 77), and on April 19, 2021, the FAA issued a Determination of No Hazard to Air Navigation acknowledging that the proposed structure “does not exceed obstruction standards and would not be a hazard to air navigation.” Any modifications as it relates to an increase in the building structure height must be submitted to the FAA under a 7460-2, Part 2 review process.

A Preliminary Water Quality Management Plan has been submitted and approved for the site. A Final Water Quality Management Plan will be signed and notarized upon the completion of building construction and prior to Certificate of Occupancy. A conceptual drainage study has been completed for the site and has been reviewed by March JPA’s Engineering Department, NPDES consultant – CGRME and Riverside County Flood Control and Water Conservation District. Additionally, the project plans comply with the March Business Center Specific Plan (SP-1, A8), the March Business Center Design Guidelines and the March Business Center Specific Plan FEIR (SCH #2002071089).

#### *Architecture/Color Palette:*

The proposed building design for the building consists of one (1) 219,000 SF concrete tilt-up building with an Anodized Aluminum storefront and glass at the end tower. Prominent low reflectance glazing not exceeding 25% reflectance (Solarcool Pacifica glass) is proposed at the tower entrance and other locations around the building to provide visual interest. The truck trailer loading area and courtyard is required to be screened by a 12’ tall decorative Meridian standard wall, and the parking lot areas as well as the rear property line will be screened using a Meridian Standard wrought iron and stacked stone pillar fence which can be seen at various locations throughout the Meridian Campus. Furthermore, as the onsite and LLMD landscaping continues to mature and grow, it will eventually create a screen for most of the site from the street.

Building colors consist of an earth tone pallet with six (6) different color variations. The proposed building colors are consistent with Section 5.10 of the Design Guidelines. The proposed roof mounted equipment is designed to be fully screened by a parapet roof system. The Project architecture, color palette and glazing all comply with the requirements of Section 5.0 of the Design Guidelines.

#### *Site Landscaping:*

As previously discussed, the proposed project exceeds the 10% minimum landscaping requirements by a little over 17%. The planting pallet proposed is California friendly landscaping including compatible plantings to complement the existing LLMD plantings and hedge. The landscape plans show a variety of trees, ground cover, shrubs, and flowers to enhance the project. Additionally, a rear LLMD area consisting of existing slope ground cover, will remain and will be protected in place.

### *Site Signage:*

All signage is required to be consistent with the Design Guidelines, including building signage and freestanding signage. To date, Project signage has not been submitted but is required to be approved by March JPA planning staff prior to construction of any on-site signage.

### *Exterior Lighting:*

Given the proximity to the airport, the Design Guidelines require all on-site lighting to be fully shielded fixtures (no light emitted above horizon), high pressure or low-pressure sodium lighting, with a maximum 750 watts, maximum fixture height of 25' above finished grade, and maximum lighting of 0.5 candle/foot<sup>2</sup> at the property line. LED lighting is also permitted; however, the LED lighting must be at or within 100 Kelvin of 2,700 Kelvin. This information is noted in the site plan notes of Project plans and is consistent with the exterior lighting requirements from Section 7.0 Lighting of the Design Guidelines. Compliance with these requirements will be confirmed when construction plans are submitted.

### **Staff Recommendation:**

Staff will make the following recommendation to the Committee:

1. As recognized and approved in *Health First v. March Joint Powers Authority, supra*, review of the Design Plan application is a ministerial process that involves determining whether the application is consistent with the requirements, fixed standards, and mitigation identified in the Design Guidelines, Specific Plan, and the Specific Plan FEIR;
2. Based on the information contained in this report, the attached exhibits, the Ministerial Review Comprehensive Checklist, and the March JPA files and records, the Committee finds that the proposed Design Plan 21-02 is consistent with the requirements of the Design Guidelines, the Specific Plan, and the Specific Plan FEIR;
3. The Committee approves Design Plan 21-02, a Design Plan one Package Delivery Facility totaling 190,000 sf on 44.8 acres within the Industrial (Ind) designation of the March Business Center Specific Plan (SP-1, A-8) subject to the Project Standard Conditions of Approval; and
4. The Committee finds that the approval of Design Plan 21-02 does not involve the exercise of judgment, deliberation or discretion and instead involves only a determination of whether the proposed plan and uses proposed in the plan conform to the existing March Design Guidelines, Specific Plan, and the Specific Plan FEIR. Accordingly, the review is ministerial, and the Committee finds that approval of Design Plan 21-02 is:
  - (1) not a project under CEQA (Pub. Res. Code § 21080(a) because CEQA only applies to discretionary actions; and
  - (2) exempt from CEQA because ministerial actions are exempt from CEQA (Pub. Resources Code § 21080(b)(1); CEQA Guidelines §§ 15268(a) and 15369.)



5. The Committee directs staff to file a Notice of Exemption in accordance with CEQA and the March Joint Powers Authority's Local Guidelines for implementing CEQA. The documents that comprise the record of proceedings will be maintained at the offices of March Joint Powers Authority, located at 14205 Meridian Parkway, Suite 140, Riverside California 92518.

**Project Findings:**

Should the Committee find Design Plan 21-02 to be consistent with the Design Guidelines, Specific Plan, and Specific Plan FEIR, the Committee should approve Design Plan 21-02 and adopt the following findings:

1. Based on preparation and analysis of the Ministerial Review Comprehensive Checklist and the evidence in the record, the Committee determines that Design Plan 21-02 complies with all requirements of the March Business Center Specific Plan (SP-1, A8), the March Business Center Design Guidelines and the Final Focused Environmental Impact Report and Mitigation Measures for the March Business Center Specific Plan;
2. Design Plan 21-02 is consistent with the goals, objectives, policies, and programs of the March JPA General Plan and the March Business Center Specific Plan (SP-1, A8);
3. Design Plan 21-02, together with the Conditions of Approval thereto, will not be detrimental to the public health, safety, or welfare, or be materially injurious to properties or improvements in the vicinity.
4. Design Plan 21-02 is not a "project" subject to CEQA because CEQA only applies to discretionary actions. (Pub. Resources Code § 21080(a).) Approval of Design Plan 21-02 is not a discretionary action because the approval is based on a ministerial review process under which the Design Plan must be approved if it complies with the Design Guidelines, the Specific Plan, and the Specific Plan FEIR. The Committee does not exercise judgment, discretion, or deliberation in reviewing the design plan.
5. Design Plan 21-02 is exempt from CEQA because it is a ministerial approval. (Pub. Resources Code § 21080(b)(1); CEQA Guidelines §§ 15268, 15369) Approval of Design Plan 21-02 is ministerial review process under which the Design Plan must be approved if it complies with the Design Guidelines, the Specific Plan, and the Specific Plan FEIR. The Committee does not exercise judgment, discretion, or deliberation in reviewing the design plan.

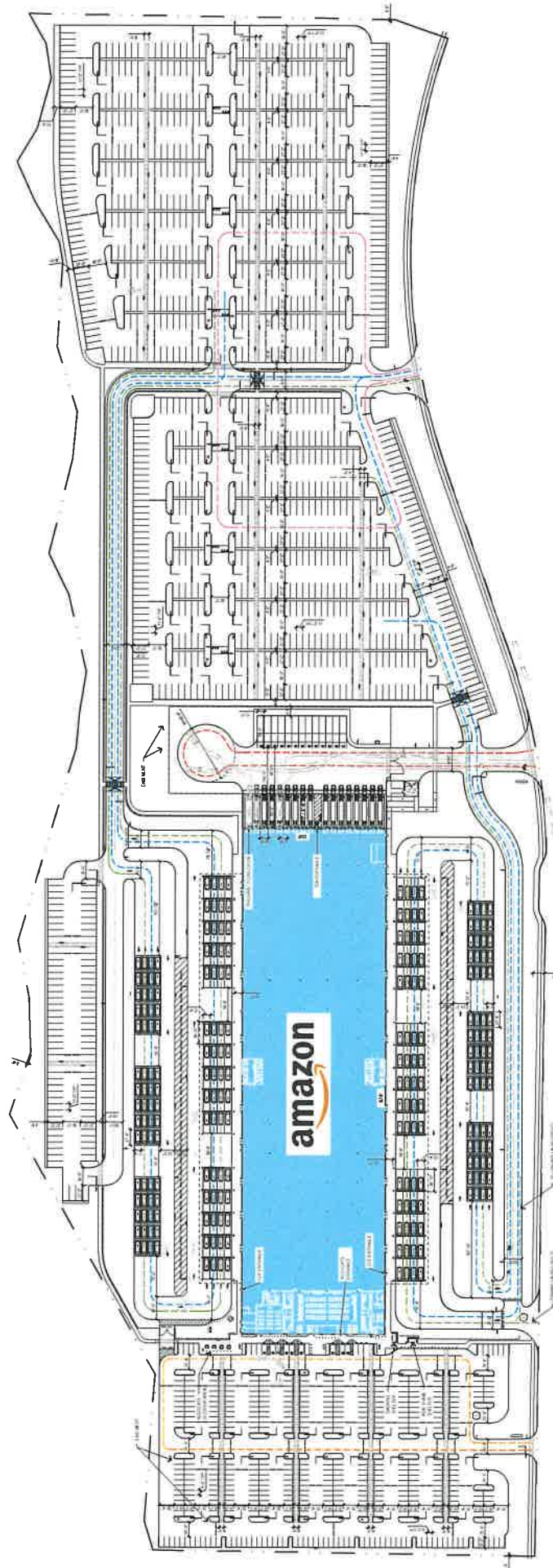
**Attachments:**

1. Reduced Site Plan.
2. Reduced Grading Plan.
3. Reduced Building Elevations.

4. Reduced Landscape Plan.
5. March Business Center Implementation Committee Ministerial Review Checklist.
6. Standard Conditions of Approval.
7. Notice of Exemption.
8. Approved Traffic Consistency Memo.

# Attachment 1

## Reduced Site Plan



SCALE: 1"=40'

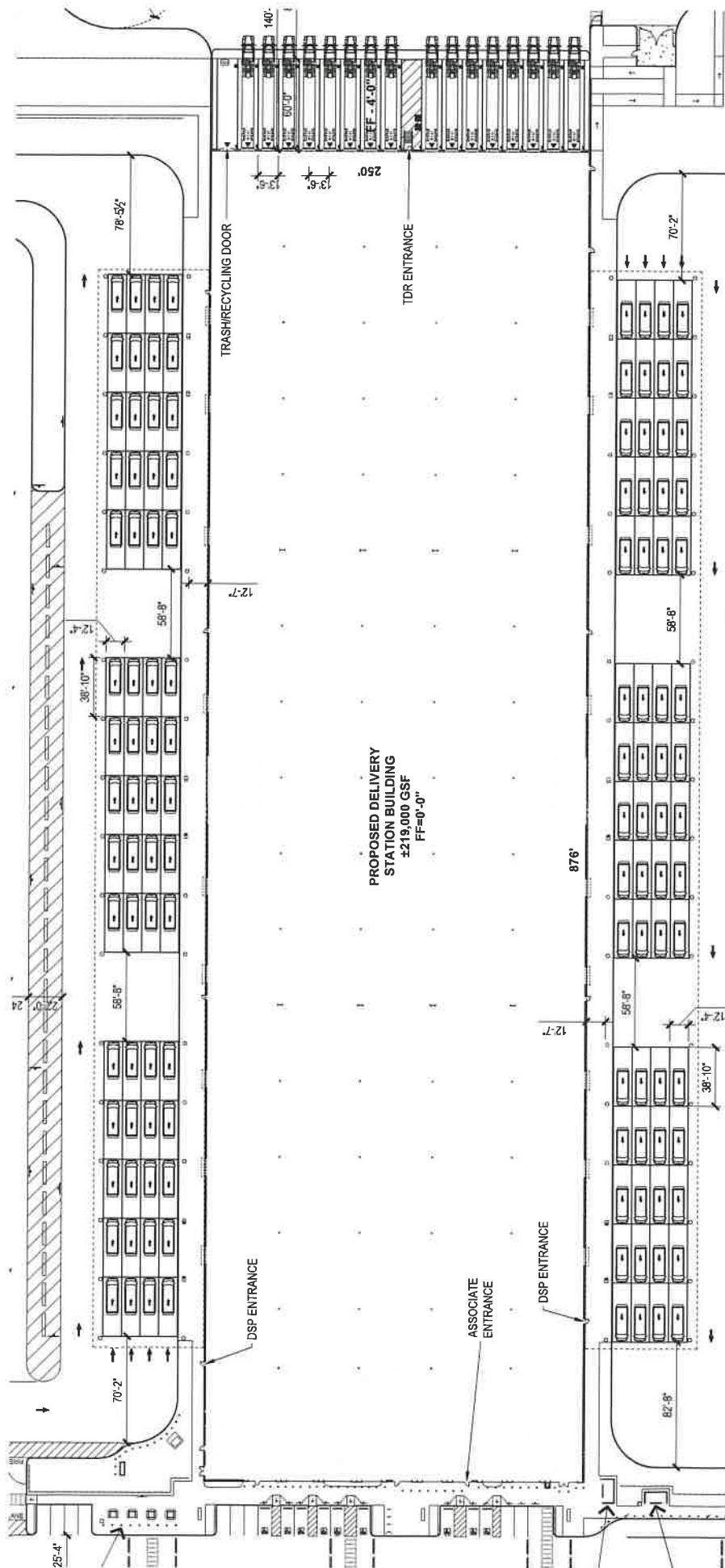
CONCEPTUAL SITE PLAN

**AMAZON - MERIDIAN PARK**  
Riverside, CA



01

2020-2021  
2021-2022  
2022-2023



1" = 10'-0"

CONCEPTUAL FLOOR PLAN



02

AMAZON - MERIDIAN PARK  
Riverside, CA



3/11/2016

Date

## Attachment 2

### Reduced Grading Plans



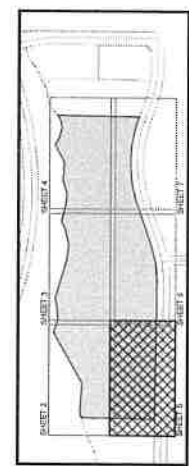
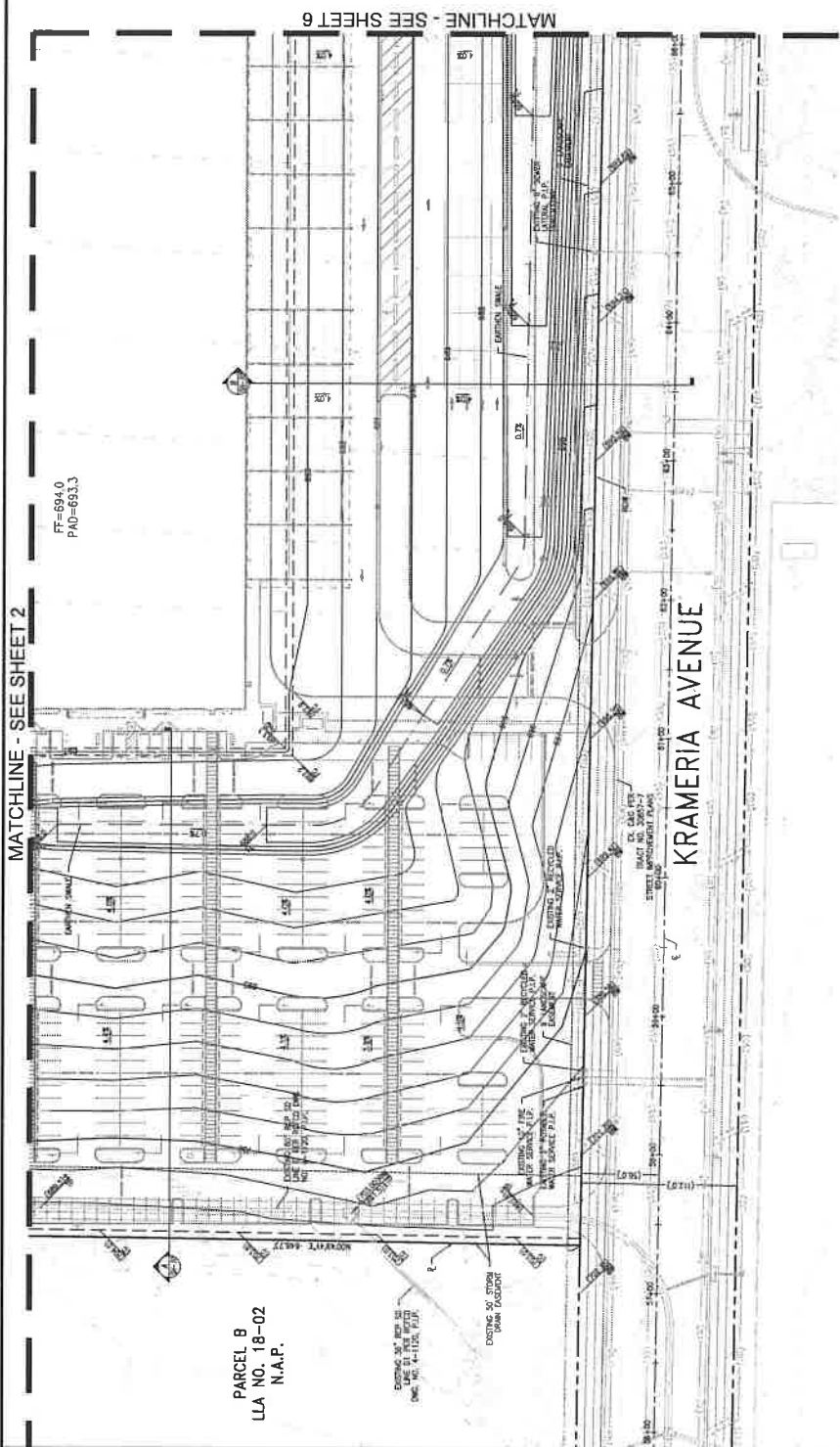






# CONSTRUCTION NOTES:

1. PROTECT IN PLACE EXISTING CURB RISE, CAP TOP OF GATE AND ADJUST TO GRADE.
2. EXTEND EXISTING CURB RISE, TOP OF GATE FOR PLAN. SEE DETAIL SHEET 7.
3. INSTALL 8" DIAMETER GRAVEL 80-100 MM 1/2" COEP.



<b>Underground Service Alert</b> Call TOLL FREE 811 TWO WORKING DAYS BEFORE YOU DIG		<b>REVISIONS</b> NO.   DATE   ENGINEER   REVISIONS	
---	--	---	--

**PROJECT DESCRIPTION**  
 UNDERGROUND UTILITY LOCATIONS  
 143.1847' IN A CONCRETE POST ALONG THE A-15  
 RAMPAGE SOUTH OF VAN BUREN BOULEVARD  
 RIVERSIDE COUNTY SURVEYOR POINT 6 100-03-18  
 DATE: APR 25 2011  
 DRAWN: J. G. GARDNER

**March Joint Powers Authority**  
 APPROVED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 SIGNATURE: \_\_\_\_\_  
 TITLE: \_\_\_\_\_

**PREPARED BY:**  
  
**darc**  
 CIVIL ENGINEERING AND SURVEYING  
 10 S. 10th Street  
 Suite 200  
 Anaheim, CA 92805  
 (714) 455-1100

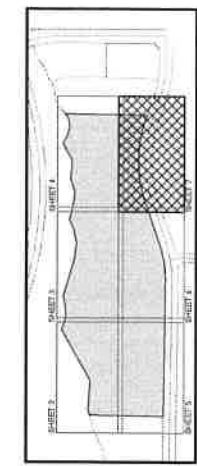
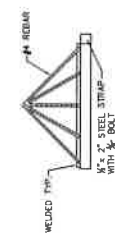
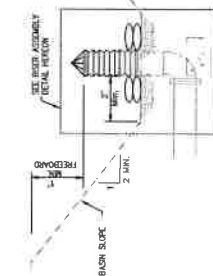
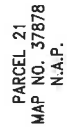
**March Joint Powers Authority**  
**ROUGH GRADING**  
 SHEET NUMBER: 5  
 MAP: W20-000-XXX  
 MAP: 10-XXX



- ① PROTECT IN PLACE EXISTING CMP RISER. CAP TOP OF GRATE AND ADJUST TO GRADE.
- ② EXTEND EXISTING CMP RISER. TOP OF GRATE PER PLAN. SEE DETAIL SHEET 7.
- ③ INSTALL 8" DIAMETER GRAVEL RCP-RAP MIN. 18" DEEP.

[illegible]

- ① PROTECT IN PLACE EXISTING CUP ROSE. CAP TOP OF GRATE AND ADJUST TO GRADE.
- ② EXTEND EXISTING CUP ROSE. TOP OF GRATE PER PLAN. SEE DETAIL SHEET 7
- ③ INSTALL 8" DIAMETER GRAVEL ROP-RAP MIN. 18" DEEP.

[illegible]

# Attachment 3

## Reduced Building Elevations





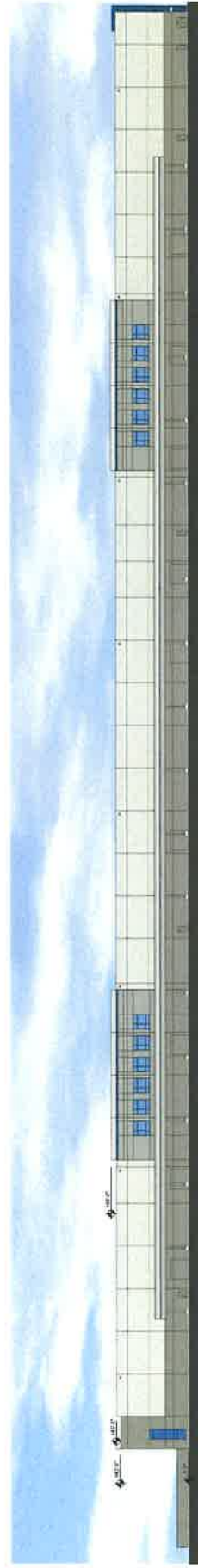
SOUTH ELEVATION



WEST ELEVATION



EAST ELEVATION



NORTH ELEVATION

FINISH LEGEND



P-1: SW 7001 P-2: SW 7016 P-3: SW 7642 P-4: SW 7019 P-5: SW 9178 GL-1

SCALE 1"=20'



MERIDIAN  
MERIDIAN PARK LLC

AMAZON - MERIDIAN PARK  
Riverside, CA

CONCEPTUAL EXTERIOR ELEVATIONS



03

DATE: 04/18/2024  
DRAWN: JMD  
CHECKED: JMD





# Attachment 4

## Reduced Landscape Plan





# TREES



*Koeberlinia paniculata*  
Goldenrain Tree  
Deciduous



*Lagerstroemia indica* 'Natchez'  
Natchez Grape Myrtle  
Deciduous



*Lophosiemon confertus*  
Brisbane Box  
Evergreen



*Pinus eblancii*  
Calabrian Pine  
Evergreen



*Platanus x acerifolia*  
'Bloodgood'  
Bloodgood London Plane  
Deciduous



*Quercus ilex*  
Holly Oak  
Evergreen



*Ulmus parvifolia* 'True Green'  
Chinese Elm  
Semi-Evergreen

## SHRUBS, GRASSES, AND ACCENT PLANTS



*Agave attenuata* 'Ray of Light'  
Foxtail Agave



*Arctostaphylos* 'John Dourley'  
John Dourley Manzanita



*Bouteloua gracilis*  
Blue Grama



*Ceanothus* 'Dark Star'  
Dark Star California Lilac



*Cistus salvifolius*  
Sage-Leaf Rock Rose



*Dietes bicolor*  
Bicolor African Iris



*Kniphofia uvaria*  
Torch Lily



*Lavandula stoechas* 'Otto Quast'  
Spanish Lavender



*Miscanthus sinensis* 'Yakushima'  
Dwarf Maiden Grass



*Muhlenbergia capillaris* 'White Cloud'  
White Cloud Muhly



*Myoporum parvifolium* 'Pink'  
Pink Myoporum



*Pennisetum setaceum* 'Cupreum'  
Red Fountain Grass



*Bougainvillea* 'Rosenka'  
Bougainvillea



*Leucophyllum* 'Green Cloud'  
Green Cloud Texas Ranger



*Rosmarinus prostratus*  
Prostrate Rosemary



*Senecio mandraliscae*  
Blue Chalk Sticks



*Senna (Cassia) artemisioides*  
Feathery Cassia



*Westringia fruticosa* 'Smokey'  
Smokey Coast Rosemary

### CONCEPTUAL PLANT PALETTE

Underground Service Alert

Call TOLL FREE 811

THIRD MONDAY IN MAY BECOME YOUR DIG

811

FOR A STAMPED & C.S. MARKS DOG STAMPS 72

1/4" (1/2") IN A CONCRETE FOOTING AND THE 4.531

PAVEMENT SURF OF ANY OTHER BUILDINGS

PREPARE COUNTY SURVEYOR PRINT RE 105-45-46

DATE: MAY 26, 2011

ILLUSTRATION 152.30'

REVISIONS

DATE

BY

FOR

DATE

BY

FOR

DATE

BY

FOR

March Joint Powers Authority

APPROVED BY:

DATE

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March Joint Powers Authority

APPROVED BY:

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DATE

HIRSH & ASSOCIATES, INC.

LANDSCAPE ARCHITECTURE & PLANNING

1000 N. GATEWAY AVENUE, SUITE 200

PO BOX 714-779-0000 FAX 714-779-0000

www.hirshandassociates.com 650.771.1111

March Joint Powers Authority

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DATE

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March Joint Powers Authority

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# Attachment 5

## Ministerial Review Checklist

# March Business Center Implementation Committee

## Ministerial Review Checklist

### March Business Center Specific Plan (SP-1, A8)

Compliance with March Business Center Specific Plan Development Standards			Complies with Requirement	Requirement Not Applicable	Does Not Comply with Requirement
Section III. F.: Compliance with Land Use Table III-1.			X		
<p><i>Meridian Park LLC. (“Applicant”) is requesting approval for Design Plan 21-02 is the proposed Meridian South Campus DJT6 project, is located on a 44.8-acre industrial zoned site that was previously mass graded along with the overall Meridian Park South Campus project and located north of the intersection of Krameria Avenue and Bundy Avenue. The Project site is adjacent to the Eagle Business Park to the west, vacant developable land to the east, an existing LLMD easement and the Least Bell’s Vireo Conservation Easement to the north, and Krameria Avenue/Amazon Warehouse, vacant developable land, and parkland to the south. The project includes 17% landscaping and includes Meridian Standard walls around the truck trailer court as well as wrought iron and stacked stone pillar fencing around the most of the site. Access to the site is achieved by four proposed driveways from Krameria Avenue, and overall access to the site is achieved from Van Buren Boulevard and Village West Drive. The proposed project is located on Parcels 19 and 20 of Parcel Map 37878 (APN’s 294-100-034, 294-120-047, 294-120-046 and 294-100-034) and is located in the Industrial designation according to the Specific Plan Land Use Table III-1 of the March Business Center Specific Plan (SP-1, A8).</i></p>					
Section III. G. Compliance with Table III-2.			X		
<p><i>The proposed development project complies with the Lot and Yard Dimensions By Land Use Category Table III-2:</i></p>					
<i>Dimensions</i>	<i>Minimum Requirements</i>	<i>Site-Specific Dimensions</i>			
<i>Area</i>	<i>30,000 sq. ft.</i>	<i>1,951,488 sq. ft.</i>			
<i>Street Frontage</i>	<i>100 ft.</i>	<i>1,724 ft.</i>			
<i>Lot Width</i>	<i>100 ft.</i>	<i>1,724 ft.</i>			
<i><u>Minimum Yards</u></i>					
<i>Front Yard Setback</i>	<i>20 ft.</i>	<i>185 ft.</i>			
<i>Interior Side Yard Setback</i>	<i>0 ft.</i>	<i>200 - 900 ft. (+/-)</i>			
<i>(Abutting Residential Zone)</i>	<i>30 ft.</i>	<i>N/A</i>			
<i>Street Side Yard Setback</i>	<i>20 ft.</i>	<i>N/A</i>			
<i>Rear Yard Setback</i>	<i>25 ft.</i>	<i>185 ft. (+/-)</i>			
<i>(Abutting Residential Zone)</i>	<i>50 ft.</i>	<i>N/A</i>			
<i>Building Height</i>	<i>50 feet 2 stories*</i>	<i>48 ft 1.5 stories</i>			
<i>Floor Area Ratio</i>	<i>0.5 or 975,744 sf</i>	<i>0.11 (219,000 sf)</i>			

(maximum)					
Site Landscaping	10% (195,148 sf)	17% (358,138 sf)			
<i>* Increased height up to 80 ft. is permitted where all building setbacks meet or exceed the proposed building height.</i>					
Section III. G. (a) (1): Zero side yard is permitted when the opposite side yard is not less than 30 feet.  <i>The project is proposing a 60 ft. side yard to the north of the facility and a 5 ft. side yard to the south of the facility. Auto access is proposed at one (1) shared driveway, with truck trailers limited to two (2) per building located within the year setback area.</i>			X		
Section III. G. (a) (2): FAA form 7460-1 shall be prepared (FAR §77.13.2.i).  <i>On April 19, 2021, the Federal Aviation Administration issued a "Determination Of No Hazard to Air Navigation" for the proposed project after conducting an aeronautical study in response to the applicant filing a FAA form 7460-1.</i>			X		
Section III. G. (a) (2): Objects taller than 50 feet in the Height Caution Zone require review by the Airport Land Use Commission.  <i>The proposed development is located within the Height Caution Zone, however the buildings do not exceed 50' in height. On June 18, 2021, ALUC Staff informed JPA Staff because the building does not exceed the 50' height threshold, and because of previous covenant on the property, no additional ALUC review is necessary.</i>			X		
Section III. G. (b): A 15 foot landscaped setback is required for all front and side yards adjacent to public streets.  <i>The proposed development project complies with this requirement through the provision of a 15' on-site landscaped setback from Krameria Avenue. In addition to the existing 17-foot LLMD landscape setback along Krameria Avenue.</i>			X		
Section III. G. (c): Driveways width and spacing shall comply with Riverside County requirements as approved by the March JPA Civil Engineer.  <i>The project provides two 50-wide driveways at each end of the parcel. The first driveway is located at the controlled intersection of Bundy Avenue and Krameria. The second driveway is on the western end of the project, and the driveways are separated by over 1,000 feet.</i>			X		
Section III. G. (d): Off-street loading shall be sized and located so that trucks are not located within the required front or street side yard.  <i>The proposed development project is designed so that all loading and unloading functions are performed within the site interior in a manner that does not impact site access/driveways.</i>			X		
Section III. G. (e): All uses shall occur entirely within a building, except for storage, loading and outdoor work. Outdoor work; storage of merchandise,			X		



<p>material and equipment is permitted in interior side yards or rear yards, provided the area is enclosed by obscuring walls, fences or a combination thereof.</p> <p><b><i>Views of the truck courts are screened by the proposed 12-foot wall standard Meridian Park decorative concrete and rock pilaster walls.</i></b></p>			
<p>Section III. G. (e) (1): Chain link fencing will not be used within 100' of a public right-of-way. Where used, it will be vinyl coated.</p> <p><b><i>The proposed development project does not incorporate fencing. The front yard setback and street side setback are screened by an existing LLMD landscape buffer. North and south truck courts are walled off by the 12-foot wall standard March Business Park (MBP) decorative concrete and rock pilaster walls. Any future fencing will need to match MBP standards (tube Steele).</i></b></p>	X		
<p>Section III. G. (e) (2): Coiled, spiraled, or rolled fencing such as razor wire or concertina wire are not be permitted.</p> <p><b><i>The proposed development project does not incorporate coiled, spiral, or rolled fencing such as razor wire or concertina wire.</i></b></p>	X		
<p>Section III. G. (f): Table III-3 summarizes the parking ratios. It is acknowledged that certain land uses will have unique parking characteristics, based on building utilization, workforce composition, and other considerations. In these cases, the March Joint Powers Commission may review a use permit application to reduce required parking through a detailed parking analysis. All uses shall provide a minimum of three bicycle parking spaces.</p> <p><b><i>The proposed development project is consistent with the Parking Ratios identified in Land Use Table III-3 for Industrial Warehouse uses. The site parking requirement is 244 total parking stalls and 246 are provided. The proposed development project does not incorporate unique parking characteristics. The conceptual site plan includes standard parking, handicap stalls, carpool stalls, and bike parking facilities. The project meets and or exceeds the minimum number of bicycle parking spaces required.</i></b></p>	X		
<p>Section III. G. (g): Development shall comply with the March Business Center Settlement Agreement.</p> <p><b><i>The proposed development project complies with the March Business Center Settlement Agreement in that there are mitigation measures assigned to the developments for reducing semi-truck emissions and to facilitate the use of bio-diesel and alternative fuels. The March Business Center Design Guidelines are in place to make certain that all development projects in the South Campus have architectural appeal and are attractive developments when visible from the Orangecrest residential neighborhood. This proposed development project also incorporates a truck route sign at the egress location. The truck route sign enforce the approved truck route and prevent trucks from traveling westbound on Van Buren Blvd.</i></b></p>	X		
<p>Section III. H. 1: Riparian Special Treatment areas will be protected.</p>	X		

<i>The project is not located next to or near a riparian treatment area or conservation area.</i>				
Section III. H. 2: Development of Lots Adjacent to the Stephen Kangaroo Rat Habitat.			X	
<i>The Project is not located in proximity to the Stephens Kangaroo habitat area.</i>				
<b>March Business Center Final Focused Environmental Impact Report, February, 2003</b>				
Compliance with the Certified FEIR Mitigation Measures (SCH# 2002071089)		Complies with Mitigation Measure	Mitigation Measure Not Applicable	Does not Comply with Mitigation Measure
A-1	<p>Development within the Clear Zone and Accident Potential Zones I and II will abide by building standards and codes including height restrictions, restrictions on use, setbacks, population densities, and insulation and materials, as outlined in the approved 2005 Air Installation Compatible Use Zone (AICUZ).</p> <p><i>The project is located within Zone C2 (High Terrain Zone) of March Air Reserve Base ALUCP. The project is also located within the March JPA: March Business Center/Meridian exception area. According to the MJPA documentation, this particular site is located in area which has a current or extended development agreement..</i></p>	X		
A-2	<p>As established in the Specific Plan, the project will comply with the policies and requirements of the Riverside County Airport Land Use Plan. Development plans will be submitted to the FAA for review in accordance with FAR 77.13.2.i. Additional ALUC review will be required for objects taller than 50 feet in the Height Caution Zone shown on Figure IV.A-4 [in the Focused EIR]. Other land use controls (relating to safety (both in the air and on the ground) and noise) have been developed in consultation with the ALUC and have been incorporated into the Specific Plan.</p> <p><i>The Project does not exceed 50' in height. No additional ALUC review was required by ALUC Staff.</i></p>	X		
A-3	<p>In accordance with the requirements of the Specific Plan, a School Buffer overlay district will be established. This district will extend 0.25 miles from the boundary of the Arnold Heights Elementary School and Tomas Rivera Elementary School. The overlay district will provide landscaping around the boundaries of the school for screening; will prohibit certain uses with the potential noise impacts and/or hazardous materials handling/generation; and will provide for enhanced review of other development proposals to limit the potential for adverse impacts on the school.</p> <p><i>This project is not located within 0.25 miles of any school site, and thus, this provision does not apply.</i></p>		X	

A-4	<p>Project detention basins shall have the following features to limit bird activity:</p> <ol style="list-style-type: none"> <li>1. The Basin shall drain within a six-hour period to reduce the potential for plant growth.</li> <li>2. Regular maintenance activities shall include the removal of vegetation</li> <li>3. Detention Basins shall be monitored regularly to determine if they attract waterfowl or other birds.</li> <li>4. A plan to discourage bird activity shall be implemented if the basins are found to be an attraction to birds.</li> </ol> <p><i>As identified in the Preliminary storm drain study, The site is proposed to be developed into one industrial building with loading docks and associated parking areas. The southwestern 14.0 acres of the project site (Area A) will drain to underground detention system 'A'. Detention system 'A' will consist of a series of parallel 60" solid wall storm drain pipe that will flow to a modular wetland system located at the southeast corner of the project site. There will be 5 rows of 520 linear feet of 60" diameter pipe in detention system 'A' for a total volume provided of 51,050 cf. The northeastern 10.0 acres of the project site (Area B) will drain to underground detention system 'B'. Detention system 'B' will consist of a series of parallel 60" solid wall storm drain pipes that will flow to a modular wetland system located at the northeast corner of the project site. There will be 4 rows of 550 linear feet of 60" diameter pipe in detention system 'B' for a total volume provided of 43,197 cf. Due to earthwork constraints limiting the D-Load over the existing 60" Riverside County Flood Control RCP storm drain line on the east of the site, the 1.1 acres on the northwest of the site (Area C) is unable to include a detention system due to it's low elevation compared to the rest of the site. As such, Area B calculations were increased to include 11.1 acres compared to the previously mentioned 10.0 acres. This design would slightly oversize Detention System B to make up for the lack of a detention system for Area C. Both designs for A &amp; B will feature weirs directly after the detention systems that will allow water to be held within the detention system, and limit flows to existing for each watershed. An outlet to the Modular Wetland System will be located at the bottom of the weir to slowly drain down the treatment volume over approximately 60-70 hours. An additional orifice will be located towards the middle of the weir to allow volumes larger than that of the treatment volume to bypass the modular wetlands unit and continue to the connections to the public storm drain. For this specific study, one orifice was used in the design to represent a combination of both the outflow of the Modular Wetlands Unit as well as the orifice mid-height for volumes larger than that of the treatment volume. Area C will feature a smaller treatment flow based MWS Unit sized to treat the calculated treatment flow. The outlet will be connected to the storm drain serving Area B and connect to the existing manhole of the public storm drain system.</i></p>	X		
B-1	<p>The Project shall contribute on a fair share basis toward the improvements identified in the Cumulative Impacts section.</p> <p><i>Fair Share proportional Fees are collected in accordance with</i></p>	X		

	<i>the MBC Standard Conditions of Approval. These improvements are required prior to the recordation of the first unit of Phase III (South Campus) and the fee is paid by the master developer (Meridian Park, LLC).</i>			
B-2	<p>The Project shall construct the transportation improvements identified in previously referenced Figures IV.B-5 through IV.B-7 (project phasing). To the extent that such improvements provide capacity benefits for local or regional (i.e., non-project) demand, the project is eligible for credits toward its contribution toward local and/or regional transportation impact fees, if any.</p> <p><i>All Phase I and Phase II improvements identified in the March Business Center Specific Plan Figure V-1 and V-2 are complete. Phase III improvements are complete. The status of all street infrastructure is consistent with the project phasing schedule.</i></p>	X		
B-3	<p>March Business Center traffic volumes shall be monitored periodically to assure that the transportation infrastructure provides sufficient capacity to serve Project volumes. Traffic monitoring shall occur at a minimum of five-year intervals.</p> <p><i>Two five-year March Business Center Traffic Study Updates have been provided to MJPAA staff by the Master Developer. The Traffic Study Update indicates that current trip generations are consistent with and less than what was analyzed in the March Business Center EIR for projected traffic. This information was reported to the March Joint Powers Authority at their Commission hearing in May, 2020. No action by the Applicant is required under this measure.</i></p>	X		
B-4	<p>The Project shall provide a site that can accommodate the future construction of a multi-modal transportation center by RCTC in the North Campus, north of Cactus Avenue and south of Alessandro Boulevard. CEQA analysis of the environmental impacts of the transportation center will be required once a proposal is brought forward by RCTC.</p> <p><i>This measure has been completed. The March Field/Moreno Valley Metrolink Station was approved on Lot 4 of the Meridian Business Park, north of Cactus Avenue, south of Alessandro Boulevard and west of the I-215 freeway. Metrolink trains and RTA busses now operate at the station. This mitigation measure is complete.</i></p>	X		
B-5	<p>The March Business Center shall require implementation of parking ratios that limit the need for on-street parking. These ratios are identified in the Specific Plan.</p> <p><i>The development requires 244 total parking spaces pursuant to the Off-street Parking Regulations within the Specific Plan (Section G(f); Table III-3; p. III-17) and 113 parking spaces are provided. All streets are striped for bike lanes and parking within the bike lanes is prohibited. Thus, no on-street parking will occur within the development.</i></p>	X		

B-6	<p>The Project shall provide for bicycle facilities to accommodate non-motorized circulation on the site and connectivity to routes in the Cities of Riverside and Moreno Valley.</p> <p><b><i>The project has been designed to accommodate the necessary bicycle facilities to accommodate non-motorized circulation.</i></b></p>	X		
B-7	<p>March Business Center shall provide truck routes on internal roadways to limit impacts of trucks on adjacent residential communities.</p> <p><b><i>As conditioned, a truck route sign is required at the driveway.</i></b></p>	X		
B-8	<p>The Project shall construct internal roadways in accordance with the <i>County Road Improvement Standards and Specifications</i> with additional landscaping as identified in the Riverside County Integrated Project (RCIP).</p> <p><b><i>All internal roads have been constructed to Riverside County standards in conformance with the approved phasing schedule.</i></b></p>	X		
B-9	<p>The March JPA shall collaborate with adjacent jurisdictions and agencies to facilitate improvements addressing the existing deficiency at the I-215/Van Buren Boulevard interchange.</p> <p><b><i>This measure has been completed. The I-215/ Van Buren Boulevard interchange was reconstructed and is in full operation.</i></b></p>	X		
B-10	<p>The March JPA shall implement Transportation Demand Management (TDM) strategies to shift trips outside the standard commuting hours and/or to non-“drive alone” modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel. Section V of the Specific Plan outlines TDM requirements.</p> <p><b><i>This mitigation measure is ongoing. The future tenants will comply by filing a TDM plan prior to occupancy. Refer to Site Plan Sheet 1.1 of the development plans Conditions of Approval Condition #30. This mitigation will be performed by the future tenant which is not presently known.</i></b></p>	X		
B-11	<p>The March JPA shall cooperate with the Riverside Transportation Agency (RTA) for the provision of bus service within the Specific Plan Area.</p> <p><b><i>Upon the recent completion of the Moreno Valley/March Field Metrolink station, RTA commenced operation of bus service on Route 26 along Meridian Parkway within the Specific Plan area.</i></b></p>	X		
B-12	<p>Signage shall be provided at the Van Buren Boulevard intersections with Coyote Bush Road and Orange Terrace to discourage truck traffic on residential streets in the Orangecrest Development. Furthermore, the March JPA, as a responsible party, shall encourage the City of Riverside and Riverside County to review and consider appropriate legislation to eliminate or curtail truck traffic, exempting local deliveries, on Alessandro Boulevard and Van Buren Boulevard west of the March Business Center Development.</p>	X		

	<i>This project is located on Krameria Avenue, a Truck Route Sign will be posted on both driveways. Additional steps have been taken to assure that traffic follows the required Truck Route ordinance, including payment by the Master Developer for increased patrol and enforcement of the rules.</i>			
C-1	<p>Preferential parking spaces shall be offered to car pools and van pools.</p> <p><i>As conditioned, the Project will provide preferential parking spaces for carpools/vanpools based on the total parking count for the project.</i></p>	X		
C-2	<p>Employers with 250 employees or more shall implement a compressed workweek schedule when feasible.</p> <p><i>This is a Standard Condition of Approval for this project.</i></p>		X	
C-3	<p>Employers with 250 employees or more shall develop a trip reduction plan to increase vehicle occupancy.</p> <p><i>This is a Standard Condition of Approval for this project.</i></p>		X	
C-4	<p>Employers shall provide on-site child care facilities when feasible.</p> <p><i>The feasibility of on-site child care facilities will be reviewed during the tenant improvement process and through the review of the required Traffic Demand Management plan, when a tenant is identified. This mitigation measure, though applicable, is implemented prior to occupancy of the building, through the review of the Traffic Demand Management plan.</i></p>	X		
C-5	<p>Design elements shall be designed to reduce vehicle queuing when entering and exiting parking structures.</p> <p><i>This measure is not applicable as the Development Project will not incorporate a parking structure. No parking structures have been constructed within the March Business Center development. Appropriate queuing would be required, as determined through a traffic study, when reviewing a parking structure application prior to plot plan approval.</i></p>		X	
C-6	<p>Projects shall provide for video conferencing facilities to the extent possible.</p> <p><i>The feasibility of a video conferencing facility will be reviewed during the tenant improvement process and through the review of the required Traffic Demand Management plan, when a tenant is identified. This mitigation measure, though applicable, is implemented prior to occupancy of the building, through the review of the Traffic Demand Management plan.</i></p>	X		
C-7	<p>Businesses shall minimize the use of fleet vehicles during smog alerts, and encourage the use of alternative fuel vehicles.</p> <p><i>Because the Project does not include a known tenant at this time, compliance with this requirement will be verified prior to</i></p>	X		

	<i>occupancy of the building, through the review of the Traffic Demand Management plan.</i>			
C-8	Buildings shall be designed to reduce energy usage by utilizing solar or low emissions water heaters, double paned glass windows, using light colored roofing materials, using skylights in warehouses, orienting buildings north to the extent practical, and increasing wall and attic installation above Title 24 requirements.  <i>These requirements are implemented at the time of construction plan review as identified in Standard Condition of Approval 11, Page 2 and assured during construction plan review.</i>	X		
C-9	CEQA Review of stationary source emissions other than natural gas and electricity shall be done on all projects with the possibility of emitting air pollutants. In addition, all projects involving stationary source emissions shall obtain permits to construct and operate from the SCAQMD.  <i>There are no stationary emissions planned in conjunction with this development. If, at a future date, equipment is proposed that would have stationary emissions, such as an emergency generator, conformance with this requirement will be assured through implementation of Standard Condition of Approval #29.</i>	X		
C-10	Trucks hauling dirt, sand, gravel or soil are to be covered or should maintain at least two feet of freeboard in accordance with Section 23114 of the California Vehicle Code.  <i>The grading contractor is required to verify compliance with this measure in a signed letter prior to a grading plan approval in compliance with Standard Condition of Approval #72.</i>	X		
C-11	Construction access roads to the main roads should be paved to avoid dirt being carried on to the roadway.  <i>The grading contractor is required to verify compliance with this measure in a signed letter prior to a grading plan approval in compliance with Standard Conditions of Approval #72.</i>	X		
C-12	A construction relations officer should be appointed to act as a community liaison to oversee on-site construction activity and all emissions and congestion related matters.  <i>The grading contractor is required to verify compliance with this measure in a signed letter prior to a grading plan approval, in compliance with Standard Condition of Approval #72 that addresses all construction mitigation measures.</i>	X		
C-13	Restrict idling emission from trucks by using auxiliary power units and electrification at the industrial warehouse facilities.  <i>This measure is implemented by Standard Conditions #63 requiring the installation of electrical conduit at dock doors for cold storage facilities, as well as a phase in schedule to assure that cold storage trailer fleets are 100% electrified within 10 years of issuance of the building certificate of occupancy.</i>	X		



C-14	<p>Landscape with appropriate drought-tolerant species to reduce water consumption.</p> <p><i>This requirement is implemented through compliance with March JPA's Water Efficient Landscape Ordinance (Ordinance #JPA 16-03).</i></p>	X		
D-1	<p>Prior to construction activity, the applicant shall coordinate with USFWS to assure that the requirements and stipulations of the 1999 Biological Opinion and the Biological Opinion Clarification Letter (September 6, 2002) are met. The 1999 Biological Opinion and the 1999 Biological Opinion Clarification letter are included in Appendices A and B of the Biological Resources Review found in Appendix F of this document. Mitigation for potential impacts to federal or state listed species shall be as per the 1999 Biological Opinion and the 1999 Biological Opinion Clarification Letter issued by USFWS. This mitigation shall include the replacement 35.2 acres of impacted occupied Stephens' kangaroo rat (SKR) habitat at a 1:1 ratio. As of September 2002, the March JPA is responsible for 14.2 acres of mitigation at a 1:1 ratio, as 21 acres of USFWS approved occupied habitat have previously been acquired by the March JPA and serve as mitigation for 21 acres of SKR occupied habitat. Other required mitigation (78.4 acres discussed in the 1999 BO Clarification letter) will be at a fee of \$500 per acre.</p> <p><i>The mitigation credits for SKR were purchased on January 24, 2005 from the County of Riverside and November 20, 2006 from the Southwestern Riverside County Multi-Species Reserve. This mitigation measure is complete. In accordance with the CMMP for impacts on Biological resources associated with the Specific Plan, 185 acres of Least Bell's Vireo habitat was placed within a perpetual conservation easement.</i></p>	X		
D-2	<p>Per the 1999 BO, avoid 13 acres of USFWS designated least Bell's vireo riparian habitat north and south of Van Buren Boulevard by utilizing 100-foot buffer zones in these areas.</p> <p><i>The site is not directly adjacent to the LVV riparian habitat area, there is an existing 100-foot wide LLMD area that separates the two properties.</i></p>		X	
D-3	<p>If construction activity is planned during nesting/breeding season, a qualified environmental scientist shall conduct a field review of the affected areas prior to vegetation clearing activity to access the areas for occupancy by least Bell's Vireo.</p> <p><i>This is a Standard Condition of Approval for this project.</i></p>		X	
D-4	<p>Prior to construction activity, the applicant shall coordinate with the L.A. District Corps office to assure conformance with the requirements of Section 404 of the Clean Water Act.</p> <p><i>This measure has been completed by the Master Developer. A USACE individual 404 permit was executed on September 8, 2010 for impacts on 2.36 acres located more than 700 feet from the Project site. All grading is complete in association with the project</i></p>	X		

	<b>404 permit.</b>			
<b>D-5</b>	<p>Prior to construction activity, the applicant shall coordinate with the Santa Ana Water Quality Board (Region 8) to assure conformance with the requirements of Section 404/401 of the Clean Water Act and the State of California Porter Cologne Water Quality Control Act.</p> <p><i>This measure has been completed by the Master Developer. Water quality certification was issued March 21, 2003 for the master development. Although there are Waters of the US near proximity to the Project, the site is separated by an LLMD area.</i></p>	<b>X</b>		
<b>D-6</b>	<p>Prior to activity within waters of the U.S., the applicant shall coordinate with the California Department of Fish and Game (Eastern Sierra and Inland Desert Region 6) relative to conformance to the Lake and Streambed Alteration permit requirements.</p> <p><i>This measure has been completed by the Master Developer. The Streambed Alteration Agreement (6-2002-291) was approved on March 3, 2003 and amended and signed on June 2, 2010.</i></p>	<b>X</b>		

<b>E-1</b>	<p>No project facilities located within one-quarter mile of the existing school shall store, handle or use toxic or highly toxic gases as defined in the most currently adopted County fire code at quantities that exceed exempt amount as defined in the most currently adopted fire code.</p> <p><i>This provision does not apply to the development as the nearest school site is located more than one-mile away from the development site.</i></p>		<b>X</b>	
<b>E-2</b>	<p>Facilities that store, handle or use regulated substances as defined in the California Health and Safety Code 25532 (g) in excess of threshold quantities shall prepare risk management plans (RMP) for determination of risks to the community. If in the event the RMP shows that the facility stores, handles or use regulated substances in excess of the thresholds described above, the activity will be prohibited.</p> <p><i>The applicant is anticipating the future tenants to operate as an Industrial warehouse use. Once tenant improvement plans are submitted, the MJP A will evaluate whether the tenant and its specific use will require a RMP. Additionally, the Applicant is acknowledging compliance with this measure within the project description.</i></p>	<b>X</b>		
<b>F-1</b>	<p>Detention basins and improvements to the storm drain system shall be constructed to reduce peak flows to less than those associated with the existing conditions in accordance with the approved Drainage Plan.</p> <p><i>As identified in the Preliminary Drainage Study, an underground infiltration trench unit for pre-treatment of storm water. As a result, storm water flows will not exceed existing conditions. Additionally, this mitigation measure has been placed on the preliminary grading plans for compliance.</i></p>	<b>X</b>		
<b>F-2</b>	The storm drain system shall include sediment basins near inlets to the	<b>X</b>		

	<p>system to intercept sediment in accessible areas where maintenance is practical.</p> <p><i>As identified in the Preliminary Drainage Study, an underground infiltration trench unit for pre-treatment of storm water. As a result, storm water flows will not exceed existing conditions. Additionally, this mitigation measure has been placed on the preliminary grading plans for compliance.</i></p>			
<b>F-3</b>	<p>Activities requiring authorization under an NPDES permit shall not be conducted prior to authorization by the Santa Ana Regional Water Quality Control Board. Best management practices identified in the Storm Water Prevention Plan shall be implemented.</p> <p><i>The development will require an NPDES Storm Water Pollution Prevention Plan. In accordance with Condition of Approval #55, the developer is required to receive approval of a SWPPP prior to grading permit issuance by March JPA.</i></p>	X		
<b>H-1</b>	<p>The following conditions shall be incorporated into the proposed Specific Plan and shall be required of future development:</p> <p>Provide for the extension of utility infrastructure to serve the development, including over-sizing facilities for future needs.</p> <p><i>All Infrastructure has been designed and is being installed consistent with Specific Plan SP-1, A8 This is a Standard Condition of Approval for this project Figure V-1, V-2 (Transportation Infrastructure), Figure V-6 (Bike and Truck Routes), Figure VI-1 (Sewer), Figure VI-2 (Water) and Figure VI-3 (Reclaimed Water).</i></p>	X		
<b>H-2</b>	<p>Construct the storm drain and flood control facilities, in accordance with the approved March Business Center Drainage Plan and Plan for March JPA Planning Area.</p>			
<b>H-3</b>	<p><i>All drainage Infrastructure has been designed and is being installed consistent with Specific Plan SP-1, A8 Figure VI-4 (Storm Drainage).</i></p> <p>All storm drain and flood control facilities shall be approved and operational prior to the issuance of certificates of occupancy for the associated development.</p> <p><i>All drainage facilities for the site development will be installed consistent with the Engineering Conditions of Approval and shall be assured prior to the issuance of the project Certificate of Occupancy.</i></p>	X		
<b>H-4</b>	<p>The Project sponsor shall incorporate the following measures to help reduce the project's potential solid waste impacts and to help in the County's effort to comply with State law in diverting solid waste from landfill disposal:</p> <ul style="list-style-type: none"> <li>Green waste generated by the project should be kept separate from other waste types in order that it can be recycled through the practice of</li> </ul>			

	<p>grass recycling (where lawn clippings from a mulching type mower are left on the lawn) or onsite composting or directed to local wood grinding and/or composting operations.</p> <ul style="list-style-type: none"> <li>· The use of mulch and/or compost in the development and maintenance of landscape areas is recommended.</li> <li>· Construction and demolition waste should be reduced and/or diverted from landfill disposal by the use of onsite grinders or by directing the materials to recycling facilities.</li> </ul> <p><b><i>The requirements for separation of green waste and use of mulch and/or compost is implemented through Standard Operating Condition #5. As identified in the applicant's narrative, construction/demolition waste will be diverted from landfills through recycling of materials.</i></b></p>	X		
H-5	<p>The proposed Project shall comply with the State Model Ordinance, implemented in 9/1/94 in accordance with AB 1327, Chapter 18, California Solid Waste Reuse and Recycling Access Act of 1991, which requires that all commercial, industrial, and multifamily residential projects provide adequate area(s) for the collections and loading of recyclable materials. Prior to building permit issuance, the applicant shall submit a Recyclables Collection and Loading Area plot plan to the March JPA for review and approval.</p> <p><b><i>Incorporation of a recyclables collection and loading area is identified on the Site Plan which identifies the location of a recycling collection facility at the trash enclosures.</i></b></p>	X		
H-6	<p>As Phase I develops, actual wastewater flows will be monitored relative to the capacity of Pump Station 3586 so that future improvement will be in place before the capacity of the pump station is reduced.</p> <p><b><i>Pump Station 3586, a remnant of the former military development was removed and replaced by an updated sewer lift facility on lot A of Final Map 30857-Unit 2. This mitigation measure is complete.</i></b></p>	X		
H-7	<p>The proposed non-potable water system will meet "Purple" pipe standards for reclaimed water systems.</p> <p><b><i>The Specific Plan incorporates a sophisticated reclaimed "purple pipe" water system identified in Figure VI-3 of the March Business Center Specific Plan. The reclaimed water infrastructure has been installed for Phase I and II of the Specific Plan. "Purple" pipe reclaimed landscape irrigation system is provided on site.</i></b></p>	X		
H-8	<p>A Fire Flow of 5,000 gallons per minute shall be used for the water distribution network.</p> <p><b><i>The water system identified on Figure VI-2 of the Focused EIR meets the requirement of 5,000 GPM.</i></b></p>	X		
I-1	<p>All projects are required to comply with the Specific Plan, Design Guidelines, landscape concept plan and Development Code, which will ensure the following:</p>			

	<ul style="list-style-type: none"> <li>Conflicts and incompatibilities between land uses will not occur through the use of landscaped setbacks, buffers, site design, site orientation, architectural features, walls or fences, density/intensity reductions, reduced hours of operation for commercial and industrial uses, shielding of lighting, and the like.</li> <li>The enhancement and preservation of natural and man-made features, such as major roadways, rail lines, drainage courses, utility corridors, groups of rock outcroppings, and tree rows to create boundaries, entryways, and separate entities for distinct geographic portions of the Specific Plan.</li> <li>Preservation of Van Buren Boulevard and Alessandro Boulevard scenic corridors and enhancement of the gateway treatment at the Riverside National Cemetery.</li> </ul> <p><i>The property is currently screened by the existing LLMD landscape buffer between Krameria Avenue and property line. Screening of the Truck Courts from the north and south is achieved by a proposed 12-foot tall decorative wall. Lighting will comply with JPA requirements, not to exceed a height of 25', fully shielded, maximum 750 watt LPS or HPS, and lighting not to exceed .5 candle/foot at the project property line. LED lighting is also permitted at or within 100 Kelvin of 2700 Kelvin shall be permitted.</i></p>	X		
J-1	<p>Short-term All proposed projects within the School Buffer Overlay District will be required to conduct an acoustical analysis. If the acoustical analysis indicates noise levels from a proposed use will be in excess of thresholds defined in this section, then the March JPA will prohibit the use within the school overlay district.</p> <p><i>There are no schools located within a mile of this site.</i></p>		X	
J-2	<p>Short-term All construction equipment used for construction activities shall be fitted with exhaust muffling and noise control filler devices to reduce noise impacts.</p> <p><i>The grading contractor is required to verify compliance with this measure in a signed letter prior to a grading plan approval in compliance with Standard Condition of Approval #72.</i></p>	X		
J-3	<p>Information and location of noise sensitive receptors shall be reviewed and updated by March JPA staff to ensure that all sensitive receptors that may be affected by the long-term implementation of the proposed Specific Plan are identified. These sensitive receptors shall include the existing school.</p> <p><i>The March Business Center Focused EIR did not identify significant noise impacts to proximate sensitive receptors. Nearby sensitive receptors were inventoried through March JPA Resolution #JPA 08-21. Proximate Sensitive Receptors to the site include Tomas Rivera Elementary, Amelia Earhart Middle School, Orangecrest neighborhood, Mission Ranch neighborhood and Westmont Village residential community. These sensitive receptors</i></p>	X		

	<i>are more than one-mile away from the Project location. The closest residence from the Project site is located in the Orangecrest neighborhood which is separated from the site by Van Buren Blvd, the LBV CE area, an LLMD area, and a proposed 12' tall decorative wall.</i>			
J-4	Building setbacks and methods of sound attenuation shall be considered and used where appropriate with specific development proposals in the planning area to limit stationary and vehicular long-term noise impacts upon sensitive noise receptors.  <i>The applicant does not anticipate outdoor storage, however if needed, the tenant would need approval of a Conditional Use Permit by the Joint Powers Commission.</i>	X		
J-5	Buildings located within the 65 dBA noise contour will include appropriate sound attenuation devices within its construction.  <i>The Project site is not located within the 65 dB noise contour for March ARB. This mitigation measure is not applicable.</i>		X	
J-6	Industrial and noise sensitive receptors (residential, schools, churches, hospitals, libraries, and senior housing) will be separated sufficiently to reduce the noise impact to sensitive receptors to an insignificant level.  <i>N/A - The site is not located near any existing schools or residential uses.</i>		X	
J-7	Separate residential uses and truck routes so that noise impacts will be contained without unnecessarily lengthening truck trips.  <i>The site is located within two miles of I-215 intersection at Van Buren Boulevard and I-215 Intersections. Van Buren Blvd eastbound is a designed truck route.</i>	X		
K-1	All grading should be performed in accordance with the grading guidelines outlined in the March JPA Development Code.  <i>All Development Code Provisions in the March JPA Development Code, Chapter 9 will be implemented during project grading.</i>	X		
K-2	All future grading and construction of the project site shall comply with the geotechnical recommendations contained in the Preliminary Geotechnical Investigation: March Business Park Phases 1-3 prepared by Inland Foundation Engineering, Inc. dated July 10, 2002. This report contains specific recommendations for mitigating geotechnical conditions related to soils earthwork, slope stability, and ground and surface waters. All recommendations contained in the report shall be incorporated into all final engineering and grading plans.  <i>All grading is required to comply with the approved geotechnical investigation as provided by law. This will be fully implemented at the time of site grading.</i>	X		
K-3	All future development shall use proper erosion control measures during and following construction.	X		



	<i>The development is subject to a Storm Water Pollution Prevention Plan (SWPPP) to control short term construction water quality impacts and a Water Quality Management Plan (WQMP) to address long term operating conditions. With compliance with the requirements for implementing the SWPPP and WQMP, proper erosion control shall be assured.</i>			
K-4	Revegetate graded area within native plants compatible to the area to prevent erosion.  <i>The project will go directly into development in a single phase. No portions of the site will remain undeveloped through a phased development concept. This provision is not applicable.</i>		X	
K-5	All future development within the project site shall adhere to the Uniform Building Code and State building requirements in effect at the time specific development is proposed.  <i>Compliance with this measure will be verified during construction plan review and prior to building permit issuance.</i>	X		
L-1	If archaeological or paleontological resources are encountered at the time of grading or project construction, all project work in the area of the resource shall cease until the area has been surveyed by a qualified archaeologist or paleontologist in conformance with the Cultural Resource Management  <i>The applicant has noted on both the site plan and grading plans and is thus required to be implemented, if resources are found, at the time of grading.</i>	X		
P-1	The March JPA will contract with the Riverside County Sheriff's Department to provide additional police service to the Specific Plan area.  <i>The March JPA continues to contract with Riverside County Sheriff's Department for patrol services. The March JPA is responsible for this measure. Additionally, the Master Developer has agreed to pay for additional traffic enforcement on Van Buren Blvd as part of the last Amendment to the Specific Plan.</i>	X		
P-2	The developer shall dedicate land within the proposed Project for a future fire station. The March JPA will develop a financing plan to fund the station.  <i>Lot U6-1A has been dedicated for the Fire Station site and is located approximately 1.5 miles to the north of the project stie. At present, all lots pay the Fire Impacts Development Impact Fee at building permit for the future development of the Fire Station. Additionally, all developments pay the structural Fire Tax which is a property tax that funds Fire Department operational and maintenance costs.</i>	X		
P-3	Development within the elementary school buffer zone will abide by land use compatibility conditions as set forth in the March Business Center Specific Plan.  <i>As shown in Specific Plan Figure III-1, the School Overlay Zone</i>		X	

	<i>for Tomas Rivera Elementary, the nearest school site, is not located in proximity to the site, and is approximately one-mile to the west of this site. Thus, this provision does not apply.</i>			
<b>Compliance with Approved March Business Center Design Guidelines, adopted November 19, 2003.</b>				
Compliance with Design Guidelines		Complies With Objective	Guideline Not Applicable	Does Not Comply With Objective
<b>3.0</b>	<b><u>General Design Criteria</u></b>			
<b>3.1</b>	For Commercial Retail sites only: Quality to be consistent with Mission Grove Shopping Center and Kohl's Shopping Center.  <i>N/A, the project is an Industrial Warehouse.</i>		<b>X</b>	
<b>4.0</b>	<b><u>Site Planning</u></b>			
Site Planning Objective: Clean, pleasant and contemporary. Compatible with adjacent sites.				
<b>4.2</b>	<b>Minimum Landscape Setbacks at Buildings/R.O.W.s</b> <ul style="list-style-type: none"> <li>10' Minimum landscaping between parking and building when adjacent to a street)</li> <li>15' landscape between public ROW &amp; parking</li> <li>No parking within landscape easements</li> </ul> <i>The landscape plan illustrates that there will be a 20-foot landscape area located between the building and the public street. Additionally, there is no proposed parking to be located in the Lighting, Landscape and Maintenance District easement area.</i>	<b>X</b> <b>X</b>	<b>X</b>	
<b>4.3</b>	<b>Building Location</b>			
4.3.1	Locate buildings to enhance visibility and identity while assuring compatibility with adjacent street frontages	<b>X</b>		
4.3.2	Loading and Service areas do not face roadways and street frontages	<b>X</b>		
4.3.3	Convenient access to building entrances and efficient circulation for vehicles and pedestrians	<b>X</b>		
4.3.4	All buildings/site plans shall provide outdoor break area with seating and shade trees	<b>X</b>		
4.3.5	Multiple buildings on a single site must be coordinated to separate service areas from more public areas	<b>X</b>		
<i>The Site Plan of the overall design plans illustrates that the building will have enhanced visibility and identity through use of bold glazing features at the office endcaps/tower elements. Truck courts are screened from public view by a 12-foot tall decorative Meridian wall.</i>				
<b>4.4</b>	<b>Site Access/Driveway Widths and Locations</b>			
	<ul style="list-style-type: none"> <li>Vehicular access points limited to minimize disruption of traffic flow and setback zones</li> <li>Driveway criteria and size locations shall be as outlined in the Dev. Code</li> </ul>	<b>X</b> <b>X</b>		

<p><i>Vehicular access to the site will be provided from two driveways at each side of the parcel. The driveway criteria and size locations conform to what is outlined in the Dev. Code.</i></p>			
<p><b>4.5 Vehicular Circulation</b></p> <ul style="list-style-type: none"> <li>▪ Avoid dead-end parking aisles.</li> <li>▪ Development needs to meet requirements of County Fire Dept. including apparatus turn arounds</li> </ul> <p><i>As per the design plans, the proposed circulation of the site is proposed without disruption of the traffic flow. The design includes Riverside County Fire Department's requirement of a 30-foot wide fire lane around the perimeter of the buildings.</i></p>	<p>X X</p>		
<p><b>4.6 Parking</b></p> <ul style="list-style-type: none"> <li>▪ Meet number of spaces per Specific Plan</li> <li>▪ Provide handicapped and carpool parking spaces in convenient locations</li> <li>▪ Meet parking dimensions of Development Code (9' X 18')</li> <li>▪ Meet parking landscape requirements of Design Guidelines (See Section 6)</li> </ul> <p><i>The proposed number of parking spaces meets the parking ratios as identified in the March Business Center Specific Plan – there are a total of 246 auto parking stalls provided whereas 244 are required, and all are designed to meet Building and MJPC Development Code requirements. The parking landscape requirements found in Section 6 of the MBC Design Guidelines are consistent with the proposed landscaping to be located in the front yard of the proposed development.</i></p>	<p>X X X X</p>		
<p><b>4.7 Pedestrian/Bicycle Circulation</b></p> <ul style="list-style-type: none"> <li>▪ Provide pedestrian access from streets to building entrances (ADA)</li> <li>▪ Minimum of three bicycle parking spaces for each building near entrance</li> </ul> <p><i>The proposed development site provides two locations for (ADA) path of travel from the street to the building entrances. The required bike parking spaces per building are provided are conditioned to comply with March JPA requirements.</i></p>	<p>X X</p>		
<p><b>4.8 Service Areas</b></p> <ul style="list-style-type: none"> <li>▪ Service, storage, maintenance, loading and refuse collection areas out of view from streets and adjacent buildings and screened by dense landscape and architectural barriers. These enclosures shall be fully screened by 6' decorative wall and finished opaque screening gates</li> <li>▪ Landscape used to deemphasize enclosures</li> <li>▪ All refuse enclosures to be compatible with development architecture</li> <li>▪ Service areas may not extend into required setbacks</li> <li>▪ Service areas do not disrupt vehicular and pedestrian circulation</li> <li>▪ No loading or unloading permitted on public streets or in front or street side yards</li> <li>▪ Concrete paving required for all service areas</li> <li>▪ Backflow preventers screened</li> <li>▪ Loading bays, tractor trailers and outdoor storage screened</li> </ul>	<p>X  X X X X  X X X</p>		

<p><i>In regards to the service areas (service, storage, maintenance, loading, and refuse collection) all are located out of view from Krameria. Screening is provided by the existing LLMD landscaping along Krameria Avenue, and a new 12-foot tall decorative wall, along with new landscape associated the development will also help to screen these areas. As identified in Standard condition #33, the Planning Department will review the method of screening transformer cabinets and backflow preventers, prior to issuance of a Certificate of Occupancy.</i></p>			
<p><b>4.9 Grading and Drainage</b></p> <ul style="list-style-type: none"> <li>▪ Surface drainage and soil erosion designed to prevent flow off-site</li> <li>▪ On-site grading compatible w/ streetscape grades</li> <li>▪ Slopes shall avoid the need for ramps with handrails</li> </ul> <p><i>As identified in the preliminary Drainage Study, the plans identify an onsite water retention system. As a result, storm water flows will not exceed existing conditions.</i></p>	<p>X X X</p>		
<p><b>4.10 Utilities</b></p> <ul style="list-style-type: none"> <li>▪ All utilities are to be installed underground</li> <li>▪ Easements that preclude planting of perimeter trees shall not be placed where landscape is required</li> </ul> <p><i>All utilities are required to be underground, as required by the approved Specific Plan and Standard Condition of Approval #10. There are no utility easements that would prevent the planting of the perimeter trees or other landscaping.</i></p>	<p>X X</p>		
<p><b>4.11 Walls and Fences</b></p> <ul style="list-style-type: none"> <li>▪ Walls, when used, must be an integral portion of the architecture and landscape</li> <li>▪ No walls are permitted within the street side landscaping setback areas except for 30" parking screening walls</li> <li>▪ Side and rear wall not to exceed 8'-0" unless approved by the JPA</li> <li>▪ Refuse enclosure walls must be 6'-0" high</li> <li>▪ All walls shall be compatible with building finish and materials</li> <li>▪ Walls adjacent to Van Buren, Cactus, Alessandro, Barton, Krameria, and Meridian Parkway shall match the development theme wall</li> <li>▪ Walls adjacent to the open space area located west of the northern campus shall comply with the typical wall/fence requirements in the Design Guidelines.</li> <li>▪ No chain link fences allowed w/in 100' of public R.O.W.</li> <li>▪ Beyond 100' R.O.W.: Black, commercial grade, vinyl clad chain link fences are permitted</li> <li>▪ Razor wire and barbed wire are not permitted</li> <li>▪ Where chain link is used, tree and shrub landscape planting providing 75% opacity is required within two years of development</li> <li>▪ Other walls and fences shall have a minimum of 30% opacity (2 years)</li> </ul> <p><i>There are two truck court walls associated with the project, the north wall will</i></p>	<p>X  X  X X X X  X  X X  X</p>		

<i>span the property line, the second wall will shield the front truck court. There is also an existing LLMD to provide additional screening from Krameria Avenue.</i>				
<b>5.0 Architecture</b>				
<b>5.1 Objective: Designed to express high quality business center which is progressive and enduring. Promotes unified image throughout the business center</b>				
<b>5.2 Architectural Character</b>				
5.2.1 Appropriate Characteristics				
▪ Clean, smooth, efficient lines		X		
▪ Distinctive yet compatible with		X		
▪ Meridian style and theme		X		
5.2.2 Inappropriate Characteristics				
▪ Trendy, historical, residential styles			X	
▪ Tricky, complicated, arbitrary forms			X	
▪ Sharp contrast w/ surroundings			X	
▪ Inappropriate or no articulation or fenestration			X	
<i>The architectural character of the proposed business park has clean, smooth and efficient lines. The building architecture includes variations in color, the uses of building articulation along the walls, and tower elements at the endcaps, with material changes and glazing. The proposed exterior colors are consistent with the Color Palette contained within the March Business Center Design Guidelines. The prominent use of glazing at the office features is distinctive yet compatible with the March style and theme.</i>				
<b>5.3 Building Form and Massing</b>				
5.3.1 Appropriate Treatment				
▪ Straightforward geometry		X		
▪ Unified composition		X		
▪ Expression of floor levels and structure		X		
▪ Solid parapets		X		
5.3.2 Inappropriate Treatment				
▪ Complicated forms			X	
▪ Arbitrary, inconsistent composition			X	
▪ Monolithic blocks			X	
▪ Glass box forms			X	
<i>The form and massing of the proposed buildings include straightforward geometry and unified composition. The building is designed to have two prominent entrances/architectural interest features located at the office areas/tower end caps, with two pop-out towers in the middle of the building to provide “architecturalizing” to the building. The color pattern, parapet relief and spandrel glass features are used as a grid module to reinforce the organization of the building design. The building features are solid concrete panels to emulate features at the tower endcaps. These features comply with the March Business Center Design Guidelines.</i>				



<p><b>5.4 Facades</b></p> <p>5.4.1 Appropriate Treatment</p> <ul style="list-style-type: none"> <li>▪ Straightforward, functional design</li> <li>▪ Expression of structure</li> <li>▪ Unity and scale reinforced through grid module</li> <li>▪ Surface delineation through expression of reveals, mullions, recesses, and structure</li> </ul> <p>5.4.2 Inappropriate Treatment</p> <ul style="list-style-type: none"> <li>▪ Arbitrary, inconsistent forms and decoration</li> <li>▪ Uninterrupted, floating horizontals</li> <li>▪ Blank surfaces</li> <li>▪ Buildings can be designed with a consistent, uniform facade with the center or corners of the façade emphasized</li> </ul> <p><i>The proposed business park includes pronounced office features. The building elevations use an integrated grid module that features spandrel glass features. The building elevations, shown within the design plans, illustrate the proposed design of each façade. Each façade includes prominent storefront glazing to offer more interest and appeal.</i></p>	<p>X X X X</p>	<p>X X X X</p>	
<p><b>5.5 Structure</b></p> <p>5.5.1 Appropriate Treatment</p> <ul style="list-style-type: none"> <li>▪ Visible vertical support</li> <li>▪ Visible structural base</li> <li>▪ Functional, straightforward columns</li> <li>▪ Columns integrated into the façade</li> <li>▪ Proper structural scale</li> </ul> <p>5.5.2 Inappropriate Treatment</p> <ul style="list-style-type: none"> <li>▪ Floating horizontal levels</li> <li>▪ Glazing direct to ground (except at entries)</li> <li>▪ False, decorative structure</li> </ul> <p><i>The structure of the proposed building is emphasized through use of the gradient color scheme which provides darker colors at the tower element and at the pop-outs, the structure is further reinforced through use of the grid module, which focuses on the repeating projecting features and additional glazing in the grid module. The main entrances also incorporate an Anodized Aluminum storefronts for further architectural detail. The applicant is not proposing to incorporate columns along the exterior of the buildings, but does propose two additional tower elements to break up the façade to provide additional visual interest.</i></p>	<p>X X  X</p>	<p>X X  X X X</p>	
<p><b>5.6 Fenestration</b></p> <p>5.6.1 Appropriate Treatment</p> <ul style="list-style-type: none"> <li>▪ Functional glass use and patterns</li> <li>▪ Glazing delineation by mullions and structure</li> <li>▪ Balance of wall and glazed surfaces</li> <li>▪ Tinted or lightly reflective glazing</li> </ul>	<p>X X X X</p>		

<p>5.6.2 Inappropriate Treatment</p> <ul style="list-style-type: none"> <li>Arbitrary, decorative glass patterns</li> <li>Uninterrupted horizontal glazing</li> <li>Sloping glass</li> </ul> <p>Overly reflective glass (more than 35% reflectance)</p> <p><i>The fenestration of the proposed building is detailed along the façades of the building. The design plans detail each building elevation design. The fenestration includes use of a balance of glazing through emphasis at the office corners while repeating the glazing theme within a repetitive grid module, use of building scoring to reinforce the grid module, and of use of an appropriate glazing design that meets the maximum reflectance (25%), while subtly contrasting the building earth tone colors.</i></p>		<p>X X X X</p>	
<p>5.6.3 Glazing Colors</p> <ul style="list-style-type: none"> <li>Preferred: earth tone palette</li> <li>Prohibited: bright or multiple colors inconsistent with the color palette</li> </ul> <p><i>The proposed glazing is high performance light-blue/grey glass. The applicant has chosen to select a subtly contrasting the blue/grey glazing color to emphasize the balance of glazing and earth tone field color, rather than pursue an earth tone glazing color. This option is provided in the Design Guidelines and is effective in this execution as the subtle contrast helps break up the length of the façade.</i></p>	<p>X</p>	<p>X</p>	
<p>5.6.4 Reflectivity</p> <ul style="list-style-type: none"> <li>Glass should generally be non-reflective</li> <li>25% maximum allowable outdoor reflectance</li> </ul> <p><i>The proposed glazing is high performance Solarclear glass having a maximum reflectance of 25%.</i></p>	<p>X X</p>		
<p><b>5.7 Roofs</b></p> <p>5.7.1 Appropriate Treatment</p> <ul style="list-style-type: none"> <li>Visible vertical support</li> <li>Horizontal planes and parapets</li> <li>Varied parapet height</li> <li>Roofing materials hidden from off-site view of adjacent roadways/streets</li> </ul> <p>5.7.2 Inappropriate Treatment</p> <ul style="list-style-type: none"> <li>Gable, hip, and mansard roof forms (may be acceptable at entries)</li> <li>Metal, tile, shingle and shake roofing (may be acceptable at entries)</li> <li>Arbitrary decoration</li> </ul> <p><i>The proposed roof design for the business park includes variation in vertical support emphasized through vertical score lines and a repeating grid module. Rooftop mounted equipment will be screened from off-site view on adjacent roadways/streets. No gabled, hip or mansard roof forms are proposed.</i></p>	<p>X X X X</p>	<p>X X X</p>	
<p><b>5.8 Entrances</b></p> <p>5.8.1 Appropriate Treatment</p> <ul style="list-style-type: none"> <li>Light, open, inviting aspect</li> </ul>	<p>X</p>		

<ul style="list-style-type: none"> <li>▪ Entry space sequence</li> <li>▪ Recessed, protected doorway</li> <li>▪ Integration w/ overall building form</li> <li>▪ Coordinated landscaping</li> </ul>	X X X X		
5.8.2 Inappropriate Treatment <ul style="list-style-type: none"> <li>▪ Exaggerated forms and color</li> <li>▪ Dark, confined appearance</li> <li>▪ Abrupt entry (Flush doorways, tacked-on entry alcove)</li>   <li>▪ Each building shall have at least one Building Entrance Element</li> <li>▪ Located at primary façade or facing street frontage</li> <li>▪ Entrance element to be at least 2/3 of overall building height</li> <li>▪ Entrance element to have large scaled, simple, clean forms</li> <li>▪ Canopies of contrasting permanent materials encouraged</li> <li>▪ Highly articulated, large-scale, bold forms encouraged</li> <li>▪ Entrances can be emphasized with portals and arcades</li> </ul>		X X X  X X X X X X X	
<p><i>The applicant is proposing an industrial warehouse project consisting of one building with two entrances. Both the site plan and building elevations identify all proposed entrances around the buildings. All entrances propose the use of prominent glazing to emphasize, through the reflectance of light, a light, open and inviting building entrance design. The prominent landscape areas () adjacent to the building entrances provides for an entry sequence completed through the recessed building entrance. The effect of the office design and coordinated landscaping at the building entrance provides for the coordination and integration of the building form and site landscaping design.</i></p>			
5.9 Materials 5.9.1 Appropriate Materials <ul style="list-style-type: none"> <li>▪ Smooth, precast or tilt-up concrete</li> <li>▪ Smooth metal panel systems used as an accent only</li> <li>▪ Ribbed or rough textured concrete used as an accent</li> <li>▪ Tinted or lightly reflective glass</li> <li>▪ Stucco or E.I.F.S. used as an accent or contrast of materials only</li> </ul> 5.9.2 Inappropriate Treatment <ul style="list-style-type: none"> <li>▪ Wood beams and siding</li> <li>▪ Brick, Spanish tile</li> <li>▪ Corrugated metal</li> <li>▪ Highly reflective glass</li> </ul> 5.9.3 Other Materials <ul style="list-style-type: none"> <li>▪ All other materials including Drivit®, concrete masonry, wall tile, etc must be approved by the JPA</li> <li>▪ Pre-fabricated buildings require Joint Powers Commission approval</li> </ul>	X X X X X	           X X X X   X  X	
<p><i>The applicant is proposing a smooth, tilt-up concrete walls with tinted and lightly reflective glass along the façade. The building color is composed of light earth</i></p>			

<i>tones with coordinating landscaping. The applicant is also proposing to use a r cement panel and Anodized Aluminum storefronts the tower end cape feature.</i>				
<b>5.10 Exterior Color</b> Appropriate Treatment <ul style="list-style-type: none"> <li>Concrete or stone should have natural finish</li> <li>Other colors may be permitted on special features, reveals or mullions</li> <li>Service door and mechanical screen colors are to be the same as the adjacent wall color</li> </ul> Inappropriate Treatment <ul style="list-style-type: none"> <li>Dark natural finishes</li> <li>Arbitrary patterns, stripes</li> <li>Garish use of color</li> <li>Broad painted band to reflect a corporate theme</li> </ul> <p><i>The proposed exterior color palette will incorporate colors allowed by the March Business Center Design Guidelines, as well as other compatible earth tone colors. Service doors are painted the background field color, so as to not emphasize the utilitarian function of the building. The concrete finish is a natural smooth finish. All exterior color and finishes are consistent with the March Business Center Design Guidelines.</i></p>		X X X	X X X X	
<b>5.11 Design Details</b> Appropriate Treatment <ul style="list-style-type: none"> <li>Coordinated mullions and details</li> <li>Expression and alignment of structural connections</li> <li>Finishes commensurate w/ building materials</li> <li>Coordinated entry spaces and landscaping</li> </ul> Inappropriate Treatment <ul style="list-style-type: none"> <li>Insufficient or excessive detailing</li> <li>Inadequate interface between materials</li> <li>No indication of scale</li> <li>Lack of interest</li> </ul> <p><i>The finishes comply with the design requirements identified in the March Business Center Design Guidelines. The repeating grid module provides the expression of structural alignment and coordinated finishes and details. As identified in Section 5.8 above, landscape and entry features are coordinated with the building design.</i></p>		X X X X	X X X X	
<b>5.12 Equipment:</b> All exterior equipment including, but not limited to, mechanical equipment, electrical equipment, electrical equipment, storage tanks, risers, electrical conduit, gas lines, cellular facilities, and satellite dishes must be screened from on-site and off-site views.				
<b>5.12.1 Appropriate Treatment</b> <ul style="list-style-type: none"> <li>Rooftop equipment shall be screened from off-site views by a parapet wall exceeding the height of the equipment</li> <li>Mechanical screen fully integrated into architecture</li> </ul>		X X		

<ul style="list-style-type: none"> <li>▪ Ground equipment hidden by screen walls or landscaping</li> <li>▪ Screen walls or same or similar material as building walls</li> </ul> <p>5.12.2 Inappropriate Treatment</p> <ul style="list-style-type: none"> <li>▪ Improperly located equipment requiring non-integrated walls</li> <li>▪ Rooftop equipment extending above parapet or screen</li> <li>▪ Rooftop screen too close to parapet</li> <li>▪ Screen material contrasting w/ adjacent surfaces</li> <li>▪ Rooftop screen “hat”</li> </ul> <p><i>The architect has designed each building in a way that all rooftop equipment will be screened by the height of the parapet. Any ground equipment will be screened by landscaping. The project screen walls are integrated into the overall design and are similar to the building design. Any future roof mounted equipment not anticipated with this submittal will be required to be screened from public view.</i></p>	<p>X</p> <p>X</p>	<p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p>	
<b>6.0 Landscaping</b>			
<p><b>6.1 Objective:</b> Promote a pleasant, distinctive, appropriate environment. Augment internal cohesion and continuity within March Business Center. Clean, simple and straight forward urban design. Promotes water conservation</p>			
<p><b>6.2 Water Conservation Measures</b></p> <p>6.2.1 Landscape areas to have complete, state-of-the-art, automatic irrigation system</p> <p><i>The project will be equipped with a reclaimed water irrigation system consisting of evapotranspiration weather based smart controller, low flow rotors, bubbler and/or drip systems used throughout. The irrigation water efficiency will meet or surpass the current state mandated AB-1881 water ordinance. All irrigation shall be approved by WMWD and March JPA prior to installation.</i></p>	<p>X</p>		
<p>6.2.2 Conservation Measures:</p> <ul style="list-style-type: none"> <li>▪ Site analysis of macro and micro climates, solar exposure, prevailing wind conditions, seasonal temperature patterns, soils and drainage, grades and slopes</li> <li>▪ Planting zone usage according to plant type, climatic exposure, soil condition and slope to facilitate use of zoned irrigation systems</li> <li>▪ Use of low water or drought tolerant plant species in landscape areas</li> <li>▪ Meets peak moisture demand of all plant materials w/in design zones, while avoiding flow rates that exceed infiltration rate of soil</li> <li>▪ Design to prevent overspray or discharge onto roadways, non-landscaped areas or adjacent properties</li> <li>▪ Timing of irrigation cycles to be set at night when wind, evaporation and people activities are at a minimum</li> </ul> <p><i>The project’s landscaping has been selected from the “Allowable Plant Materials for Meridian Lots. The plant species selected consist of low water use or drought tolerant and non-invasive plant species. Additionally, the project will be equipped with a low flow irrigation system consisting of evapotranspiration weather based smart controller, low flow rotors, bubbler and/or drip systems used throughout.</i></p>	<p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p>		



<p><i>The irrigation water efficiency will meet or surpass the current state mandated AB-1881 water ordinance. All irrigation shall be approved by WMWD prior to installation. The specifics to this measure will be determined during the construction plan review process where irrigation plans are reviewed to determine compliance with these measures. The developer has ensured these measures will be complied with in the detailed project description.</i></p>			
<p><b>6.3 Streetscapes</b></p> <ul style="list-style-type: none"> <li>▪ Landscape along public streets reinforces unified appearance and street hierarchy</li> <li>▪ Establishes sense of place, particularly at street intersections</li> </ul> <p><i>The March Business Center Specific Plan establishes a sense of place through use of a double row of trees with a detached sidewalk located between the rows of trees on arterial streets. This design is proposed to enhance the existing LLMD landscaping adjacent to Krameria Avenue with the use of trees and shrubs consistent with the March Business Center Specific Plan.</i></p>	<p><b>X</b></p> <p><b>X</b></p>		
<p><b>6.3.1 General Design Criteria</b></p> <ul style="list-style-type: none"> <li>▪ Minimum 24" box size trees planted along all street frontages</li> <li>▪ Street trees planted 40' on center or clustered as approved by JPA, clustered trees to average 40' on center</li> <li>▪ Hedges on street frontages should be trimmed to a formal, rectangular shape</li> <li>▪ Hedge minimum sizes at installation is five gallon</li> <li>▪ If required interior landscape creates an operational hazard, the JPA may allow latitude to shift landscape to larger landscape areas, as approved on a case-by-case basis</li> <li>▪ Turf area usage should be approximately 20% of the landscape area. (in no case shall it exceed 50%)</li> <li>▪ Turf shall not exceed 50% of landscape area nor shall it be less than 10% of landscape area.</li> </ul> <p><i>The site incorporates 24" and 36" box trees along the driveway entrance from Krameria Avenue. The street trees are existing and within the Landscape Lighting and Maintenance District (LLMD) easement area along Krameria Avenue. The LLMD planting areas in the Meridian South Campus includes the use of low-growing shrubs and trees The turf provisions in the Design Guidelines have been superseded by the irrigation water efficiency mandated by the AB-1881 water ordinance, to reduce water waste in landscape designs. No turf is proposed as part of the onsite landscape design for the business park.</i></p>	<p><b>X</b></p> <p><b>X</b></p>	<p><b>X</b></p> <p><b>X</b></p> <p><b>X</b></p> <p><b>X</b></p> <p><b>X</b></p>	
<b>6.4 On-Site Landscaping</b>			
<p><b>6.4.1</b> Use landscape to reinforce site planning such as trees to define parking lots and drive aisles</p> <p><i>According to the conceptual landscape plans, , the site will define the parking lots and drive aisles with African Sumac, Crape Myrtle and Afghan Pine trees.</i></p>	<p><b>X</b></p>		
<p><b>6.4.2</b> Plant materials shall be selected from Plant Selection List in Section 6.5 of the Meridian Business Center Design Guidelines.</p>		<p><b>X</b></p>	

<b><i>The proposed landscape plan is consistent with the Plant Selection List of Section 6.5 of the MBC Design Guidelines.</i></b>				
6.4.3	<p>Parking area trees shall be provided per the Development Code. Parking shall be screened from public streets by naturally contoured landscape berms measuring 2.0' to 3.5', or a 30" parking screening wall. Landscape must be used to supplement or replace the berms or wall.</p> <p><b><i>According to the conceptual landscape plans, the parking area shall be screened from the roadway by the existing LLMD planting area, on-site planting which include a continuous screen shrub along parkway curb, assorted drought tolerant back drop, layered accent plants and the trees as noted in section .</i></b></p>	X		
6.4.4	<p>Comprehensive planting required along building and site perimeters</p> <ul style="list-style-type: none"> <li>▪ Minimum landscape set back of 15' required along all front yards</li> <li>▪ Minimum front yard setback is typically 20' or 25' (see Specific Plan)</li> <li>▪ Minimum landscape setback of 5' required along at least one side of internal property lines</li> </ul> <p><b><i>According to the conceptual landscape plan, the applicant proposes a 20-30+/- foot landscaped setback consistent with the Design Guidelines and Specific Plan, a 15 foot landscaped setback between property line and the north parking lot and a 5 foot landscape setback along the rear and interior side-yard. All landscape provisions are consistent with the Design Guidelines.</i></b></p>	<p>X</p> <p>X</p> <p>X</p>		
6.4.5	<p>All buildings facing a roadway frontage shall have a foundation planting area</p> <p><b><i>According to the conceptual landscape plan Sheet T, the proposed buildings identify a foundation planting. The proposed landscaping used for the foundation planting includes trees, shrubs and groundcover.</i></b></p>	X		
6.4.6	<p>Large natural granite boulders are <u>encouraged</u> as part of the landscape theme</p> <p><b><i>The use of large, natural granite boulders is not proposed within the on-site landscaping for the project. As this was only encouraged, the design is still consistent with the Design Guidelines.</i></b></p>		X	
6.4.7	<p>Drainage/retention areas should be landscaped with native vegetation in a natural manner w/ minimal use of turf</p> <p><b><i>The proposed drainage/retention areas are located underground. As the site meets the minimum 10% landscape provision, the design is consistent with the March Business Center Design Guidelines.</i></b></p>	X		
6.5	<p><b>On-Site Plant Selection List</b></p> <ul style="list-style-type: none"> <li>▪ See Design Guidelines</li> </ul> <p><b><i>On-site Plant Selection meets the minimum standards of the March Business Center Design Guidelines.</i></b></p>		X	

<b>7.0     <u>Lighting</u></b>			
<b>7.1     Objective:</b> Exterior lighting is to be provided to enhance the safety and security of motorists, pedestrians, and cyclists. To reinforce identity and unity of March Business Center, all exterior lighting is to be generally consistent in height, spacing, color and type of fixture.			
<b>7.2     Public Street lighting is coordinated throughout March Business Center</b>  <i>All March Business Center streets are illuminated with High Pressure Sodium street lighting consistent with Riverside County Road Standards Ordinance #461, specific detail #1000 and #1001. This measure has been complied with and is ongoing during development.</i>	<b>X</b>		
<b>7.3     General On-Site Lighting Parameters</b>  7.3.1 On-site lighting includes parking areas, vehicular and pedestrian circulation, building exteriors, service areas, landscaping, security, and special effects  <i>The applicant is proposing to include on-site lighting in the parking areas of the site for security and also along the building exterior for the purposes of visibility and security. All lighting is HPA, LPS or LED at or within 100 Kelvin of 2,700 Kelvin, maximum height 25', full shielded and not to exceed 750 watts, as identified in Standard Condition of Approval # 27.</i>	<b>X</b>		
7.3.2 All exterior on-site lighting must be fully shielded and generally confined w/in site boundaries. Maximum lighting level at the property line of a site is .5 candle/ft. Maximum wattage for exterior lighting shall be 750 watts.  <i>Standard JPA note #7 on site plan requires full compliance with this measure. This item is in compliance with the March Business Center Design Guidelines.</i>	<b>X</b>		
7.3.3 Lighting fixtures are to be of clean, contemporary design All exterior illumination shall be high-pressure sodium or low-pressure sodium. Metal Halide is prohibited  <i>Standard JPA note #6 and #7 on site plan requires full compliance with this measure. This item is in compliance with the March Business Center Design Guidelines.</i>	<b>X</b>		
7.3.4 Point-by-point lighting analysis required at construction plan submittal  <i>A point-by-point lighting analysis will be completed as part of the construction plan submittal process.</i>	<b>X</b>		
7.3.5 Tilted wall fixtures (emitting light above 90 degree horizontal) are not permitted. ▪ Maximum height of building mounted light fixtures is 25'.  <i>Full compliance with fully shielded light fixtures and 25' maximum height is assured through Conditions of Approval.</i>	<b>X</b>		
7.3.6 Wall-mounted utility lights that cause off-site glare are prohibited "Shoobox" or cut-off, glare-controlled lights are preferred  <i>This measure will not be determined until the lighting plans are submitted during</i>		<b>X</b>	

<i>the construction plan review process, and the actual details of the light fixtures are reviewed.</i>				
<b>7.4 Vehicular Circulation and Parking Area Lighting</b>				
7.4.1 All vehicular circulation and parking lot lighting shall have cut-off fixtures (i.e., bulb/source is not visible above horizontal)	X			
▪ Pole and fixture height shall not exceed 25' overall				
<i>Full compliance with fully shielded light fixtures and 25' maximum height is assured Conditions of Approval.</i>				
7.4.2 Pole footings in traffic areas shall be above grade and a maximum of 24" high.		X		
<i>Pole footings specs are not a component of the design plan review. Pole footings are called out in the construction plans. This measure will be determined during the construction plan check review process.</i>				
7.4.3 All luminaries shall be high-pressure sodium or low-pressure sodium lighting.	X			
<i>The applicant's narrative identifies full compliance with these lighting types, and is conditioned for the project.</i>				
7.4.4 Lighting Levels				
▪ Vehicle circulation lighting shall maintain an average lighting level of .52 ft/candle, minimum .13 ft/candle and 4:1 uniformity ratio			X	
▪ Parking areas shall have an average lighting level of 1.60 ft/candle, minimum .14 ft/candle and 4:1 uniformity ratio			X	
<i>This measure will not be determined until lighting plans are submitted during the construction plan review process. The level of lighting will be confirmed through review of the point-by-point lighting analysis, submitted with the project construction plans. Conformance cannot be assured until the construction plans and future tenant are known.</i>				
<b>7.5 Pedestrian Circulation Lighting</b>				
7.5.1 Pedestrian walkways and building entries shall be illuminated to provide a secure route between parking areas and points of entry to the building			X	
▪ Building entries may be lit with soffit, bollard, step or comparable lighting			X	
<i>This measure will not be determined until lighting plans are submitted during the construction plan review process. Conformance cannot be assured until construction plans and future tenant are known.</i>				
7.5.2 Step or bollard lighting shall be used to clearly illuminate level changes and handrails for stairs and ramps. Bollard lighting shall not exceed 42"				
<i>This measure will not be determined until lighting plans are submitted during the construction plan review process.</i>			X	
7.5.3 Courtyards, arcades, and seating areas shall be lighted to promote pedestrian use and safety				
<i>This measure will be determined during the construction plan check review</i>			X	

<b>process.</b>				
7.5.4	Pedestrian lighting shall be high-pressure sodium, low-pressure sodium, or incandescent lamps		X	
<i>This measure will not be determined until lighting plans are submitted during the construction plan review process. Conformance cannot be assured until construction plans and future tenant are known.</i>				
<b>7.6 Architectural Lighting</b>				
7.6.1	Utilize indirect or hidden lighting sources for wall washing, overhead down lighting, and interior lighting that spills outside). All architectural lighting shall be fully shielded to eliminate views of the source of illumination		X	
<i>No accent lighting for architectural features, landscape features or pedestrian areas is currently planned as a component of the development. However, the future tenant may have an interest in accent lighting. Accordingly, it is premature to address specialized accent lighting until the ultimate tenant is known and the lighting designer has reviewed the March Business Center Design Guidelines.</i>				
7.6.2	Entry areas should be lit to provide an inviting glow from the interior		X	
<i>This measure will not be determined until lighting plans are submitted during the construction plan review process. The future lighting plans shall be required to comply with the March Business Center Design Guidelines.</i>				
<b>7.7 Service Area Lighting</b>				
▪	Lighting must be fully shielded, HPS or LPS type	X		
<i>This item is in compliance with the March Business Center Design Guidelines as conditioned.</i>				
<b>7.8 Accent Lighting</b>				
▪	Unique lighting may be used to feature architectural elements, landscaping, entries and pedestrian areas		X	
▪	Accent lighting for landscaping and pedestrian areas shall be shielded and be HPS only		X	
<i>No accent lighting for architectural features, landscape features or pedestrian areas is currently planned as a component of the speculative business park development. However, the future tenants may have an interest in accent lighting. Accordingly, it is premature to address specialized accent lighting until the ultimate tenant is known and the lighting designer has reviewed the March Business Center Design Guidelines.</i>				
<b>8.0 Signage Requirements</b>				
<b>8.2 Materials</b>				
▪	Contemporary, professional, and creative use of simple, low maintenance materials		X	
▪	Metal or masonry materials of high quality and high longevity industrial/commercial grade		X	

<p><i>Since the proposed development is designed as a speculative industrial warehouse and no tenants are known at this time, the signage requirements will not be applicable during the design plan review process. A sign program will be required to ensure the signage is uniform throughout the development.</i></p>			
<p><b>8.3 Monument Signs at Lots</b></p> <ul style="list-style-type: none"> <li>One monument sign provided per lot.</li> <li>May be up to 10' long x 8' tall x 2' deep</li> <li>Monument signs should be perpendicular to the adjacent roadway</li> <li>Signs of made of prefinished, flat-faced metal panels</li> <li>Letters 14" high. 1" deep.</li> <li>Reverse or exterior lit encouraged.</li> <li>Illumination of the sign copy and logo only, w/ no illumination of sign background</li> <li>Shall have the numerical street address on sign w/ a minimum 6" high copy</li> </ul> <p><i>Since the proposed development is designed as a speculative warehouse and tenants are not known, the signage requirements are not applicable during the design plan review process. A sign program will be required to ensure the signage is uniform throughout the business park. Additionally, it is unknown whether the applicant is seeking a monument sign, however these requirements will be adhered to during the sign program review for the project.</i></p>		<p>X X X X X X X X</p>	
<p><b>8.4 Directional/Informational Signage</b></p> <p><b>8.4.1 Building Signs</b></p> <ul style="list-style-type: none"> <li>Permitted (1) building mounted illuminated sign on the facade facing the road serving as primary vehicular access to site and one additional sign if located on another street frontage (not to exceed two signs total)</li> <li>Height of sign shall not exceed 30% of the height of the façade and 10% of the building length</li> <li>Maximum length of (20) feet and a maximum of (80) S.F. For buildings in excess of 200 feet in length, they can have a maximum of (2) signs on each approved façade. The maximum length of the (2) signs shall not exceed 10% of the building length (or a maximum length of forty (40) feet and a maximum of 160 square feet)</li> </ul> <p><i>Since the proposed development is designed as a speculative warehouse and tenants have not be determined at this time, the signage requirements will not be applicable during the design plan review process. These requirements will be applicable once a tenant has been determined during the tenant improvement process.</i></p>		<p>X  X  X</p>	
<p><b>8.4.2 Freeway Signs</b></p> <ul style="list-style-type: none"> <li>Three freeway signs shall be permitted for the entire March Business Center</li> <li>Maximum height of 45', a maximum sign area of 150' and signage limited to the master project name logo and up to three tenants each</li> </ul> <p><i>Page 10-9 of the March Business Center Design Guidelines identifies the approved locations for the Freeway Signs. This locations is more than 1-mile from the site and are not applicable to this development.</i></p>		<p>X X</p>	



8.4.3	<p><b>Multi-Tenant Signage</b></p> <ul style="list-style-type: none"> <li>▪ For buildings which house more than one tenant, (1) additional building sign shall be permitted for each tenant, and tenant space</li> <li>▪ The signs shall not exceed (2) feet in height and not exceed 60% of the width of the tenant's lease space, up to a maximum of 15' in length</li> <li>▪ Permitted monument sign may be constructed in a way to allow identification of each tenant in addition to any identification of the building, up to 8 tenant maximum</li> </ul> <p><i>Since the project is designed as a speculative building development, signage will be required to meet the minimum requirements of the March Business Center Design Guidelines. Tenants have not been determined at this time, therefore these signage requirements will not be applicable during the design plan review process.</i></p>		<p>X</p> <p>X</p> <p>X</p>	
8.5	<p><b>Not Permitted</b></p> <ul style="list-style-type: none"> <li>▪ Animated Signs</li> <li>▪ Awning (containing signage)</li> <li>▪ Automatic Changing Sign</li> <li>▪ Special Event Sign</li> <li>▪ Changeable Copy Sign</li> <li>▪ Flags (Exception: Up to 3 flags/flagpoles may be provided, not to exceed 20' in height. Flags may contain the American flag, California flag, and corporate logo, no marketing information)</li> <li>▪ Flashing Sign</li> <li>▪ Marquee Sign</li> <li>▪ Outdoor Advertising Sign</li> <li>▪ Portable Sign</li> <li>▪ Pennant</li> <li>▪ Rotating Sign</li> <li>▪ Billboards, searchlights and inflatable signs</li> <li>▪ Other signs not specifically permitted in this section</li> </ul> <p><i>Since the project is designed as a speculative warehouse, signage will be required to meet the minimum requirements of the March Business Center Design Guidelines. Tenants have not been determined at this time, therefore these signage requirements will not be applicable during the design plan review process.</i></p>		<p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p> <p>X</p>	
8.6	<p><b>Additional Submittal Requirements</b></p> <ul style="list-style-type: none"> <li>▪ All signs must be approved by March JPA. Sign program approval required for multi-tenant sign programs.</li> <li>▪ Complies with the Uniform Building Code and the National Electrical Code Requirements</li> </ul> <p><i>Since the project is designed as a speculative warehouse, signage will be required to meet the minimum requirements of the March Business Center Design Guidelines. Tenants have not been determined at this time, therefore the signage requirements will not be applicable during the design plan review process.</i></p>		<p>X</p> <p>X</p>	
9.0	<p><b>Maintenance Requirements</b></p>			

<p><b>9.1 Common Areas</b></p> <ul style="list-style-type: none"> <li>▪ Landscaped areas within each lease parcel, including side and rear yards, shall be maintained by individual lease parcel tenants</li> <li>▪ Maintenance of each individual lease parcel shall conform to standards for common area maintenance</li> </ul> <p><i>The proposed landscape areas within the parcel shall be maintained by the individual lease parcel tenants. This is consistent with the Standard Operational Mitigation Measures, Condition # 4, all site landscaping is required to be maintained in a healthy and thriving condition.</i></p>	<p><b>X</b></p> <p><b>X</b></p>		
<p><b>9.2 Landscape</b></p>			
<p><b>9.2.1 General</b></p> <ul style="list-style-type: none"> <li>▪ Individual tenants shall be responsible for landscaping in their development areas within their property boundaries</li> <li>▪ If any existing landscape is damaged or removed during demolition or construction, it shall be repaired and/or replaced in kind and equivalent size per the approved plans within 30 days of completion of construction by the tenant</li> </ul> <p><i>The developer has provided the March JPA with conceptual landscape plans that comply with the March Business Center Design Guidelines. In compliance with the Standard Operational Mitigation Measures, Condition # 4, all site landscaping is required to be maintained in a healthy and thriving condition, free from weeds and trash.</i></p>	<p><b>X</b></p> <p><b>X</b></p>		
<p><b>9.2.2 Irrigation</b></p> <ul style="list-style-type: none"> <li>▪ Irrigation systems shall be designed to maintain the proposed vegetation</li> <li>▪ Irrigation systems shall comply with the March JPA Development Code</li> <li>▪ Submission of a complete irrigation system design shall be required consistent with Section 2 of these Guidelines and the Development Code</li> </ul> <p><i>According to the conceptual landscape plans, the irrigation system shall be designed to maintain the proposed landscaping. The proposed irrigation systems are designed to comply with the Landscape Water Efficiency Ordinance. Submission of a complete irrigation system design shall be required consistent with Section 2 of these Guidelines and the March JPA Water Efficiency Ordinance #JPA 16-03.</i></p>	<p><b>X</b></p> <p><b>X</b></p> <p><b>X</b></p>		

# Attachment 6

## Standard Conditions of Approval

**Standard Conditions of Approval**  
March Business Center, South Campus DJT6  
Design Plan 21-02

**PLANNING DEPARTMENT, MARCH JOINT POWERS AUTHORITY**

**General Planning Conditions:**

1. The site shall be developed in accordance with the approved Design Plan 21-06 on file in the Planning Department, the March Business Center Specific Plan (SP-1, Amendment #8), the applicable portions of the March JPA Development Code, the General Plan, and the conditions contained herein. Prior to any use of the site or business activity being commenced thereon, all Conditions of Approval shall be completed to the satisfaction of the Planning Director or designee. Building elevations shall comply with the color scheme and materials approved with the Design Plan. (MIPA 9.14.020)
2. The Applicant shall indemnify, protect, defend, and hold harmless March JPA and its constituent public agency members, officers, employees, attorneys, and agents from any and all actual or alleged claims, actions, proceedings, against March JPA to attack, set aside, void, annul, or seek monetary damages arising out of the approval of the Applicant's proposed development or related approval, including but not limited to CEQA approvals, permits, variances, design plans, plot plans, maps, licenses and amendments. March JPA shall promptly notify the Applicant of any and all claims, actions, and proceedings, covered by this Agreement. This indemnification shall include, but not be limited to, damages, fees, and/or costs awarded against March JPA, if any, and cost of suit, attorney's fees and other costs, liabilities and expenses incurred in connection with such claim, action, or proceeding whether incurred by Applicant, March JPA, and/or any parties bringing such forth.
3. This approval shall expire three years after the date of approval unless substantial construction contemplated by this approval has occurred within the three-year period, which is thereafter pursued to completion, or the March Joint Powers Authority has extended approval, otherwise the approval shall become null and void. (MIPA 9.02.230)
4. This development is located within Specific Plan SP-1, Amendment #8. Requirements of the March Business Center Specific Plan including the permitted uses, development standards, the March Business Center Design Guidelines, and the Conditions of Approval shall prevail unless modified herein. (MIPA 9.13)
5. Development within the Clear Zone and Accident Potential Zones I and II will abide by building standards and codes including height restrictions, restrictions on use, setbacks, population densities, and insulation and materials, as outlined in the approved 2005 Air Installation Compatible Use Zone (AICUZ).

6. As established in the Specific Plan, the project will comply with the policies and requirements of the Riverside County Airport Land Use Plan. Development plans will be submitted to the FAA for review in accordance with FAR 77.13.2.i. Additional ALUC review will be required for objects taller than 50 feet in the Height Caution Zone shown on Figure IV. A-4 [in the Focused EIR]. Other land use controls (relating to safety (both in the air and on the ground) and noise) have been developed in consultation with the ALUC and have been incorporated into the Specific Plan. (MMRP)
7. The development shall comply with the requirements of the Americans with Disabilities Act. All disabled parking spaces shall measure 14' wide to include a 9' stall and 5' loading area, by 20' length. One in every 8 disabled spaces, but in no case less than one, shall be van accessible with a minimum 9' wide space and 8' wide loading area. All signage, striping, ramps and slopes shall meet ADA requirements. Overhang of parking all not encroach upon handicapped access routes. (MJP 9.11.040)
8. The development shall incorporate 5% of parking spaces as preferential parking for car pools and vanpools in extremely convenient locations. (MMRP)
9. Any signs indicated on the submitted plans are not included with this approval. All signs proposed for this development shall comply with the requirements of the March JPA Development Code. If requirements differ, the proposed signage shall meet the stricter of the provisions. In a multiple tenant facility, all signage shall meet the approved Sign Program for the development. (MJP 9.12.020)
10. All new and existing utilities located on the site shall be under grounded. (MJP 9.13.130)
11. Buildings shall be designed to reduce energy usage by utilizing solar or low emission water heaters, double paned windows, using light colored roofing materials, using skylights in ambient warehouses, orienting buildings to the north for natural cooling to the extent practical, and increasing insulation beyond Title 24 requirements. (MEIR)
12. All development adjacent to the 175 acre Least Bell's Vireo conservation easement shall comply with the Biological mitigation measures pertaining to the Least Bell's Vireo habitat identified in the March Business Center Specific Plan (SCH #2002071089). (MMRP)
13. Preconstruction surveys will be conducted for burrowing owls prior to grading. These surveys shall conform to the survey protocol established by the California Burrowing Owl Consortium (1993). Preconstruction surveys shall be conducted no more than 30 days prior to the initiation of construction activities and at a 30-day intervals if construction activities have not been initiated in an area. If Burrowing Owls are observed within the Project area then passive relocation measures will be implemented consistent with the survey protocol identified above. (MMRP)
14. The project applicant shall incorporate the following measures to help reduce the project's potential solid waste impacts and to help in the County's effort to comply with State law in diverting solid waste from landfill disposal. Green waste generated by the project should be kept separate from other waste types in order that it can be recycled through the practice of grass recycling (where lawn clippings from a mulching type mower are left on the lawn) or onsite composting or directed to local wood grinding and/or composting operations. All projects shall

provide adequate area(s) for the collections and loading of recyclable materials. Prior to building permit issuance, the applicant shall submit a Recyclables Collection and Loading Area Plot Plan to the March JPA for review and approval. (MMRP)

15. Prior to individual lot development approval, the developer will coordinate with March JPA and shall contact WMWD to obtain the current flow reading and submit a sewer analysis demonstrating that the individual lot development projected sewer flows do not exceed the VBPS capacity (1.07 mgd). (MMRP)
16. Prior to individual lot development approval by the March JPA, the developer will coordinate with March JPA and WMWD to obtain the water demand for the proposed development to assure that the development is consistent with the water budget and Water Supply Assessment for the Project. (MMRP)
17. Uses located in proximity of noise sensitive uses shall comply with the noise mitigation measures identified in the March Business Center Specific Plan (SCH #2002071089). (MMRP)
18. All uses shall comply with the noise emission criteria identified in the March JPA Development Code. (MMRP)

**Planning Conditions to be Performed Prior to Issuance of Building Permits:**

19. Prior to obtaining a building permit, a site/building-specific acoustical report shall be prepared by an acoustical engineer or professional acoustician showing that interior noise levels within all habitable rooms do not exceed 45dB Ldn or CNEL for all new noise sensitive receptors (inclusive or transient lodging, educational facilities, and childcare facilities) which are identified in Figure IV. H-3, State of California Noise Compatibility Chart. Normally Unacceptable Uses identified in Figure IV. H-3, State of California Noise Compatibility Chart shall not be allowed. The recommendations of the noise report shall be specific window treatments, such as dual glazing, and mechanical ventilation when the 45 dBA CNEL limit can only be achieved with a closed window condition. (MMRP)
20. Office areas and noise sensitive uses within the 65 dB CNEL noise contour shall provide noise attenuation to provide an interior noise level of 45 dB CNEL, as certified by an acoustical engineer or architect. (MMRP)
21. Facilities that store, handle or use regulated substances as defined in the California Health and Safety Code 25532 shall comply with the Hazards and Hazardous Materials Mitigation Measures identified in the March Business Center Specific Plan (SCH #2002071089).
22. Prior to issuance of precise grading permits, all site plans, grading plans, landscape and irrigation plans, and street improvement plans shall be coordinated for consistency with this approval. Prior to issuance of building permits, final landscaping and irrigation plans shall be approved as a component of the construction plan review. (MJPA 9.02.030)
23. Prior to issuance of building permits, the developer or developer's successor-in-interest shall pay the applicable March JPA Development Impact Fee (DIF).

24. Individual lot developers shall contribute on a fair share basis toward the cumulative off-site impacts identified in Specific Plan Chapter IV. B-1-3. (MMRP)
25. Prior to issuance of building permits, the developer shall provide to the Planning Director written certification by the affected school district that either: (1) the development has complied with the fee levied by the governing board of the District, pursuant to Government Code Section 65996; or (2) the fee or other requirement does not apply to the development.
26. Prior to issuance of building permits, screening details for roof top equipment shall be submitted for Planning Division review and approval as a component of the construction plan review. All equipment shall be completely screened so as not to be visible from public view, and the screening shall be an integral part of the building. (MJP A Design Guidelines)
27. Prior to issuance of building permits, the Planning Division shall review and approve a computer generated, point-by-point exterior lighting analysis plan as a component of the construction plan review. The lighting plan shall be generated on the site plan and shall be integrated with the final landscape plan. The plan shall indicate the manufacturer's specifications for fully shielded light fixtures and shall include fixture catalog cuts, light cut-off shielding details, wattage, illumination type (high or low pressure sodium only), location, and height. The lighting shall not exceed a 750 watt maximum, shall not be mounted more than 25' above finished grade, and site lighting of outdoor areas shall be limited to high pressure or low pressure sodium lighting. The lighting shall be designed in such a manner so that it does not exceed 0.5 candle/foot<sup>2</sup> illumination beyond the property line. (MJP A 9.08.100)
28. The project construction plan submittal shall contain an on-site traffic striping and signage plan which shall be reviewed and approved by the Planning Department prior to issuance of the building permits.
29. CEQA Review of stationary source emissions other than natural gas and electricity shall be done on all developments emitting stationary source air pollutants. In addition, all developments involving stationary source emissions shall obtain permits to construct and operate from the SCAQMD. (MMRP)

**Planning Conditions to be Performed Prior to Issuance of Certificate of Occupancy:**

30. Prior to the issuance of a Certificate of Occupancy and once a tenant is known, the tenant shall submit to March JPA and shall receive approval of a Traffic Demand Management Plan which shall include the following elements: 1) identification of alternative modes of transportation and incentives available to employees; 2) the location of a minimum of 30" x 42" wall area for the posting of alternative transportation mode information including financial incentives by responsible agencies, transit schedules and carpooling information; 3) feasibility of flexible work hours including compressed work week; 4) availability of on-site and off-site child care facilities; 5) feasibility of video conference facilities; and 6) feasibility of minimizing the use of fleet vehicles during smog alerts. (MMRP)
31. Prior to issuance of a Certificate of Occupancy, the developer or developer's successor in interest shall pay the applicable Transportation Uniform Mitigation Fee (TUMF). This provision does not prohibit paying the TUMF earlier at the time of building permit issuance if permitted by the WRCOG. (MJP A Ord.)



32. Prior to Certificate of Occupancy, the developer shall demonstrate that all South Coast Air Quality Management District permits have been obtained. (MEIR)
33. Prior to Certificate of Occupancy, the Planning Division shall review and approve the location and method of enclosure or screening of transformer cabinets, commercial gas meters and back flow preventers as components of the construction plan review. Location and screening shall comply with the following criteria: All transformer cabinets and commercial gas meters shall not be located within driveway/street view triangles in such a way that they obscure traffic visibility. Service Electrical Systems shall be fully enclosed and incorporated into the overall architectural design of the building(s). Back-flow preventers and gas meters shall be screened by landscaping that will provide appropriate landscape screening upon maturity. (MJP Design Guidelines)
34. Prior to Certificate of Occupancy, each development incorporating truck docks or roll-up doors shall provide a minimum 6-sq/ft sign identifying the approved truck route plan at all service driveway locations. (MMRP)
35. Prior to Certificate of Occupancy, each development incorporating truck docks shall install one a 6-sq/ft. sign for each four roll-up dock doors identifying the maximum 5-minute diesel idling time. All signs shall be installed on the building at eye level in a manner to clearly convey the maximum diesel idling time. (CARB)

#### **Landscape Provisions (Timing Varies)**

36. All landscape located adjacent to parking areas shall be protected by a raised 6" extruded curb. (MJP Design Guidelines)
37. The Project is to provide a 6" concrete mow curb on property line along LLMD border.
38. LLMD maintenance manager shall review the proposed landscape and handicap accessible paths proposed from the public right-of-way through the LLMD area and onto the Project site as detailed on the landscape plans.
39. Prior to the issuance of the Certificate of Occupancy, the applicant shall provide a letter from the landscape architect identifying that all landscape has been installed consistent with the approved landscape plan and that the landscape complies with the provisions of the March JPA Landscape Irrigation Efficiency Ordinance (JPA ORD.#16-03).
40. The landscape irrigation schedule which identifies appropriate watering times, duration and quantities, for 1) initial landscaping and 2) established landscape, shall be continually available at the site in perpetuity, in a convenient location near the irrigation controlling unit. (JPA ORD. #16-03).
41. All on-site landscape irrigation shall be designed and constructed to meet non-potable "Purple Pipe" standards for reclaimed water systems. (MMRP)
42. All on-site parking shall be screened by one of the following methods: 1) 30" parking screen wall; 2) continuous landscape shrub; or 3) combination of shrub, wall and earthen berm. (MJP Design Guidelines).

43. The facility shall incorporate an on-site green/landscape waste recycling/mulching area, or as an option the facility operator may identify in writing how green/landscape waste is recycled off-site. (MMRP)
44. Prior to site grading, the grading contractor shall contact the March JPA Landscape and Lighting Maintenance District (LLMD) manager at (951) 656-7000 to coordinate all activity and impacts upon LLMD facilities within the public right-of-way and landscape easements. The coordination shall assure uninterrupted operation of the LLMD irrigation system and shall identify any impacts on LLMD mainlines, wires, laterals and irrigation components, inclusive of the need to move or sleeve LLMD facilities as determined by the LLMD manager.
45. All LLMD pressure main line and laterals crossing beneath paved areas shall be encased in schedule 40 PVC sleeves twice the diameter of the pipe. Sleeves shall be placed as a depth of 24" beneath finished surface, measured from the top of sleeve. Control wires shall be separate sleeves with a minimum diameter of two inches. Wire sleeves shall be sized so that wire bundles may be pulled without binding. On each side of paved areas, a 10" round valve box with wire connections shall be installed. Pin tie connectors shall be used in wire connection box and must be branded on the valve box lid with "SW" for future identification.
46. All changes to the LLMD easement area shall be completed in accordance with the approved LLMD landscape plans and an "as built" drawing shall be submitted and approved upon completion and inspection of work. All repairs and/or installations in the LLMD right-of-way and easement areas must be inspected by the LLMD manager.
47. All LLMD landscaping and irrigation shall be protected in place. Irrigation or landscape damaged by construction activities shall be repaired by a qualified irrigation technician within twenty-four (24) hours to ensure LLMD plant material does not incur additional damage or loss or a \$500 fine per day will be assessed until completed. Alternatively, the MJPA can have their LLMD contractor repair any damage and the bill owner directly at the request of the owner. Prior to removal of any landscaping or irrigation, the developer or contractor shall arrange an on-site meeting to physically delineate locations of driveways, pathways, etc, that will ultimately require the removal of landscaping or the relocation of irrigation lines, etc. All landscaping and irrigation shall be replaced, where possible, if removed for such purposes. Additionally, prior to the removal of the rear-lot landscaping, the developer or contractor shall hold a on-site meeting to discuss how existing irrigation lines, and associated equipment, if any, will be capped or moved. To the best extent possible, the developer shall attempt to retain as many existing shrubs and trees as feasible, to be incorporated into the project landscaping.
48. All LLMD plant material damaged shall be replaced with like sized material within three (3) business days unless an extension is approved, in writing, by the LLMD manager.
49. At no time shall any contractor store or place equipment, signs, temporary utilities or any other items within the public right-of-way or LLMD landscape easement areas.
50. No contractor parking will be allowed on the MJPA streets at any time during construction.
51. No construction personnel or subcontractors may conduct their break time in the LLMD area. Cost for damages will apply as per COA 47.

52. No Certificate of Occupancy will be issued until all LLMD irrigation and/or plantings are restored in-kind and to the satisfaction of the MJPA Operations Director and any outstanding fines are paid in full.
53. Prior to any wall and fence construction or encroachment into the LLMD, the MJPA must be notified for LLMD pre-inspection and schedule of work.

#### **NPDES Provisions (Timing Varies)**

54. Consistent with the requirements of the NPDES Guidelines, the applicant/owner will record a NPDES Maintenance Covenant and Agreement verifying that the property owners or association will have maintenance responsibility for the on-site storm water treatment in perpetuity.
55. Prior to the issuance of grading or building permits, the developer shall submit evidence of an approved Storm Water Pollution Prevention Plan (SWPPP) approved by the State Water Resources Control Board.
56. At all times during construction, the approved Storm Water Pollution Prevention Plan (SWPPP) shall be located in the construction office, and available for review of development compliance.
57. During construction, the developer or designee shall be responsible for: 1) storm water sampling for all storm events where storm water leaves the site and 2) storm water reporting to the State Water Resources Control Board as required by applicable law.
58. Prior to the issuance of building permits, the developer or developer's successor-in-interest applicant shall submit and receive approval of a Storm Water Management Plan addressing long term development storm water mitigation measures.
59. The future site user is responsible for obtaining a NPDES General Industrial Activities Permit (Order No. 97-03-DWQ, CAS000001) for businesses that have applicable Standard Industrial Codes (SIC code).
60. The structural BMPs selected for this project have been approved in concept. The owner shall submit a final WQMP including plans and details providing the elevations, slopes, and other details for the proposed structural BMPs including the *grass lined bio-swales*. The Engineering Department shall review and approve the final WQMP text, plans and details.

#### **Clean Air Provisions (Timing Varies)**

61. Employers with 250 or more employees shall comply with SCAQMD Rule 2202 necessitating development of trip reduction plans, including peak commute trip reduction, ride share programs, and implementing compressed work week schedules. (SCAQMD)
62. Employers with over 250 employees shall provide on-site food vending machines, refrigerator, microwave oven and mail facilities and use reasonable effort to provide an ATM, on-site computer, internet connection, and other services to reduce the need for employees to leave for services during business hours. (MMRP)

63. All cold storage facilities shall install conduit to all loading dock doors accessing the cold storage warehouse in order to accommodate future use plug-in electrical outlets. Additionally, all cold storage facilities shall have a minimum of 20% of the loading dock doors activated for plug-in use prior to the issuance of the certificate of occupancy. The trailer fleets of all cold storage warehouses shall be 100% plug-in ready within 10 years of the certificate of occupancy. (MMRP)
64. The operator shall cooperate with March JPA in pursuit of Carl Moyer clean air grants. Grants shall be used to pursue clean air technology inclusive of clean burning engines, alternate fuel vehicles, diesel particulate traps, alternate fuel refrigeration units, and electrification of dock ramp positions.
65. Projects shall provide short- and long-term bicycle facilities to meet peak season maximum demand (e.g., one bike rack space per 20 vehicle/employee parking spaces). Project shall also provide "end-of-trip" facilities including showers, lockers, and changing space (e.g., four clothes lockers and one shower provided for every 80 employee parking spaces, separate facilities for each gender for projects with 160 or more employee parking spaces). (MMRP)
66. Project shall provide a parking lot design that includes clearly marked and shaded pedestrian pathways between transit facilities and building entrances. (MMRP)
67. Project shall include permanent TMA membership per the Specific Plan Amendment. (MMRP)
68. Project shall be oriented towards existing transit, bicycle, or pedestrian corridor. Setback distance between the project and existing or planned adjacent uses shall be minimized. Setbacks between project buildings and planned or existing sidewalks shall be minimized. Buildings shall be oriented towards existing or planned street frontage. Primary entrances to buildings shall be located along planned or existing public street frontage. Project shall provide bicycle access to any planned bicycle corridor(s). Project shall provide pedestrian access to any planned pedestrian corridor(s). (MMRP)
69. Project shall install Energy Star labeled roof materials. (MMRP)
70. Developer is encouraged to use materials which are resource efficient, recycled, with long life cycles and manufactured in an environmentally friendly way. (MMRP)
71. Developer shall consider an increase of exterior wall insulation and roof insulation over Title 24 requirements; however, a specific percent increase is not required. (MMRP)
72. Prior to issuance of grading or building permit, the Construction Contractor shall submit a Construction Management Plan identifying compliance with all construction mitigation measures identified in the March Business Center Specific Plan (SCH #2002071089). (MMRP)
73. All construction activities shall be limited to between 7:00 a.m. and 8:00 p.m. as feasible. (MEIR)

**MJPA PUBLIC WORKS DEPARTMENT, TRILAKE ENGINEERING**

**General Conditions:**

74. Truck access points will require the intersection be placed in concrete. Currently only the Krameria Avenue and Bunder intersections have concrete.
75. Truck access shave be Krameria Avenue to Village West Drive, Village West Drive to Van Buren Boulevard to I-215.
76. Prior to issuance of the grading permit, the condition of the existing pavement along the property frontage will be evaluated by the City Engineer to determine whether the pavement would require rehabilitation.
77. Access shall be constructed per Riverside County Standard 207A

**Prior to Issuance of Grading Permit:**

78. The developer/property owner shall submit the following to MJP A for review and approval:
  - a. Onsite Grading Plan and Erosion Control Plan
  - b. Drainage Plan, Hydrology and Hydraulic Report for onsite and offsite improvements
  - c. Final WQMP (for reference)
79. The project's design shall be in compliance with WMWD, Riverside County Flood Control and Water Conservation District, Riverside County, CalTrans, MJP A and ADA most recent standards, criteria and requirements and in effect at the time of construction and coordinated with approved plans for adjacent developments.
80. The incremental increase in runoff volume between developed and undeveloped state for the 85<sup>th</sup> percentile storm, shall be retained within onsite private detention facilities.

**Prior to Issuance of Building Permit:**

81. The developer/property owner shall provide water and sewer service to this project in accordance with the requirement of the appropriate utility purveyors. This project is within Western Municipal Water District (WMWD) service area. Water and sewer improvements shall be installed per WMWD Standards and Fire Department requirements. Fire Department and WMWD approval required prior to MJP A approval.
82. Onsite fire hydrants shall be installed as determined by the Fire Department.
83. The developer/property owner shall secure MJP A's and appropriate agency's approval of the improvement plans.
84. All rights-of-way shall be offered to the public and shall continue in force until MJP A accepts or abandons such offers.
85. All weather access shall be provided to the proposed buildings per Fire Department Standards.
86. The developer/property owner shall submit a compaction certification from the Soils Engineer in compliance with the approved geotechnical/soils report.

**Prior to Issuance of First Certificate of Occupancy:**

87. Unless currently annexed, this project shall annex to existing landscape, lighting, and drainage maintenance districts established by MJPA. Owner shall be responsible to pay towards maintenance of all offsite facilities including drainage improvements as determined by the Districts. Onsite underground basin(s), pipes, lights and landscape shall be maintained by the owner.
88. The existing improvements along Krameria Avenue if damaged during construction of this project shall be removed and replaced to the satisfaction of the MJPA.
89. Striping of Krameria Avenue shall be refreshed along the site to the limits determined by MJPA's Inspector.

#### **MJPA NPDES DEPARTMENT, CGRME**

90. The development shall be subject to all provisions of the March JPA General Plan Section 1.8, which establishes requirements to meet all local, state, and federal environmental requirements. This includes the implementation of stormwater/urban runoff management and discharge control to improve water quality and comply with federal regulations, and any subsequent amendments, revisions, or ordinances pertaining thereto. This project is included into the North Campus Master WQMP.

The project must meet the following criteria:

91. Site Use Determination – if the site will allow for Statewide Industrial General Permit businesses, each parcel must be self-treating/self-retaining. If this is not feasible, the Conditions of Approval must state that the businesses which require the Statewide Industrial General Permit (IGP) cannot be allowed.
92. Infiltration Testing- The provided infiltration testing needs to be reviewed. The geotechnical engineer's recommendations were not used in the BMP calculations. The project calculations must be compliant with the Riverside County LID BMP Design Handbook Appendix A "Infiltration Testing Guidelines". The BMP's will need to be larger based on a corrected infiltration rate.
93. Self-Treating/Retaining Landscape – Self –treating or self-retaining landscape must be included in the project design.
94. Trash enclosures must be covered.
95. The structural BMP's selected for this project have been approved in concept only. The proposed structural BMP's are only approved if non-IGP businesses are located at this site. The owner shall submit a final a final WQMP including plans and details providing the elevations, slopes, and other details for the proposed structural BMP's including underground detention system, self-treating/retaining landscaping, roof drains to vegetation and trash enclosure. The Engineering Department shall review and approve the final WQMP text, plans and details.

## **MJPA BUILDING AND SAFETY DEPARTMENT, WILLDAN**

Provide (4) sets of construction plans and documentation for plan review of the proposed project. Below you will find a list of the plans and documents Building and Safety will need for plan review. The initial plan review will take approximately two weeks on most projects. Please contact the March JPA Plans Expediter, 951-656-7000 for electronic plan review submittals.

### **Building & Safety General Information**

96. All structures shall be designed in accordance with the 2019 California Building Code, 2019 California Mechanical Code, 2019 California Plumbing Code, and the 2019 California Electrical Code, and the 2019 California Green Buildings Standards adopted by the State of California.
97. The Developer/Owner is responsible for the coordination of the final occupancy. The Developer/Owner shall obtain clearances from each department and division prior to requesting a final building inspection from Building & Safety. Each agency shall sign the bottom of the Building & Safety Job Card.
98. Building & Safety inspection requests can be made twenty-four (24) hours in advance for next day inspection.
99. All construction sites must be protected by a security fence and screening. The fencing and screening shall be maintained always to protect pedestrians.
100. Temporary toilet facilities shall be provided for construction workers. The toilet facilities shall be maintained in a sanitary condition. Construction toilet facilities of the non sewer type shall conform to ANSI requirements.
101. Construction projects which require temporary electrical power shall obtain an Electrical Permit from Building & Safety. No temporary electrical power will be granted to a project unless one of the following items are in place and approved by Building & Safety and the Planning Department.
  - (A) Installation of a construction trailer, or,
  - (B) Security fenced area where the electrical power will be located.
102. Installation of construction/sales trailers must be located on private property. No trailers can be in the public street right of way.
103. Site development shall be designed to provide access to all entrances and exterior ground floors exits and access to normal paths of travel, and where necessary to provide access. Paths of travel shall incorporate (but not limited to) exterior stairs, landings, walks and sidewalks, pedestrian ramps, curb ramps, warning curbs, detectable warning, signage, gates, lifts, and walking surface materials. The accessible route(s) of travel shall be the most practical direct route between accessible building entrances, site facilities, accessible parking, public sidewalks, and the accessible entrance(s) to the site, California Building Code, (CBC) Chapter 11, Sections, 11A and 11B.
104. March JPA enforces the State of California provisions of the California Building Code



disabled access requirements. The Federal Americans with Disabilities Act (ADA) standards may differ in some cases from the California State requirements; therefore, it is the building owner's responsibility to be aware of those differences and comply accordingly.

105. Site facilities such as parking open or covered, recreation facilities, and trash dumpster areas, guard shacks and common use areas shall be accessible per the CBC, Chapter 11.
106. Separate plan submittals and permits are required for all accessory structures; example would be trash enclosures, patios, block walls, play equipment and storage buildings.
107. Pursuant to the California Business and Professions Code Section 6737, most projects are required to be designed by a California Licensed Architect or Engineer. The project owner or developer should review the section of the California Codes and comply with the regulation.
108. Building & Safety, (Willdan Engineering) will review building plans, inspect all foundation work, structural work on the building to include plumbing and electrical in the building and on site, mechanical, title 24, green building code items. Building & Safety will also inspect the electrical conduit in the common areas for light standards as well as accessibility path of travel from the right of way to the building and the disabled parking stalls. Signage will be inspected by Building & Safety as well as trash enclosures and other common use structures on the site.
109. Engineering, (Tri Lake Engineering) will review rough and precise grading plans on site as well as all improvements in the public right of way. Tri Lake Engineering will inspect on site utilities starting 5 foot outside of the building as well as all the curb, gutter, and sidewalk construction.
110. Storm Water, (CG Recourse Management) will inspect the Storm Water requirements on and off site.
111. Western Municipal Water District inspects all water service installations on site and off site as well as any sewer connections to grease interceptors on site.

#### **Building & Safety Conditions**

112. Prior to the issuance of a building permit, the applicant shall pay all Development Improvement Fee's; this also includes school fees and outside agency fees such as sewer water and utilities. Copies of receipts shall be provided to Building & Safety prior to permit issuance.
113. All on site utilities shall be underground to the new proposed structure unless prior approval has been obtained by the utility company and March JPA.
114. Prior to issuance of building permits, on site water service shall be installed and approved by the responsible agency. On site fire hydrants shall be approved by the Fire Department. No flammable materials will be allowed on the site until the fire hydrants are established and approved.
115. Prior to issuance of building permits, site grading certification and pad certifications shall be submitted to Building & Safety. Prior to concrete placement, submit a certification for the finish floor elevation and setbacks of the structures. The certification needs to reflect that the

structure is in conformance with the precise grading plans. Compaction reports shall accompany pad certifications. The certifications are required to be signed by the engineer of record.

116. Prior to issuance of building permits, provide Building & Safety with a Will Service Letter from Western Municipal Water District.
117. All construction projects shall comply with the National Pollutant Discharge Elimination Systems (NPDES).
118. Special inspectors and structural observation inspectors that are required by the building code shall register with Building & Safety. The special inspectors shall provide a copy of their certifications as well as a copy of their driver's license before they start work as an inspector on the project. Special inspectors are required to provide a hard copy of the reports in the construction office daily for the inspectors use. The special inspectors shall make all necessary and required inspections before March JPA inspectors provide periodic inspections.
119. A Certificate of Occupancy will be granted to the Developer/Owner when the building is complete, and all agencies have signed for permanent occupancy. If the Developer/Owner has a tenant that will be performing a tenant improvement, Building & Safety may issue a permit for the tenant improvement prior to completion of the original building permit if all other agencies agree to allow the permit to be issued and the plans for the tenant improvement have been approved.

#### **RIVERSIDE COUNTY FIRE DEPARTMENT, OFFICE OF THE FIRE MARSHALL**

120. Prior to issuance of Certificate of Occupancy or Building Final, the applicant/developer shall install a fire sprinkler system based on the information provided. Fire sprinkler plans shall be submitted and approved by the Office of the Fire Marshal prior to installation. (CFC Section 903)
121. Prior to issuance of Certificate of Occupancy or Building Final, the applicant/developer shall install a monitored manual/automatic fire alarm system in accordance with CFC Section 907. Plans shall be submitted and approved by the Office of the Fire Marshal prior to installation. (CFC Section 907)
122. Prior to issuance of Certificate of Occupancy or Building Final, all commercial buildings shall display street numbers in a prominent location on the address side and additional locations as required to facilitate emergency response. The numerals shall be a minimum of twelve inches in height. (CFC 505.1)
123. Prior to issuance of Certificate of Occupancy, approval shall be required from the Office of the Fire Marshal to maintain, store, use, or handle hazardous materials regulated by the fire code and to install equipment used in connection with such activities. (CFC 105)
124. Prior to issuance of Building Permit, plans for an emergency responder radio coverage system in accordance with Riverside County Fire Department policy shall be submitted to the Office of the Fire Marshal for review and approval. (CFC 510.1)
125. Final fire and life safety conditions will be addressed when the Office of the Fire Marshal

reviews building plans. These conditions will be based on occupancy, use, California Building Code (CBC), California Fire Code (CFC), and related codes, which are in effect at the time of building plan submittal.

126. Minimum fire flow for the construction of all commercial buildings is required per CFC Appendix B and Table B105.1. Prior to building permit issuance, the applicant/developer shall provide documentation to show there exists a water system capable of delivering the fire flow based on the information given. Subsequent design changes may increase or decrease the required fire flow.
127. Prior to building construction, fire apparatus access roads extending beyond 150 feet which have not been completed shall have a turnaround capable of accommodating fire apparatus. (CFC 503.2.5)
128. During phased construction, fire apparatus access roads extending beyond 150 feet which have not been completed shall have a turn-around capable of accommodating fire apparatus. (CFC 503.1 and 503.2.5)
129. If construction is phased, each phase shall provide approved emergency vehicular access for fire protection prior to any building construction. (CFC 501.4)
130. Prior to issuance of Building Permits, an approved site plan for fire apparatus access roads and signage shall be submitted and approved by the Office of the Fire Marshal. (CFC 501.3)
131. The Fire Apparatus Access Road shall be (all weather surface) capable of sustaining an imposed load of 75,000 lbs. GVW. The fire apparatus access road or temporary access road shall be reviewed and approved by the Office of the Fire Marshal and in place during the time of construction. (CFC 501.4)
132. Fire apparatus access roads shall have an unobstructed width of not less than twenty-four (24) feet as approved by the Office of the Fire Marshal and an unobstructed vertical clearance of not less the thirteen (13) feet six (6) inches or 15 feet if project is located in a State Responsibility Area Fire Hazard Zone. (CFC 503.2.1)
133. Prior to issuance of a Certificate of Occupancy or Building Final, a “Knox Box Rapid Entry System” shall be provided. The Knox-Box shall be installed in an accessible location approved by the Fire Code Official. All electronically operated gates shall be provided with Knox key switches and automatic sensors for access by emergency personnel. (CFC 506.1)
134. The minimum number of fire hydrants required, as well as the location and spacing of fire hydrants, shall comply with the C.F.C. and NFPA 24. Fire hydrants shall be located no closer than 40 feet from a building. A fire hydrant shall be located within 50 feet of the fire department connection for buildings protected with a fire sprinkler system. The size and number of outlets required for the approved fire hydrants are (6” x 4” x 2 ½” x 2 ½”) (CFC 507.5.1, 507.5.7, Appendix C, NFPA 24-7.2.3)
135. Prior to issuance of Certificate of Occupancy or Building Final, “Blue Reflective Markers” shall be installed to identify fire hydrant locations in accordance with City specifications. (CFC 509.1)

136. Prior to issuance of Building Permits, the applicant/developer shall furnish one copy of the water system plans to the Office of the Fire Marshal for review and approval.
137. The required water system, including fire hydrants, shall be installed, made serviceable, and be accepted by the Office of the Fire Marshal prior to beginning construction. They shall be maintained accessible.
138. Existing fire hydrants on public streets are allowed to be considered available. Existing fire hydrants on adjacent properties shall not be considered available unless fire apparatus access roads extend between properties and easements are established to prevent obstruction of such roads. (CFC 507, 501.3)
139. Fire safety during construction shall comply with the requirements of Chapter 33. (CFC Chapter 33 & CBC Chapter 33)
140. Requests for installation of traffic calming designs/devices on fire apparatus access roads shall be submitted and approved by the Office of the Fire Marshal.

#### **RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

141. Prior to the issuance of building permits, the landowner shall convey an avigation easement to the MARB/MIP Airport.
142. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
143. The following uses shall be prohibited:
  - a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - b) Any use which would cause sunlight to be reflected toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport.
  - c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, incinerators, and landfills.)

- d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- e) The following notice shall be provided to all potential purchasers and tenants of the buildings. While it is not required, the applicant and its successors-in-interest are encouraged to provide a copy of said "Notice of Airport in Vicinity" to employees who would regularly be working therein:

**NOTICE OF AIRPORT IN  
VICINITY**

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business and Professional Code Section 11010 (b) (13) (A)

- f) Any retention basin shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
- g) Noise attenuation measures shall be incorporated into the design of office areas of structures, as necessary to ensure interior office noise levels from aircraft operations are at or below 45 CNEL.

**WESTERN MUNICIPAL WATER DISTRICT**

- 144. Compliance with water efficient landscape requirements per the March Joint Powers Authority Ordinance.
- 145. All applicable Added Facilities Charges (for water facilities), Sewer Connection Fees, and Meter Installation Fees must be paid as well as Cross Connection Testing and regulatory compliance approved prior to the installation of any water meter.
- 146. Proposed facilities for water, sewer, recycled water service as well as onsite irrigation facilities must be designed by a Registered Civil Engineer, and reviewed and approved by

Western. Deposit for Plan Check must be received prior to plan submittal and Deposit for Inspection must be received prior to approval of the plans.

147. Western, as a member agency of Metropolitan Water District of Southern California (MWD) will enforce MWD's Plan for Water Use Efficiency Guideline requirements for water conservation.
148. The property is located within the 1900' Pressure Zone. Currently, Western has an existing 18" water pipeline located Krameria Avenue for service connection to the proposed project. The available Fire Flow must be determined by a flow test or fire flow modeling.
149. Coordinate with fire protection agency of jurisdiction to determine required fire flow for the proposed project and advise Western of the fire flow requirements. Submit request to Western for fire flow modeling to determine if existing water systems capacity is available to provide the required fire flow. Depending on the results of the fire flow modeling additional conditions of approval such as upsizing of existing pipes, extension of pipes, installation of parallel piping or installation of pumps, at the developer's cost, may be required.
150. All backflow prevention devices shall be Reduced Pressure Detector Assemblies (RPDA's) per Western's standards (current maximum size of 10-inch). If larger backflow device is needed developer may be required to install RPDA's in parallel.
151. Western has an existing 12" recycled water pipeline in Krameria Avenue for future recycled water service connection to the proposed project. The project landscape shall be designed and installed for future recycled water use. Please consult Western to determine recycled water landscape areas that require separate recycled water services to the property. Existing recycled water services may be existing that can remain, new recycled (future) and potable water services may need to be added by developer.
152. Western has an existing 15" sewer pipeline in Krameria Avenue for service connection to the proposed project.
153. Developer to submit a 22" x 34" preliminary onsite and/or offsite plan of water, sewer and/or recycled water plan layout to Western before formal submittal of Water, Sewer and/or Recycled Improvement Plans.
154. Preliminary water, sewer and/or recycled water plans shall show the following items:
  - a. Delineate all existing utility facilities (i.e.; pipe diameter, pipe material, manholes, water meters, air/vac, blow-off, fire hydrants, valves, sewer, gas, communication, electrical, etc.) within project boundaries.
  - b. Delineate all easements within project boundaries.
  - c. Delineate all proposed and existing lots, streets, and storm drains.
  - d. Delineate all proposed water, sewer and/or recycled water facilities within project boundaries. Include pipeline diameters and type of material.
155. Developer to submit a detailed engineer's construction cost estimate to Western for review and approval. Once approved, developer shall make a deposit for plan checking services for Water, Sewer and/or Recycled Water Improvement Plans.

156. Water, Sewer and/or Recycled Water Improvement Plans shall be designed per Western's Standard Specifications. Please review Western's Developer Handbook for submittal formats and requirements online at:

<http://www.wmwd.com/162/Developer-Handbook>

157. Water, Sewer and Recycled Water Commercial Plans for connections only shall be designed per Western's Standards and Specifications. Please review Western's Commercial Plan Check Package for submittal formats and requirements online at:

<http://www.wmwd.com/DocumentCenter/Home/View/237>

158. Developer to submit grading plans for Western's review and approval before grading permit is issued.

159. Water, Sewer and/or Recycled Water Improvement Plans or Commercial Plans shall not be approved until all items mentioned above are received and approved by Western.

160. The developer is responsible for installing, paying all costs and obtaining an encroachment permit from the local jurisdiction having authority over installation of water, sewer and/or recycled water facilities including laterals in the public right-of-way. If the customer chooses to cross private property then the customer is responsible to obtain private easements from adjacent property owners. Proof of private easement must be provided to Western prior to plan approval.

161. Provide and/or pay for all applicable cost and fees including connection facilities, relocation of facilities, and additional facilities that may be necessary to accommodate applicant's proposed water and sewer usage, while maintaining resiliency of pipelines within Western's distribution system. This may include the upsizing of pipelines, installation of pressure reduction, and/or pump stations (subject to the application of appropriate credits for additional facilities provided by applicant).

162. For water, sewer and/or recycled water service by Western, the developer must comply with the "Standard Conditions," and all applicable Rules, Regulations, and General Policies of Western at the time of construction.

163. Contact Western's Development Services Department at (951) 571-7100 for further information.

### **UNITED STATES POSTAL SERVICE**

164. Prior to the issuance of building permits, the developer shall contact the U.S. Postal Service to determine the appropriate type and location of mailboxes.

## **Exhibit A**

### **Standard March JPA Operation Mitigation Measures**



### **To be placed on the cover sheet of all construction plan submittals**

1. Future uses shall comply with all applicable permitting requirements for new or modified emission sources subject to SCAQMD rules and regulations.
2. All internal combustion engine-driven equipment will be properly maintained and tuned according to manufactures specifications.
3. Hazardous materials certified handler and disposal service shall conduct the disposal and servicing of any hazardous materials.
4. All landscaped areas shall be maintained in a healthy and thriving condition, free from weeds, trash and debris by the developer or the developer's successor-in-interest. The developer, or the developer's successor-in-interest, shall be responsible for maintaining any undeveloped portion of the site in a manner that provides for the control of weeds, erosion and dust.
5. Green waste generated by the Development shall be kept separate from other waste types in order that it can be recycled for onsite composting or directed to local wood grinding and/or composting facility.
6. Any graffiti located onsite shall be removed within 72 hours. The site shall be maintained in a graffiti-free state at all times.

# Attachment 7

## Notice of Exemption

# MARCH JOINT POWERS AUTHORITY



## NOTICE OF EXEMPTION

TO:	FROM:
<input checked="" type="checkbox"/> Office of Planning and Research P. O. Box 3044, Room 113 Sacramento, CA 95812-3044	(Public Agency) March Joint Powers Authority
<input checked="" type="checkbox"/> Riverside County Clerk 2724 Gateway Drive Riverside, CA 92507	Address 14205 Meridian Parkway, Suite 140 Riverside, CA 92518

1. Project Title:	Design Plan 21-02: March Business Center – South Campus Building DJT6 Package Delivery Facility project.
2. Project Applicant:	Meridian Park, LLC.
3. Project Location – Identify street address and cross streets or attach a map showing project site (preferably a USGS 15' or 7 1/2' topographical map identified by quadrangle name):	
4. Project Location	County of Riverside
5. Description of nature, purpose, and beneficiaries of Project:	Design Plan 21-02 is a project consisting of a 219,000 square foot Package Delivery Facility with 16 dock doors and a building height of 48 feet to the top of the parapets. The Project proposes 17% landscaping coverage (10% required), 449 vehicle parking spaces, including 7 ADA spaces and 2 ADA-Van spaces, 1024 van parking spaces, 6 bicycle parking spaces, and 13 truck/trailer parking stalls along with associated drive aisles, as well as necessary utilities. This project supersedes Design Plan 21-01 for the Meridian South Campus Building F project located exclusively on Parcel 19 of the Meridian South Campus map.
6. Name of Public Agency approving project:	March Joint Powers Authority

7.	Name of Person or Agency undertaking the project, including any person undertaking an activity that receives financial assistance from the Public Agency as part of the activity or the person receiving a lease, permit, license, certificate, or other entitlement of use from the Public Agency as part of the activity:	Timothy Reeves Meridian Park, LLC
8.	Exempt status: (check one)	
(a)	<input checked="" type="checkbox"/> Ministerial project.	(Pub. Res. Code § 21080(b)(1); State CEQA Guidelines § 15268 and 15061(b)(3))
(b)	<input type="checkbox"/> Not a project.	
(c)	<input type="checkbox"/> Emergency Project.	
(d)	<input type="checkbox"/> Categorical Exemption. State type and section number:	
(e)	<input type="checkbox"/> Declared Emergency.	
(f)	<input type="checkbox"/> Statutory Exemption. State Code section number:	
(g)	<input type="checkbox"/> Other. Explanation:	
9.	Reason why project was exempt:	The approval of the design plan is exempt from environmental review pursuant to State CEQA Guidelines section 15268 (a) for Ministerial Projects. The project was previously analyzed under the Certified Environmental Impact Report for the March Business Center Specific Plan (SCH #2002071089). The project underwent a Ministerial Review process and was found to be consistent with the development standards under the March Business Center Specific Plan (SP-1, A7), the adopted March Business Center Specific Plan Mitigation Measures, March Business Center Design Guidelines and the Certified Final Focused Environmental Impact Report (SCH#2002071089). All services and access to the parcel is made available through SP-1. Determining that the design plan was consistent involved no personal judgement by the March Business Center Implementation Committee and involved only the use of fixed and objective standards rather than subjective decision making. The project is also exempt under State CEQA Guidelines section 15061(b)(3), because it can be seen with certainty that the approval of a design plan makes no material changes to a previously analyzed and approved Specific Plan and will not have a significant impact on the environment.
10.	Lead Agency Contact Person:	Mathew Evans
	Telephone:	(951) 656-7000

11.	If filed by applicant: Attach Preliminary Exemption Assessment (Form "A") before filing.
12.	Has a Notice of Exemption been filed by the public agency approving the project? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
13.	Was a public meeting held by the Lead Agency to consider the exemption? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> If yes, the date of the public hearing was: <u>August 24, 2021</u>

Signature: \_\_\_\_\_ Date: \_\_\_\_\_ Title: \_\_\_\_\_

Name: Mathew Evans

☒ Signed by Lead Agency      ☐ Signed by Applicant

Date Received for Filing: \_\_\_\_\_

(Clerk Stamp Here)

Authority cited: Sections 21083 and 21110, Public Resources Code.  
Reference: Sections 21108, 21152, and 21152.1, Public Resources Code.

Attachment 8  
Approved Traffic Consistency Memo



July 23, 2021

Mr. Timothy Reeves  
Lewis Retail Centers  
1156 N. Mountain Avenue  
Upland, CA 91785

**SUBJECT: MERIDIAN SOUTH CAMPUS DJT6 TRAFFIC CONFORMANCE LETTER**

Dear Mr. Timothy Reeves:

Urban Crossroads, Inc. is pleased to provide the following Traffic Conformance Letter for Meridian South Campus DJT6 development (**Project**) which is located north of Krameria Avenue and on either side of Bundy Avenue within the jurisdiction of the March Joint Powers Authority (March JPA). As discussed in more detail below, this work effort has determined the proposed Project is consistent with and within the envelope of traffic evaluated for the currently adopted uses in the Meridian South Campus Traffic Impact Analysis (dated April 29, 2020, referred to as **2020 Traffic Study**) which was approved as part of the March Business Center Specific Plan SP-1 Amendment #8 (dated May 2020).

**PROPOSED PROJECT TRIP GENERATION**

The Project proposes a 219,000 square foot delivery station facility with approximately 202,570 square feet of operational space (see Exhibit 1). Trip generation was calculated based on trip generation rates developed and published in the Delivery Station California Trip Generation Study White Paper (prepared by NV5, dated February 2021). The Project trip generation rates are provided on Table 1.

**TABLE 1: TRIP GENERATION RATES**

Land Use <sup>1</sup>	Units <sup>2</sup>	AM Peak Hour			PM Peak Hour			Daily
		In	Out	Total	In	Out	Total	
Actual Vehicles:								
Delivery Station	TSF	0.26	0.05	0.31	0.26	0.29	0.55	9.66
Passenger Cars		0.25	0.05	0.30	0.13	0.29	0.42	6.78
Delivery Vans		0.00	0.00	0.00	0.13	0.00	0.13	2.71
Line Haul Trucks		0.00	0.01	0.01	0.00	0.00	0.00	0.17

<sup>1</sup> Source: Delivery Station California Trip Generation Study White Paper, prepared by NV5, February 2021.

<sup>2</sup> TSF = thousand square feet

The Project trip generation summary is shown on Table 2. Note that trip generation for the delivery station is based on the operational square footage and not the overall building square footage. The proposed Project generates a total of 1,958 two-way trips per day with 63 AM peak hour trips and 112 PM peak hour trips (in actual vehicles). The Project generates a total of 34 daily two-way line-haul truck trips.

**TABLE 2: PROJECT TRIP GENERATION SUMMARY**

Land Use	Quantity Units <sup>1</sup>	AM Peak Hour			PM Peak Hour			Daily
		In	Out	Total	In	Out	Total	
<b>Actual Vehicles:</b>								
Delivery Station	202,570 TSF							
Passenger Cars:		51	10	61	27	58	85	1,374
Delivery Vans:		0	0	0	27	0	27	550
Line-Haul Trucks:		0	2	2	0	0	0	34
<b>Total Trips (Actual Vehicles)<sup>2</sup></b>		<b>51</b>	<b>12</b>	<b>63</b>	<b>54</b>	<b>58</b>	<b>112</b>	<b>1,958</b>

<sup>1</sup> TSF = thousand square feet

<sup>2</sup> Total Trips = Passenger Cars + Delivery Vans + Truck Trips.

## CONSISTENCY WITH TRAFFIC PLANNING DOCUMENTS

### SITE SPECIFIC ANALYSIS

The Project site encompasses a 44.8-acre Industrial area, which was previously evaluated in the 2020 Traffic Study assuming 700,000 square feet of high-cube cold storage warehouse use and 274,437 square feet of warehousing use. Table 3 summarizes the land uses and associated trip generation evaluated in the 2020 Traffic Study for the Project site.



**TABLE 3: 2020 TRAFFIC STUDY TRIP GENERATION**

Land Use	Quantity Units <sup>1</sup>	AM Peak Hour			PM Peak Hour			Daily
		In	Out	Total	In	Out	Total	
Warehousing	274.437 TSF							
Warehouse Passenger Cars		31	9	40	13	34	47	302
Warehouse Truck Trips		14	4	18	3	10	13	178
Warehousing Subtotal		45	13	58	16	44	60	480
High-Cube Cold Storage Warehouse	700.000 TSF							
Cold Storage Passenger Cars		41	12	53	18	48	66	1,000
Cold Storage Truck Trips		18	6	24	5	13	18	584
High-Cube Cold Storage Warehouse Subtotal		59	18	77	23	61	84	1,584
Currently Approved Passenger Car Trips		72	21	93	31	82	113	1,302
Currently Approved Truck Trips		32	10	42	8	23	31	762
<b>Currently Approved Trip Generation</b>		<b>104</b>	<b>31</b>	<b>135</b>	<b>39</b>	<b>105</b>	<b>144</b>	<b>2,064</b>

<sup>1</sup> TSF = Thousand Square Feet

As shown on Table 4, the proposed Project is anticipated to generate 106 fewer two-way trips per day with 72 fewer AM peak hour trips and 32 fewer PM peak hour trips as compared to the uses evaluated in the 2020 Traffic Study. Additionally, the proposed Project is anticipated to generate 728 fewer daily truck trips as compared to the uses evaluated in the 2020 Traffic Study. **As shown on Table 4, the proposed Project would result in fewer trips, and therefore reduced impacts, compared to the currently approved uses evaluated in the 2020 Traffic Study.**

**TABLE 4: TRIP GENERATION COMPARISON**

Land Use	AM Peak Hour			PM Peak Hour			Daily
	In	Out	Total	In	Out	Total	
Currently Approved Land Uses							
Passenger Cars:	72	21	93	31	82	113	1,302
Total Truck Trips:	32	10	42	8	23	31	762
<b>Total Trips</b>	<b>104</b>	<b>31</b>	<b>135</b>	<b>39</b>	<b>105</b>	<b>144</b>	<b>2,064</b>
Proposed Project							
Passenger Cars:	51	10	61	54	58	112	1,924
Total Truck Trips:	0	2	2	0	0	0	34
<b>Total Trips</b>	<b>51</b>	<b>12</b>	<b>63</b>	<b>54</b>	<b>58</b>	<b>112</b>	<b>1,958</b>
<b>Variance</b>							
Passenger Cars:	-21	-11	-32	23	-24	-1	622
Total Truck Trips:	-32	-8	-40	-8	-23	-31	-728
<b>Total Trips</b>	<b>-53</b>	<b>-19</b>	<b>-72</b>	<b>15</b>	<b>-47</b>	<b>-32</b>	<b>-106</b>

## OVERALL SOUTH CAMPUS TRAFFIC ANALYSIS

A summary of the trip generation evaluated for the Meridian South Campus Specific Plan is shown on Table 5, along with the buildings that have been constructed/approved in conjunction with the proposed Project to determine the trips that remain within the Specific Plan. The summary of trips has been provided for both total vehicles and truck trips. Attachment A includes the trip generation for the currently approved South Campus as evaluated in the 2020 Traffic Study. **As shown on Table 5, the proposed Project would not exceed the overall trips evaluated in the 2020 Traffic Study for the South Campus Specific Plan.**

TABLE 5: SUMMARY OF REMAINING TRIPS WITHIN THE SPECIFIC PLAN

Land Use	AM Peak Hour		PM Peak Hour		Daily	
	Total Vehicles	Trucks	Total Vehicles	Trucks	Total Vehicles	Trucks
A. Total South Campus	3,065	461	3,891	414	34,916	4,972
B. Building A	388	79	455	93	2,898	592
C. Building B + Parking Lot	683	210	805	174	4,672	1,720
D. Building C	87	27	94	20	870	320
E. Building D	64	20	80	17	1,120	414
F. Commercial (Parcel 72)	121	2	137	2	1,534	32
G. Proposed Project	63	2	112	0	1,958	34
<b>Remaining After Project<sup>1</sup></b>	<b>1,659</b>	<b>121</b>	<b>2,208</b>	<b>108</b>	<b>21,864</b>	<b>1,860</b>

<sup>1</sup> Remaining After Project = Lines A - B - C - D - E - F - G

## INTERSECTION AND FREEWAY ANALYSIS

The 2020 Traffic Study evaluated 47 intersections and 24 freeway facility segments and ramp junctions, recommending improvements and fair share contributions as necessary. The March JPA included these recommendations as part of the South Campus project approval. Since the proposed Project is anticipated to generate fewer vehicle trips and significantly fewer truck trips, as analyzed above, the proposed Project would have reduced impacts to the region's intersections and roadways. **Therefore, no additional improvements or fair share contributions would be required.**

## SITE ACCESS

The proposed Project has four access points on Krameria Avenue, two located to the west of Bundy Avenue, one aligning with Bundy Avenue, and one to the east on Bundy Avenue.

- The westerly driveway (Driveway 1) is approximately 25-feet wide and would provide access into the delivery station employee parking field (passenger cars only).
- Driveway 2 is approximately 30-feet wide and would provide access to exiting delivery vehicles which are only loading on the south side of the building. No turning conflicts are anticipated with the driveway for Building A on the south side.

Mr. Timothy Reeves  
Lewis Retail Centers  
July 23, 2021  
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- Driveway 3 is 40-feet wide and is proposed to align with the intersection of Bundy Avenue and Krameria Avenue, creating a 4-leg intersection and is also not anticipated to have any turning conflicts with driveways for Building A. Driveway 3 would serve both inbound and outbound trucks only.
- Driveway 4 is the easterly driveway on Krameria Avenue and is approximately 30-feet wide. It is anticipated to serve all inbound delivery vans loading on the north and south sides of the building and departing delivery vans that are loaded on the north side of the building. This driveway will also serve delivery van drivers who park their personal vehicles on-site.

As such, the proposed Project accommodates adequate site access as currently proposed.

## CONCLUSION

As shown on Table 4, a comparison of the trip generation for the proposed Project versus the uses evaluated in the 2020 Traffic Study shows a net reduction in trips. The summary of trips shown on Table 5 indicates the proposed Project-related trips are within the trips evaluated as part of the South Campus 2020 Traffic Study (both for total vehicles and truck trips). With fewer trips, the proposed Project would have reduced impacts to the region's intersections and freeway facilities. As such, the impacts and mitigation measures are anticipated to be the same or less than those identified in the 2020 Traffic Study with the implementation of the proposed Project. Therefore, the proposed Project is consistent with the traffic planning documents and mitigation measures identified in the March Business Center Specific Plan (SP-1 Amendment #8). The site access proposed for the Project accommodates the proposed uses without conflicting with adjacent uses. If you have any questions, please contact me directly at (949) 861-0177.

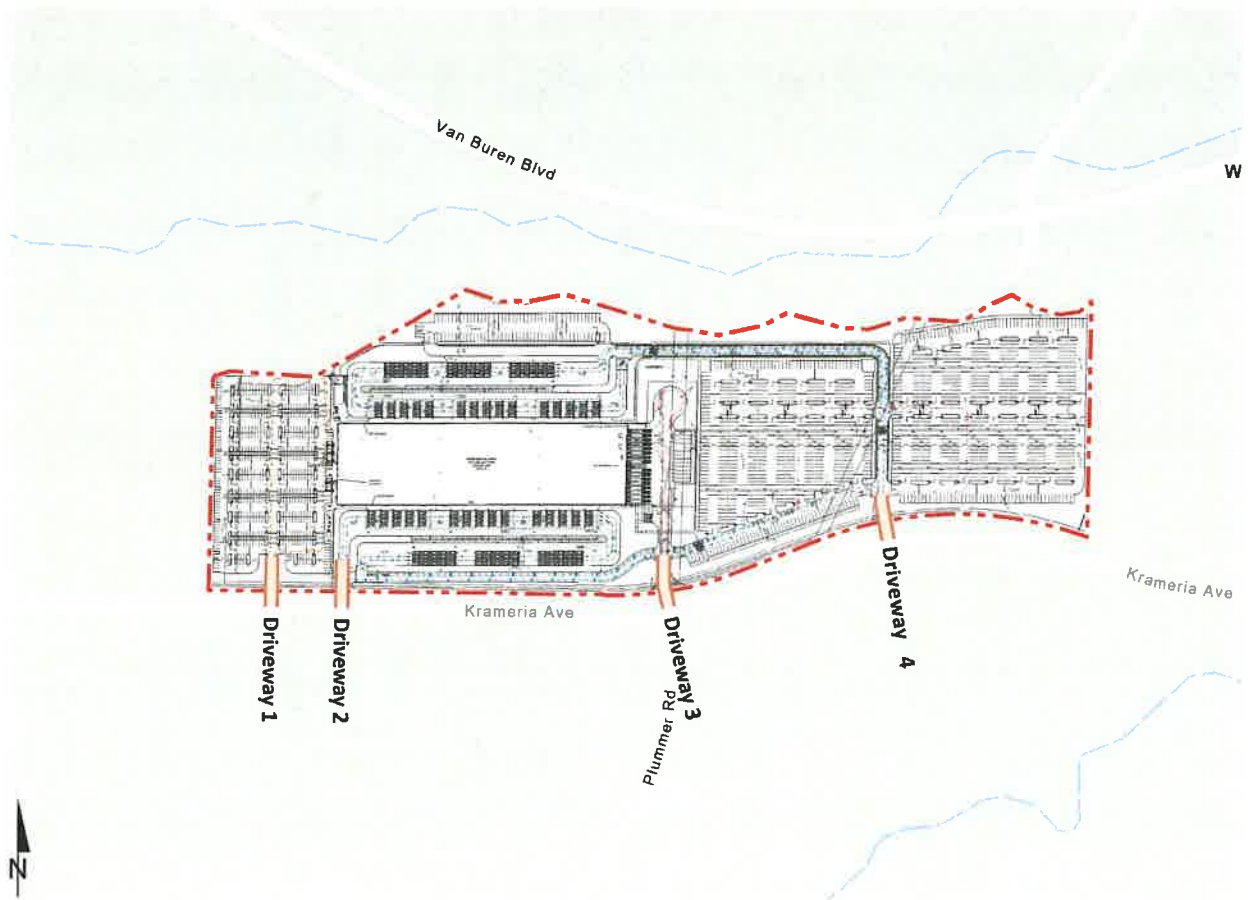
Respectfully submitted,

URBAN CROSSROADS, INC.



Charlene So, PE  
Associate Principal

EXHIBIT 1: PRELIMINARY SITE PLAN



**ATTACHMENT A: TRIP GENERATION EXCERPT FROM MERIDIAN SOUTH CAMPUS TRAFFIC  
IMPACT ANALYSIS (APRIL 2020)**

Table 4-1

## Trip Generation Rates

Land Use <sup>1</sup>	ITE LU	Units <sup>2</sup>	AM Peak Hour			PM Peak Hour			Daily
	Code		In	Out	Total	In	Out	Total	
Trip Generation Rates <sup>1</sup>									
Warehousing <sup>3,4</sup>	150	TSF	Based on the ITE Fitted Curve Equation						
Warehousing (274.437 TSF)			0.163	0.049	0.212	0.060	0.162	0.222	1.746
Warehousing (1,234.926 TSF)			0.108	0.032	0.140	0.038	0.104	0.142	1.617
High-Cube Transload Short-Term Warehouse <sup>3,4</sup>	154	TSF	0.062	0.018	0.080	0.028	0.072	0.100	1.400
High-Cube Cold Storage Warehouse <sup>3,4</sup>	157	TSF	0.085	0.025	0.110	0.032	0.088	0.120	2.263
General Office	710	TSF	Based on the ITE Fitted Curve Equation						
General Office (388.011 TSF)			0.870	0.140	1.010	0.170	0.890	1.060	10.190
General Office (529.254 TSF)			0.850	0.140	0.990	0.170	0.880	1.050	10.090
Shopping Center	820	TSF	Based on the ITE Fitted Curve Equation						
Shopping Center (221.394 TSF)			0.740	0.450	1.190	2.120	2.300	4.420	46.620
Supermarket	850	TSF	Based on the ITE Fitted Curve Equation						
Supermarket (61.336 TSF)			2.368	1.452	3.820	4.516	4.339	8.855	90.660
Public Park	411	AC	Based on the ITE Fitted Curve Equation						
Public Park (6.20 AC)			0.012	0.008	0.020	2.038	1.667	3.705	14.908

<sup>1</sup> Trip Generation Source: Institute of Transportation Engineers (ITE), Trip Generation Manual, Tenth Edition (2017).

<sup>2</sup> AC = Acres; TSF = Thousand Square Feet

<sup>3</sup> Vehicle Mix Source: ITE High Cube Warehouse Vehicle Trip Generation Analysis, October 2016 and SCAQMD.

<sup>4</sup> Truck Percentage: ITE Trip Generation Handbook, 3rd Edition (2017) for Industrial Park; 2% trucks used for office commercial areas

Truck Mix Source: South Coast Air Quality Management District (SCAQMD) Warehouse Truck Trip Study Data Results and Usage (2014).

Normalized % - Without Cold Storage: 16.7% 2-Axle trucks, 20.7% 3-Axle trucks, 62.6% 4-Axle trucks

Normalized % - With Cold Storage: 34.7% 2-Axle trucks, 11.0% 3-Axle trucks, 54.3% 4-Axle trucks.

Table 4-2

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## Proposed Project Trip Generation Summary

Project Land Uses	Quantity	Units <sup>1</sup>	AM Peak Hour			PM Peak Hour			Daily						
			In	Out	Total	In	Out	Total							
Vacant Land Uses															
Office	70.132	TSF													
Office (75% of Mixed Use)	317.879	TSF													
Office Passenger Cars (98%)		331								53	384	65	338	403	3,874
Office Truck Trips (2%)		7	1	8	1	7	8	80							
Office Subtotal	388.011	TSF	338	54	392	66	345	411	3,954						
Commercial Retail	115.434	TSF													
Commercial Retail (25% of Mixed Use)	105.960	TSF													
Commercial Retail Subtotal	221.394	TSF								164	100	264	469	509	978
Pass-by Reduction (AM: 0%, PM/Daily: 34%) <sup>3</sup>		0	0	0	-159	-159	-318	-3,510							
Commercial (Grocery Store)	61.336	TSF	145	89	234	277	266	543	5,562						
Pass-by Reduction (AM: 0%, PM/Daily: 36%) <sup>3</sup>		0	0	0	-96	-96	-192	-2,004							
Commercial Passenger Cars (98%)		303	185	488	481	510	991	10,162							
Commercial Truck Trips (2%)		6	4	10	10	10	20	208							
Commercial Subtotal	282.730	TSF	309	189	498	491	520	1,011	10,370						
Business Park	1,764.180	TSF													
Office (30% of Business Park)	529.254	TSF													
Office Passenger Cars (98%)		441								73	514	88	457	545	5,234
Office Truck Trips (2%)		9	1	10	2	9	11	108							
Warehouse (70% of Business Park)	1,234.926	TSF	133	40	173	47	128	175	1,998						
Warehouse Passenger Cars (69.2% AM, 78.3% PM, 63.2% Daily)		92	28	120	37	100	137	1,262							
Warehouse Truck Trips (30.8% AM, 21.7% PM, 36.8% Daily)		41	12	53	10	28	38	736							
Business Park Subtotal	1,764.180	TSF	583	114	697	137	594	731	7,340						
Industrial	1,774.437	TSF													
Warehousing	274.437	TSF													
Warehouse Passenger Cars (69.2% AM, 78.3% PM, 63.2% Daily)		31								9	40	13	34	47	302
Warehouse Truck Trips (30.8% AM, 21.7% PM, 36.8% Daily)		14	4	18	3	10	13	178							
Warehousing Subtotal		45	13	58	16	44	60	480							
High-Cube Cold Storage Warehouse	700.000	TSF													
Cold Storage Passenger Cars (69.2% AM, 78.3% PM, 63.2% Daily)		41								12	53	18	48	66	1,000
Cold Storage Truck Trips (30.8% AM, 21.7% PM, 36.8% Daily)		18								6	24	5	13	18	584
High-Cube Cold Storage Warehouse Subtotal		59	18	77	23	61	84	1,584							
High-Cube Transload Short-Term Warehouse (Building D)	800.000	TSF													
High-Cube Warehouse Passenger Cars (69.2% AM, 78.3% PM, 63.2% Daily)		34								10	44	17	45	63	706
High-Cube Warehouse Truck Trips (30.8% AM, 21.7% PM, 36.8% Daily)		15								5	20	5	13	17	414
High-Cube Warehousing Subtotal		49	15	64	22	58	80	1,120							
Industrial Subtotal	1,774.437	TSF	153	46	199	61	163	224	3,184						
Dog Park & Paseo	6.200	AC	0	0	0	13	10	23	94						
Vacant Land Uses Passenger Car Trips			1,273	370	1,643	732	1,542	2,275	22,634						
Vacant Land Uses Truck Trips			110	33	143	36	90	125	2,308						
Vacant Land Uses Total Trips <sup>2</sup>			1,383	403	1,786	768	1,632	2,400	24,942						

**Table 4-2**  
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**Proposed Project Trip Generation Summary**

Project Land Uses	Quantity	Units <sup>1</sup>	AM Peak Hour			PM Peak Hour			Daily
			In	Out	Total	In	Out	Total	
Built/Entitled Land Uses									
LGB6 (Building A) <sup>4</sup>	1,000.000	TSF							
LGB6 (Building A) Passenger Cars			222	87	309	127	235	362	2,306
LGB6 (Building A) Truck Trips			57	22	79	33	60	93	592
LGB6 (Building A) Subtotal			279	109	388	160	295	455	2,898
Parcel Delivery Site (Building B + Parking Lot) <sup>5</sup>	1,000.000	TSF							
Parcel Delivery Site (Building B + Parking Lot) Passenger Cars			341	132	473	221	410	631	2,952
Parcel Delivery Site (Building B + Parking Lot) Truck Trips			151	59	210	61	113	174	1,720
Parcel Delivery Site (Building B + Parking Lot) Subtotal			492	191	683	282	523	805	4,672
Commercial (Parcel 72) <sup>6</sup>	15.485	TSF	65	56	121	66	71	137	1,534
Commercial Passenger Cars (98%)			64	55	119	65	70	135	1,502
Commercial Truck Trips (2%)			1	1	2	1	1	2	32
Warehousing (Building C) <sup>6</sup>	500.000	TSF							
Warehousing (Building C) Passenger Cars			46	14	60	20	54	74	550
Warehousing (Building C) Truck Trips			21	6	27	5	15	20	320
Warehousing (Building C) Subtotal			67	20	87	25	69	94	870
Built/Entitled Passenger Car Trips			673	288	961	433	769	1,202	7,310
Built/Entitled Truck Trips			230	88	318	100	189	289	2,664
Built/Entitled Total Trips <sup>2</sup>			903	376	1,279	533	958	1,491	9,974
Vacant + Built/Entitled Passenger Car Trips			1,946	658	2,604	1,165	2,311	3,477	29,944
Vacant + Built/Entitled Truck Trips			340	121	461	136	279	414	4,972
Vacant + Built/Entitled Subtotal Trips <sup>2</sup>			2,286	779	3,065	1,301	2,590	3,891	34,916
Vacant + Built/Entitled Passenger Car Trips (With 10% Internal Trip Reduction)			1,751	592	2,344	1,049	2,080	3,129	26,950
Vacant + Built/Entitled Truck Trips (With 10% Internal Trip Reduction)			306	109	415	122	251	374	4,475
Vacant + Built/Entitled Subtotal Trips (With 10% Internal Trip Reduction)			2,057	701	2,759	1,171	2,331	3,503	31,424
Previous EIR Ph. III Trips			2,965	648	3,613	808	2,907	3,715	31,267
Previous EIR Ph. III Passenger Car Trips (92.6%) (With 10% Internal Trip Reduction)			2,471	540	3,011	673	2,423	3,096	26,058
Previous EIR Ph. III Truck Trips (7.4%) (With 10% Internal Trip Reduction)			197	43	240	54	194	248	2,082
Previous EIR Ph. III Subtotal Trips (With 10% Internal Trip Reduction)			2,668	583	3,251	727	2,617	3,344	28,140
Proposed Project Net Passenger Car Trips <sup>7</sup>			-720	52	-667	376	-343	33	892
Proposed Project Net Truck Trips <sup>7</sup>			109	66	175	68	57	126	2,393
Proposed Project Net Trip Generation <sup>7</sup>			-611	118	-493	444	-286	159	3,284

<sup>1</sup> AC = Acres; TSF = Thousand Square Feet

<sup>2</sup> Total Trips (Actual Vehicles) = Passenger Cars + Truck Trips (Actual Trucks).

<sup>3</sup> Pass-by reduction percentage consistent with ITE [Trip Generation Handbook](#), 3rd Edition (2017)

<sup>4</sup> Source: [LGB6 Project Substantial Conformance Traffic Assessment](#) (November 13, 2017, prepared by Urban Crossroads, Inc.)

<sup>5</sup> Source: [Meridian South Parcel Delivery Traffic Impact Study Report](#) (August 2017, prepared by VRPA Technologies, Inc.)

<sup>6</sup> Source: [Meridian South Campus Addendum #3 Focused Traffic Impact Analysis](#) (August 15, 2018, prepared by Urban Crossroads, Inc.)

<sup>7</sup> Proposed Project = Vacant + Built/Entitled Subtotal Trips (With 10% Internal Trip Reduction) - Previous EIR Ph. III Subtotal Trips (With 10% Internal Trip Reduction)